

Planning Committee Agenda



To: Councillor Toni Letts (Chair)
Councillor Paul Scott (Vice-Chair)
Councillors Muhammad Ali, Sherwan Chowdhury, Chris Clark, Joy Prince,
Jason Perry, Scott Roche, Ian Parker and Gareth Streeter

Reserve Members: Felicity Flynn, Bernadette Khan, Clive Fraser, Leila Ben-Hassel, Helen Redfern, Michael Neal, Badsha Quadir, Jan Buttinger and Andrew Pelling

A meeting of the **Planning Committee** which you are hereby summoned to attend, will be held on **Thursday, 27 February 2020** at the rise of Planning Sub-Committee but not earlier than **6.00 pm** in **Council Chamber, Town Hall, Katharine Street, Croydon CR0 1NX**

JACQUELINE HARRIS BAKER
Council Solicitor and Monitoring Officer
London Borough of Croydon
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www.croydon.gov.uk/meetings
Wednesday, 19 February 2020

Members of the public are welcome to attend this meeting, or you can view the webcast both live and after the meeting has completed at <http://webcasting.croydon.gov.uk>

If you would like to record the meeting, we ask that you read the guidance on the recording of public meetings [here](#) before attending.

To register a request to speak, please either e-mail Democratic.Services@croydon.gov.uk or phone the number above by 4pm on the Tuesday before the meeting.

The agenda papers for all Council meetings are available on the Council website
www.croydon.gov.uk/meetings

If you require any assistance, please contact Michelle Ossei-Gerning
020 8726 6000 x84246 as detailed above.

AGENDA – PART A

1. Apologies for Absence

To receive any apologies for absence from any members of the Committee.

2. Minutes of Previous Meeting (Pages 7 - 10)

To approve the minutes of the meeting held on Thursday 6 February 2020 as an accurate record.

3. Disclosure of Interest

In accordance with the Council's Code of Conduct and the statutory provisions of the Localism Act, Members and co-opted Members of the Council are reminded that it is a requirement to register disclosable pecuniary interests (DPIs) and gifts and hospitality to the value of which exceeds £50 or multiple gifts and/or instances of hospitality with a cumulative value of £50 or more when received from a single donor within a rolling twelve month period. In addition, Members and co-opted Members are reminded that unless their disclosable pecuniary interest is registered on the register of interests or is the subject of a pending notification to the Monitoring Officer, they are required to disclose those disclosable pecuniary interests at the meeting. This should be done by completing the Disclosure of Interest form and handing it to the Democratic Services representative at the start of the meeting. The Chair will then invite Members to make their disclosure orally at the commencement of Agenda item 3. Completed disclosure forms will be provided to the Monitoring Officer for inclusion on the Register of Members' Interests.

4. Urgent Business (if any)

To receive notice of any business not on the agenda which in the opinion of the Chair, by reason of special circumstances, be considered as a matter of urgency.

5. Development Presentations (Pages 11 - 12)

To receive the following presentations on a proposed development:

There are none.

6. Planning Applications for Decision (Pages 13 - 16)

To consider the accompanying reports by the Director of Planning & Strategic Transport:

6.1 19/04987/FUL Land Adjacent To Croydon College, College Road, Croydon, CR0 1PF (Pages 17 - 68)

Redevelopment of the site to provide a part 49 storey and part 34 storey building with basements, comprising 817 co-living units (Use Class Sui Generis) within Tower A and 120 residential units (Use Class C3) within Tower B, a cafe (Use Class A3), community use (Use Class D1), associated communal facilities for co-living residents, amenity spaces, cycle parking, disabled parking spaces, refuse and cycle storage and associated landscaping and public realm works.

Ward: Fairfield

Recommendation: Grant permission

6.2 18/06068/FUL 19 Hartley Old Road, Purley, CR8 4HH (Pages 69 - 90)

Demolition of a single-family dwelling and erection of a 3 storey block containing 9 flats with associated access, car parking, cycle and refuse storage (Amended plans).

Ward: Purley and Woodcote

Recommendation: Grant permission

6.3 19/03628/FUL 5 Croham Valley Road (Pages 91 - 118)

Demolition of the existing property and erection of 6 houses (3 houses fronting Croham Valley Road and 3 houses fronting Ballards Farm Close), gardens, car parking, new accesses, refuse and recycling.

Ward: Selsdon & Ballards

Recommendation: Grant permission

6.4 19/03965/FUL 8 Coulsdon Road, Coulsdon, CR5 2LA (Pages 119 - 146)

Demolition of the existing property and erection of six new apartments and 2 houses (houses to front Petersfield Crescent), with associated new access, parking, refuse and cycle stores and landscaping.

Ward: Old Coulsdon

Recommendation: Grant permission

**6.5 19/04615/FUL Land rear of 31-33 Croham Valley Road,
(Facing onto Ballards Rise), South Croydon
(Pages 147 - 168)**

Erection of 2 two storey detached buildings with accommodation within the roofspace comprising 8 flats, bin store, formation of vehicular access and provision of 7 parking spaces.

Ward: South Croydon
Recommendation: Grant permission

**6.6 19/04705/FUL 16-18 Ash Tree Close, Croydon, CR0 7SR
(Pages 169 - 184)**

Demolition of the existing dwellings. Erection of 8 dwellings with associated access, parking, refuse and cycle stores.

Ward: Shirley North
Recommendation: Grant permission

**6.7 19/05034/FUL 6 Croham Valley Road, South Croydon, CR2
7NA (Pages 185 - 204)**

Demolition of existing house; erection of a two storey building plus roof space to provide 9 apartments; provision of 8 car parking spaces, refuse store and new landscaping.

Ward: South Croydon
Recommendation: Grant permission

7. Items referred by Planning Sub-Committee

To consider any item(s) referred by a previous meeting of the Planning Sub-Committee to this Committee for consideration and determination:

There are none.

8. Other Planning Matters (Pages 205 - 206)

To consider the accompanying report by the Director of Planning & Strategic Transport:

**8.1 Weekly Planning Decisions and Performance
(Pages 207 - 298)**

This report provides a list of cases determined (since the last Planning Committee) providing details of the site and description of development

(by Ward), whether the case was determined by officers under delegated powers or by Planning Committee/Sub Committee and the outcome (refusal/approval).

8.2 Planning Appeal Decisions (January 2020) (Pages 299 - 304)

This report provides details of town planning appeal outcomes and the range of planning considerations that are being taken into account by the Planning Inspectors, appointed by the Secretary of State for Housing, Communities and Local Government.

9. Exclusion of the Press & Public

The following motion is to be moved and seconded where it is proposed to exclude the press and public from the remainder of a meeting:

"That, under Section 100A(4) of the Local Government Act, 1972, the press and public be excluded from the meeting for the following items of business on the grounds that it involves the likely disclosure of exempt information falling within those paragraphs indicated in Part 1 of Schedule 12A of the Local Government Act 1972, as amended."

Planning Committee

Meeting of Croydon Council's Planning Committee held on Thursday, 6 February 2020 at 6.30pm in Council Chamber, Town Hall, Katharine Street, Croydon CR0 1NX

This meeting was Webcast – and is available to view via the Council's Web Site

MINUTES

Present: Councillor Toni Letts (Chair);
Councillor Paul Scott (Vice-Chair);
Councillors Chris Clark, Jason Perry, Scott Roche, Ian Parker, Gareth Streeter, Bernadette Khan (In place of Joy Prince), Clive Fraser (In place of Sherwan Chowdhury) and Andrew Pelling (In place of Muhammad Ali)

Also Present: Councillor Margaret Bird and Stephen Mann

Apologies: For lateness from Councillor Roche

PART A

21/20 **Minutes of Previous Meeting**

RESOLVED that the minutes of the meetings held on Thursday 16 January 2020 with the amendment of the start time of 6:37pm, and Thursday 23rd January 2020 be signed as a correct record.

22/20 **Disclosure of Interest**

There were no disclosures of a pecuniary interest not already registered.

23/20 **Urgent Business (if any)**

There was none.

24/20 **Development presentations**

There were none.

25/20 **Planning applications for decision**

26/20 **19/05106/FUL Land Adjoining 96 Beulah Hill and 1-24 Founders Gardens**

Erection of 3 x 2-storey and 5 x 3-storey 3 bedroom dwelling houses with associated car and cycle parking, waste stores, amenity space and landscaping.

Ward: Crystal Palace and Upper Norwood

The officers presented details of the planning application and officers responded to questions for clarification.

[Councillor Scott Roche, having already given apologies for lateness, had arrived at the meeting during the consideration of this matter at 6:45pm and, in accordance with the Council's constitution, took no part in this item.]

Mr Keith Cunningham spoke against the application.

Mr Malcolm Aslop, the agent, spoke in support of the application.

Referring Ward Member Councillor Stephen Mann spoke against the application.

Councillor Scott proposed a motion to **APPROVE** the application based on the officer's recommendation. Councillor Pelling seconded the motion.

Councillor Parker proposed a motion to **REFUSE** the application on the grounds of loss of trees, over development, impact on the neighbours and out of keeping of the neighbourhood. Councillor Streeter seconded the motion.

The motion to approve the application was put forward to the vote and carried with five Members voting in favour, three Members voting against and one Member abstained their vote. The motion to refuse therefore fell.

The Committee therefore **RESOLVED** to **GRANT** the application for the development of Land Adjoining 96 Beulah Hill and 1-24 Founders Gardens.

27/20 **19/03003/FUL 2 Coulsdon Road, Coulsdon, CR5 2LA**

Demolition of existing dwelling and erection of new building to create 9 flats with associated car and cycle parking provision, refuse storage and landscaping.

Ward: Old Coulsdon

The officers presented details of the planning application and updated the recommendation to include a condition regarding flood mitigation measures. Officers also responded to questions for clarification.

Mr Richard Wosiek spoke against the application.

Mr Matt Smith, the applicant, spoke in support of the application.

Referring Ward Member Councillor Margaret Bird spoke against the application.

Councillor Scott proposed a motion to **APPROVE** the application based on the officer's recommendation with the addition of a condition regarding flood mitigation measures. Councillor Clark seconded the motion.

Councillor Streeter proposed a motion to **REFUSE** the application on the grounds of over development of the site by its dense of its size and massing, compromised amenity and not respectful of the street scene. Councillor Parker seconded the motion.

The motion to approve the application was put forward to the vote and carried with six Members voting in favour and four Members voting against. The motion to refuse therefore fell.

The Committee therefore **RESOLVED** to **GRANT** the application for the development of 2 Coulsdon Road, Coulsdon, CR5 2LA.

28/20 19/05428/FUL 1 Addington Road, South Croydon, CR2 8RE

Demolition of buildings and construction of 60 bed care home for the elderly (C2 use) including car parking and refuse store. Provision of new access on to Sanderstead Hill (closure of existing vehicle access) and associated landscaping.

Ward: Sanderstead

THIS ITEM WAS WITHDRAWN FROM THE AGENDA.

29/20 Items referred by Planning Sub-Committee

There were none.

30/20 Other planning matters

31/20 Weekly Planning Decisions

The report was received for information.

The meeting ended at 8.03 pm

Signed:

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Date:

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PLANNING COMMITTEE AGENDA

PART 5: Development Presentations

1 INTRODUCTION

- 1.1 This part of the agenda is for the committee to receive presentations on proposed developments, including when they are at the pre-application stage.
- 1.2 Although the reports are set out in a particular order on the agenda, the Chair may reorder the agenda on the night. Therefore, if you wish to be present for a particular application, you need to be at the meeting from the beginning.
- 1.3 The following information and advice applies to all those reports.

2 ADVICE TO MEMBERS

- 2.1 These proposed developments are being reported to committee to enable members of the committee to view them at an early stage and to comment upon them. They do not constitute applications for planning permission at this stage and any comments made are provisional and subject to full consideration of any subsequent application and the comments received as a result of consultation, publicity and notification.
- 2.2 Members will need to pay careful attention to the probity rules around predisposition, predetermination and bias (set out in the Planning Code of Good Practice Part 5.G of the Council's Constitution). Failure to do so may mean that the Councillor will need to withdraw from the meeting for any subsequent application when it is considered.

3 FURTHER INFORMATION

- 3.1 Members are informed that any relevant material received since the publication of this part of the agenda, concerning items on it, will be reported to the Committee in an Addendum Update Report.

4 PUBLIC SPEAKING

- 4.1 The Council's constitution only provides for public speaking rights for those applications being reported to Committee in the "Planning Applications for Decision" part of the agenda. Therefore reports on this part of the agenda do not attract public speaking rights.

5 BACKGROUND DOCUMENTS

- 5.1 For further information about the background papers used in the drafting of the reports in part 8 contact Mr P Mills (020 8760 5419).

6 RECOMMENDATION

- 6.1 The Committee is not required to make any decisions with respect to the reports on this part of the agenda. The attached reports are presented as background information.

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PLANNING COMMITTEE AGENDA

PART 6: Planning Applications for Decision

1 INTRODUCTION

- 1.1 In this part of the agenda are reports on planning applications for determination by the Planning Committee.
- 1.2 Although the reports are set out in a particular order on the agenda, the Chair may reorder the agenda on the night. Therefore, if you wish to be present for a particular application, you need to be at the meeting from the beginning.
- 1.3 Any item that is on the agenda because it has been referred by a Ward Member, GLA Member, MP or Resident Association and none of the person(s)/organisation(s) or their representative(s) have registered their attendance at the Town Hall in accordance with the Council's Constitution (paragraph 3.8 of Part 4K – Planning and Planning Sub-Committee Procedure Rules) the item will be reverted to the Director of Planning and Strategic Transport to deal with under delegated powers and not be considered by the committee.
- 1.4 The following information and advice applies to all reports in this part of the agenda.

2 MATERIAL PLANNING CONSIDERATIONS

- 2.1 The Committee is required to consider planning applications against the development plan and other material planning considerations.
- 2.2 The development plan is:
 - the London Plan (consolidated with Alterations since 2011)
 - the Croydon Local Plan (February 2018)
 - the South London Waste Plan (March 2012)
- 2.3 Decisions must be taken in accordance with section 70(2) of the Town and Country Planning Act 1990 and section 38(6) of the Planning and Compulsory Purchase Act 2004. Section 70(2) of the Town and Country Planning Act 1990 requires the Committee to have regard to the provisions of the Development Plan, so far as material to the application; any local finance considerations, so far as material to the application; and any other material considerations. Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires the Committee to make its determination in accordance with the Development Plan unless material planning considerations support a different decision being taken. Whilst third party representations are regarded as material planning considerations (assuming that they raise town planning matters) the primary consideration, irrespective of the number of third party representations received, remains the extent to which planning proposals comply with the Development Plan.
- 2.4 Under Section 66 of the Planning (Listed Buildings and Conservation Areas) Act 1990, in considering whether to grant planning permission for development which affects listed buildings or their settings, the local planning authority must have special regard to the desirability of preserving the building or its setting or any features of architectural or historic interest it possesses.

- 2.5 Under Section 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990, in considering whether to grant planning permission for development which affects a conservation area, the local planning authority must pay special attention to the desirability of preserving or enhancing the character or appearance of the conservation area.
- 2.6 Under Section 197 of the Town and Country Planning Act 1990, in considering whether to grant planning permission for any development, the local planning authority must ensure, whenever it is appropriate, that adequate provision is made, by the imposition of conditions, for the preservation or planting of trees.
- 2.7 In accordance with Article 31 of the Development Management Procedure Order 2010, Members are invited to agree the recommendations set out in the reports, which have been made on the basis of the analysis of the scheme set out in each report. This analysis has been undertaken on the balance of the policies and any other material considerations set out in the individual reports.
- 2.8 Members are reminded that other areas of legislation covers many aspects of the development process and therefore do not need to be considered as part of determining a planning application. The most common examples are:
- **Building Regulations** deal with structural integrity of buildings, the physical performance of buildings in terms of their consumption of energy, means of escape in case of fire, access to buildings by the Fire Brigade to fight fires etc.
 - Works within the highway are controlled by **Highways Legislation**.
 - **Environmental Health** covers a range of issues including public nuisance, food safety, licensing, pollution control etc.
 - Works on or close to the boundary are covered by the **Party Wall Act**.
 - **Covenants and private rights** over land are enforced separately from planning and should not be taken into account.

3 **ROLE OF THE COMMITTEE MEMBERS**

- 3.1 The role of Members of the Planning Committee is to make planning decisions on applications presented to the Committee openly, impartially, with sound judgement and for sound planning reasons. In doing so Members should have familiarised themselves with Part 5D of the Council's Constitution 'The Planning Code of Good Practice'. Members should also seek to attend relevant training and briefing sessions organised from time to time for Members.
- 3.2 Members are to exercise their responsibilities with regard to the interests of the London Borough of Croydon as a whole rather than with regard to their particular Ward's interest and issues.

4. **THE ROLE OF THE CHAIR**

- 4.1 The Chair of the Planning Committee is responsible for the good and orderly running of Planning Committee meetings. The Chair aims to ensure, with the assistance of officers where necessary, that the meeting is run in accordance with the provisions set out in the Council's Constitution and particularly Part 4K of the Constitution 'Planning and Planning Sub-Committee Procedure Rules'. The Chair's most visible responsibility is to ensure that the business of the meeting is conducted effectively and efficiently.
- 4.2 The Chair has discretion in the interests of natural justice to vary the public speaking rules where there is good reason to do so and such reasons will be minuted.

- 4.3 The Chair is also charged with ensuring that the general rules of debate are adhered to (e.g. Members should not speak over each other) and that the debate remains centred on relevant planning considerations.
- 4.4 Notwithstanding the fact that the Chair of the Committee has the above responsibilities, it should be noted that the Chair is a full member of the Committee who is able to take part in debates and vote on items in the same way as any other Member of the Committee. This includes the ability to propose or second motions. It also means that the Chair is entitled to express their views in relation to the applications before the Committee in the same way that other Members of the Committee are so entitled and subject to the same rules set out in the Council's constitution and particularly Planning Code of Good Practice.

5. PROVISION OF INFRASTRUCTURE

- 5.1 In accordance with Policy 8.3 of the London Plan (2011) the Mayor of London has introduced a London wide Community Infrastructure Levy (CIL) to fund Crossrail. Similarly, Croydon CIL is now payable. These would be paid on the commencement of the development. Croydon CIL provides an income stream to the Council to fund the provision of the following types of infrastructure:
- i. Education facilities
 - ii. Health care facilities
 - iii. Projects listed in the Connected Croydon Delivery Programme
 - iv. Public open space
 - v. Public sports and leisure
 - vi. Community facilities
- 5.2 Other forms of necessary infrastructure (as defined in the CIL Regulations) and any mitigation of the development that is necessary will be secured through A S106 agreement. Where these are necessary, it will be explained and specified in the agenda reports.

6. FURTHER INFORMATION

- 6.1 Members are informed that any relevant material received since the publication of this part of the agenda, concerning items on it, will be reported to the Committee in an Addendum Update Report.

7. PUBLIC SPEAKING

- 7.1 The Council's constitution allows for public speaking on these items in accordance with the rules set out in the constitution and the Chair's discretion.

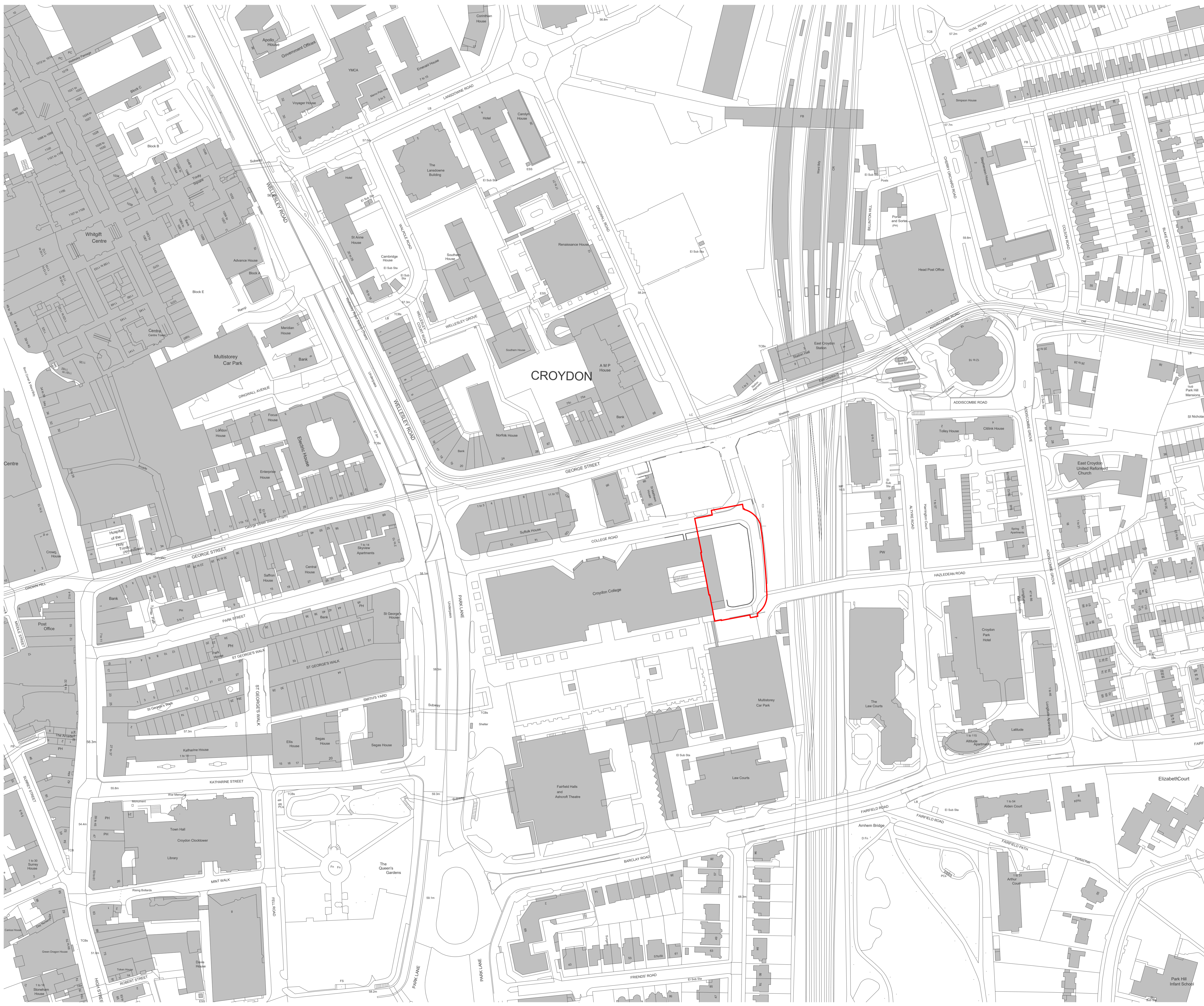
8. BACKGROUND DOCUMENTS

- 8.1 The background papers used in the drafting of the reports in part 6 are generally the planning application file containing the application documents and correspondence associated with the application. Contact Mr P Mills (020 8760 5419) for further information. The submitted planning application documents (but not representations and consultation responses) can be viewed online from the Public Access Planning Register on the Council website at <http://publicaccess.croydon.gov.uk/online-applications>. Click on the link or copy it into an internet browser and go to the page, then enter the planning application number in the search box to access the application.

9. RECOMMENDATION

- 9.1 The Committee to take any decisions recommended in the attached reports.

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APPLICATION BOUNDARY

Notes -
 Do not scale from drawings unless by agreement with HTA. Use figured dimensions only. Check all dimensions on site prior to commencing the works. Drawing to be read in conjunction with other relevant consultant information.
 This drawing is the copyright of HTA Design LLP and must not be copied or reproduced in part, or in whole, without the express permission of HTA Design LLP. Drawing to be read in conjunction with specification.



PLANNING

14.10.2019	HTA	Planning Application
revision	date	drawn description

LOCATION PLAN

TID-CRC_HTA-A_DR_0100
 number

Tide Construction
 Croydon-College Road

TID-CRC 1:1250 @ A1
 job reference scale

0m 25m 50m 75m 100m 125m
 HTA Design LLP
 www.hta.co.uk
 London | Edinburgh | Manchester | Bristol

Agenda Item 6.1
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1.0 SUMMARY OF APPLICATION DETAILS

Ref: 19/04987/FUL
 Location: Land Adjacent to Croydon College, College Road, Croydon, CR0 1PF
 Ward: Fairfield
 Description: Redevelopment of the site to provide a part 49 storey and part 34 storey building with basements, comprising 817 co-living units (Use Class Sui Generis) within Tower A and 120 residential units (Use Class C3) within Tower B, a cafe (Use Class A3), community use (Use Class D1), associated communal facilities for co-living residents, amenity spaces, cycle parking, disabled parking spaces, refuse and cycle storage and associated landscaping and public realm works.
 Drawing Nos: See Appendix 1
 Applicant: Tide Construction Ltd
 Agent: HTA Design LLP
 Case Officer: Louise Tucker

Accommodation	Co-living units	1 bed 2 person	2 bed 3 person	3 bed 4 person
Co-living market rent (Tower A)	817	N/A	N/A	N/A
Intermediate housing (Tower B)	N/A	60	30	30
Total	817	120		

	Type of floor space	Amount proposed
Tower A	Co-living (Class Sui Generis)	35,791.1sqm
	Restaurants and Cafes (Class A3)	56sqm
Tower B	Residential (Class C3)	11,804.5sqm
	Non-residential Institutions (Class D1)	316sqm
	Total	47,966.1sqm

	Number of car parking spaces	Number of cycle parking spaces
Tower A	11 blue badge disabled spaces	283 long stay spaces for Tower A (with 130 spaces for hire cycles)
Tower B	4 blue badge disabled spaces	180 long stay spaces
Total	15 blue badge disabled spaces	463 long stay spaces (with 16 short stay spaces within the public realm)

1.1 This application is being reported to Planning Committee in accordance with the Committee consideration criteria as the scheme proposes more than 200 new residential dwellings.

2.0 BACKGROUND

2.1 An earlier iteration of this proposal was presented to the Planning Committee at pre-application stage on 10th October 2019. This proposed the erection of a part 33 part 48 storey building comprising 836 co-living units (use class sui generis), 120 residential units (use class C3), and associated parking, servicing, landscaping and public realm works.

2.2 The main issues raised were as follows:

- Members had differing views with regards to the height of the towers. Some Members raised concern (with regard to clustering and daylight/sunlight impact on the surrounding area), whilst some Members welcomed the height (but only if the two towers had their own separate identity) and some Members supported a tiered approach, stepping down from 101 George Street to College Green.
- Members welcomed the principle of C3 affordable housing delivery in Tower B (100% shared ownership), however there is a need for affordable rented accommodation provision within the scheme, either as affordable rent within Tower B or consideration of discounted rented accommodation within the co-living Tower A. Members also questioned how the developer will calculate the rental values of the co-living units and what services would be included in the rent.
- Members generally supported the principle of co-living, but requested further assurance/justification in terms of operation, management and safety of them as a living alternative, including what would happen if co-living did not work and needed to be retrofitted to C3. There was general consensus that communal space at top and bottom was appropriate. The variety of finishes on each floor to give them uniqueness was positively received. Some Members were concerned that the scheme does not provide accommodation for facilities. Members challenged the applicant to demonstrate safety of all users.
- There were mixed views on the design of the building. Members discussed the geometry of the building and the awkward points where the façade is stretched, particularly on the corners which needs further work. Members wanted to see individuality in the towers with a different cladding approach on both to avoid coalescence.
- Members reiterated the importance of the link between East Croydon rail station and the Fairfield Halls. Members indicated the colonnade looked positive, but challenged the applicant to ensure the space was as safe, active and well-lit as possible. There were suggestions for the applicant to work further with the neighbouring applicants for elements of Fairfield to be brought into the scheme.
- There was request for provision of blue badge spaces and mobility scooters.
- Daylight and sunlight impact on the surrounding area identified as important and assessment to reflect consented and emerging schemes.

- Some Members challenged the separation of the scheme from other tall buildings, such as 101 George Street and whether this was too close.
- Fire safety was identified as important given the height and clarity needed with submission.

2.3 Since the Committee presentation, the proposal has been further developed, in consultation with officers and the above comments have been taken into account (where possible) in amendments made to the scheme.

2.4 The scheme was presented on two occasions to the Place Review Panel (PRP) at pre-application stage. The main issues raised by the Panel were as follows:

- With regard to the concept of co-living, the Panel felt that this could work for the site given the accessible town centre location. However, given there are very few UK precedents for this housing typology, they felt a lot more research was required to present robust justification of appropriate space standards and design for the individual units, communal spaces and circulation spaces (including of schemes from Europe and around the world).
- The Panel felt it was vital that the building includes a range of high quality shared facilities and enables social interaction between residents for their physical and mental wellbeing, and to mitigate for the small unit sizes.
- In terms of design and townscape impact, the Panel opposed any increase in height of Tower B from the consented scheme and opposed increasing the footprint, due to coalescence with surrounding buildings and harm to townscape and setting of heritage assets. The Panel considered the height of Tower A above that of 101 George Street could be acceptable, subject to views testing.
- The Panel looked forward to see how the design of the public realm will be developed to integrate successfully with the public realm design for the cultural quarter and play a key role in enhancing the important pedestrian route between the cultural quarter and East Croydon station, as well as providing some outdoor space for the scheme's residents.
- The Panel felt microclimate and fire safety need careful consideration.

2.5 Since presenting to the PRP, the proposal has been further developed in consultation with officers and the above comments (where possible) have been addressed in amendments and additional justification provided for the scheme.

3 RECOMMENDATION

2.1 That the Planning Committee resolve to GRANT planning permission subject to:

A. Any direction by the London Mayor pursuant to the Mayor of London Order

B. The prior completion of a legal agreement to secure the following planning obligations:

- a) Affordable housing – 120 C3 residential units (London Shared Ownership) (the entirety of Tower B)
- b) Affordable housing review mechanism (early and late stage review)

- c) Co-living management plan
- d) Units all for rent with minimum tenancy lengths of no less than three months
- e) Operator confirmation and detailed management plan including security
- f) Public realm strategy and contribution
- g) Public realm strategy for wider area and contribution
- h) Financial contribution to child playspace of £13,051
- i) Local Employment and Training strategy and financial contribution of £75,000
- j) Financial contribution to car club space improvements of £24,075
- k) Travel Plan monitoring for 3 years and monitoring fee of £3,988
- l) Retention of scheme architects
- m) Off-site highway works and wind mitigation – s.278 Agreement to cover all associated highway works to facilitate development, and any off site wind mitigation measures
- n) Air quality financial contribution of £19,380
- o) Carbon offsetting contribution for Tower B of £115,200
- p) Remove access for future residents to CPZ permits and season tickets for Council car parks
- q) Transport contributions and requirements (to include cycle contribution, car club contributions/membership, EVCP, ATZ improvements, ANPR camera, cycle scheme on College Road, Delivery and Servicing Plan bond)
- r) Future district heat network connection
- s) TfL financial contribution towards improvements and upgrades to the local public transport network
- t) TV satellite dish mitigation
- u) Wind mitigation works
- v) Marketing and fitting out of all ground floor units for the eventual end occupier
- w) Access to upper floor to the public on at least one occasion per year
- x) Monitoring fees
- y) And any other planning obligations considered necessary

2.2 That the Director of Planning and Strategic Transport has delegated authority to negotiate the detailed terms of the legal agreement, securing additional/amended obligations if necessary.

2.3 That the Director of Planning and Strategic Transport has delegated authority to issue the planning permission and impose conditions and informatives to secure the following matters:

Conditions

1. Commencement of the development within 3 years
2. Development to be carried out in accordance with the approved drawings

Both Tower A and Tower B

Pre-commencement

3. Design development of the crown of the buildings
4. Design development of the plinth articulation
5. Typical façade materials and detailing – 1:20 details used then to produce 1:1 mock-ups, with 1:5 details to confirm following approval
6. 1:1 mock ups of the Crown, Tower A and B portion showing interface, and of the lower amenity levels and colonnade

7. External facing materials, including physical samples and detailed drawings of design elements
8. Public Art strategy, designs and implementation (brief and commissioned pieces for both the colonnade and the elevations including physical samples)
9. Details of public realm and landscape design to ensure it is coordinated with neighbouring developments and the emerging Council College Green public realm designs
10. Tree planting and management strategy
11. Vehicle Dynamics Assessment with hostile vehicle mitigation and anti-terrorist measures
12. Construction logistics plan and method statement
13. Construction Environment Management Plan
14. NATS radar mitigation scheme
15. Aviation warning lights (including construction)
16. Sustainable urban drainage strategy (detailing any on and/or off site drainage works)
17. Impact study of the existing water supply infrastructure
18. Secured by Design/engagement with the Police
19. Accord with Air Quality Assessment and submission of air quality Low emission strategy

Pre-occupation

20. Hard and soft landscaping, including rooftop amenity spaces, children's playspace and equipment, boundary treatments and planters (including detailed sections with proposed planting)
21. Public realm and building lighting scheme (linked with development of the colonnade and elevations) and to include night time illumination
22. Landscaping and public realm management and maintenance strategy
23. Details of fenestration of the ground floor, including shop fronts, glazing, signage zones and co-ordination and enhancement of the public realm
24. Lighting and CCTV of bin and bike stores, surface and under-croft parking areas
25. Details of cycle parking and storage (including staff provision)
26. Access routes and signage down the ramp and into basement for pedestrians, cyclists and cars
27. Refuse store and collection management plan
28. Detailed delivery and Servicing Plan to be submitted
29. Car park management plan (including EVCP)
30. Rooftop amenity for both towers to be agreed including mitigation
31. Window ventilation systems and sound insulation
32. Biodiversity enhancements
33. Café and co-living kitchens extraction details
34. Details of air handling units/plant/machinery and screening
35. Building maintenance strategy including window cleaning

Compliance

36. Co-living elements to be under single management
37. Use of ground floor as Class A3/D1/co-living reception
38. Restriction on hours of use of non-residential uses
39. 35% CO2 reduction on site
40. BREEAM excellent for non-residential
41. All features and materials must comply with Part B of the Building Regulations in relation to fire safety

42. Parking spaces, disabled parking, cycle parking installed in accordance with the approved details prior to occupation
43. Public accessibility of areas within the building to be provided as specified
44. Compliance with fire statement
45. Accord with contaminated land assessment with validation report submitted prior to occupation
46. Development shall stop if unexpected contamination found, and appropriate remediation agreed, carried out and verified
47. Petrol and oil interceptors for car park
48. Accord with mitigation outlined in Noise Assessment
49. Noise from air and plant units should not increase background noise
50. Accord with submitted Residential Green Travel Plan
51. Meanwhile strategy, plan and implementation if required
52. In accordance with submitted energy strategy

Tower A specific

Pre-commencement

53. Final details of number and details of kitchen stations, booking systems for amenity areas within building and smart lift system, seating within corridors, seating areas at ends of east/west corridors, typical unit entrances

Pre-occupation

54. Details of cycle hire scheme for residents only, details of adaption to public use if required

Compliance

55. Minimum co-living floorspace - total amenity (3,016sqm), kitchen areas (1,361sqm), laundry (69sqm), gym and gym studio (198sqm), rooftop (556sqm)
56. Maximum co-living floorspace - co-working and maker space (240.9sqm), plant areas as specified in the application
57. Co-living facilities to be for use by residents only
58. Units CS:B1 and CS:B2 on each floor to be DDA compliant
59. Public café to be used as leasing/marketing suite for year after first occupation, following this shall revert to A3 use for lifetime of development

Tower B specific

Pre-occupation

60. Any extract systems for community use

Compliance

61. 10% of units M4(3) and 90% M4(2)
62. Community use retained as D1 use for lifetime of development
63. 110 litre/person/day water consumption target
64. Any other planning condition(s) considered necessary by the Director of Planning and Strategic Transport

Informatives

- 1) Granted subject to a Section 106 Agreement
- 2) Community Infrastructure Levy

- 3) Material/detailing conditions information
- 4) Code of practise for Construction Sites
- 5) Light pollution
- 6) Requirement for ultra-low NOx boilers
- 7) Thames Water - groundwater discharge and water pressure
- 8) Site notice removal
- 9) Environmental health
- 10) Network Rail construction and information
- 11) Any other informative(s) considered necessary by the Director of Planning and Strategic Transport

2.4 That the Committee confirms that it has had special regard to the desirability of preserving the settings of listed buildings and features of special architectural or historic interest as required by Section 66 of the Planning (Listed Buildings and Conservation Areas) Act 1990.

2.5 That the Committee confirms that it has paid special attention to the desirability of preserving or enhancing the character and appearance of the Chatsworth Road and Central Croydon Conservation Areas as required by Section 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990

2.6 That if by 27th May 2020 the legal agreement has not been completed, the Director of Planning and Strategic Transport is delegated authority to refuse planning permission.

3.0 PROPOSAL AND LOCATION DETAILS

Proposal

3.1 The application seeks permission for the following:

- Redevelopment of the former car park with the erection of a part 34 (Tower B), part 49 (Tower A) storey building
- Tower A comprising 817 co-living units for rent (Class Sui Generis)
- The co-living tower would contain 3,016sqm of co-living communal amenity space including 1,331sqm of communal catering facilities, 241sqm of co-working and maker space at second floor level and 556sqm of roof top garden amenity space and terrace on Floor 48. A café/reception for residents is proposed at ground floor, with offices and parcel room. A public café (Class A3) at ground floor level of 56sqm is also proposed
- Tower B comprising 120 residential units (Class C3) made up of 60 x 1 bedroom 2 person, 30 x 2 bedroom and 30 x 3 bedroom units
- The C3 tower would provide 100% affordable housing (120 London Shared Ownership flats) equating to 30.5% of the whole scheme by habitable room) with roof top garden amenity space for residents on Floor 33. Ground and first floor to include 316sqm of non-residential floor space (Class D1) and residential cycle store providing 180 spaces at second floor level.
- The provision of 15 blue badge disabled parking spaces at basement level (11 for Tower A and 4 for Tower B), accessed from the ramp on College Road.
- The provision of 283 cycle parking spaces for Tower A at basement level 1 accessed from the ramp on College Road and 180 spaces for Tower B located at second floor level with visitor spaces for both towers located at grade in the adjacent public realm.
- Public realm improvements, including the provision of a new colonnaded pedestrian and cycle link between East Croydon Station through to College Green.



3.2 The applicant proposes to construct the scheme as a modular building, meaning that each 'module' will be manufactured off site in a controlled manufacturing environment. The modular build method offers a wide array of unit types and incorporates a bespoke, contextual and crafted façade design. It would also allow the applicant to construct the

building within a substantially shorter timeframe than a conventional concrete frame building of this scale.

Site and Surroundings

- 3.3 The site is located on the southern side of College Road and was previously occupied by a car park at basement level which served the adjoining college. The land level within the site is below that of College Road. The site includes the access ramp to the east of the car park which serves the remaining College car park and servicing accommodation, the adjacent car park at Mondial House to the east and a Network Rail sub-station to the south east. The ramp also provides pedestrian access to the adjacent public car park (NCP/Fairfield Halls).



- 3.4 The surrounding area is mainly commercial in character, being occupied by offices, educational uses and the Fairfield Halls. 101 George Street, to the north of the site across College Road, is currently being redeveloped to provide a part 38/44 storey building with 546 residential units and flexible non-residential uses at ground floor. St Mathews House lies beyond to the north-west, also containing some residential accommodation. 102 George Street (Mondial House), located to the east of the site on the opposite side of the access ramp, is a 15 storey office building (with extant planning permission for the erection of a part 35, part 13, part 11 storey building comprising mixed residential, office and retail uses). Fairfield Halls, to the south east of the site, is currently finalising refurbishment, and there is a live planning application in that is being considered by officers for Fairfield Homes immediately to the south (more in planning history below).

Constraints

- 3.5 The site is within the Croydon Opportunity Area (Edge Area – covered by policy DM38.4) and Croydon Metropolitan Centre. The site is allocated (site 31) in the Croydon Local Plan (2018) for “*mixed use redevelopment comprising hotel and residential*”, with 159 homes projected to be provided. The justification for the option is as follows:

“The site is to be used to fund improvements to the remaining parts of Croydon College, who do not need the car park. Residential development will help meet the need for new homes in the borough. The site lies within Croydon Metropolitan Centre close to East Croydon station but outside of the Primary Shopping Area so is suitable for all town centre uses except retail.”

- 3.6 The site forms part of the Fairfield Masterplan area. A Primary Shopping Area lies immediately to the north extending west along George Street, which is a classified road. The site has excellent Public Transport Accessibility (PTAL 6B), being in close proximity to East Croydon Station (with West Croydon Station to the north-west) and numerous bus and tram links.

Planning History

- 3.7 There is a substantial amount of planning history on the site (and indeed surrounding sites), but the following applications are considered to be of most relevance:
- 3.8 06/00854/P - Erection of 29 storey building including 2 basement levels and plant areas at roof level providing a vocational college on the lower 10 floors, a fitness suite and plant area on 10th floor and 173 flats and a crèche on the upper floor; provision of associated parking in basement areas – Permission granted. This permission was not implemented and has now expired.
- 3.9 14/01603/P - Erection of part 16/38 storey building (plus basement and mezzanine levels) comprising 159 residential units, 225 bedroom hotel and restaurant (within use class A3); provision of associated amenity areas, landscaping and car/cycle parking and alterations/partial enclosure of access ramp – Planning permission granted. Works have commenced on site in pursuance of this permission.

College Green Hybrid - Land bounded by George Street, Park Lane, Barclay Road, and main London to Brighton Railway Line

- 3.10 16/00944/P - Outline planning permission for demolition and redevelopment to provide: flexible class A1 (shops) and/or class A2 (financial and professional services) and/or class A3 (food and drink); class B1 (business); class C1 (hotel); class C3 (dwelling houses); class D1 (non-residential institutions); class D2 (assembly or leisure); public realm and landscaping; and associated car and cycle parking, servicing, and access arrangements (with all matters reserved); and Full planning permission for demolition including multi-storey car park and Barclay Road Annexe; extensions and alterations to Fairfield Halls including class A3 (food and drink); erection of buildings for flexible class A1 (shops) and/or class A2 (financial and professional services) and/or class A3 (food and drink) and/or class D1 (non-residential institutions) and/or class D2 (assembly and leisure) and class C3 (dwelling houses); change of use of basement car park (part) to class D1 (non-residential institutions); public realm and landscaping; and associated car and cycle parking, servicing, and access arrangements – Permission granted

(NB. This was a hybrid planning application comprising full planning permission for Phase 1A and outline planning permission for Phase 1B, 2 and 3 with all matters reserved – the College Tower site formed part of the outline element)

Mondial House, 102 George Street

- 3.11 16/00180/P - Demolition of the existing office building; erection of a part 35, part 13, part 11 storey building comprising plus basement, to provide 220 flats, 1,787 sqm B1 office space, and 490 sqm A1 retail floor space with associated works – Permission granted. Not currently implemented.

101 George Street (Former Essex House)

- 3.12 17/04201/FUL – Redevelopment of the site to provide a part 38 and part 44 storey building with 546 residential flats, with the ground floor to incorporate a flexible space including retail (Class A1), cafe (Class A3), business space (Class B1) and gallery space (Class D1) uses with basement accommodating parking spaces, cycle storage and refuse storage, and associated hard and soft landscaping – Permission granted. Currently being implemented and nearing completion on site.

Land to the south east of Croydon College (directly adjoining the application site to the south)

- 3.13 19/04516/FUL - Erection of five buildings ranging in height from 7 to 29 storeys to provide 421 residential flats (Use Class C3), flexible commercial space at ground floor of Building A (Use Class A1/A2/A3) and Buildings C and E (A1/A2/A3 and/or B1/D1 or D2) together with associated cycle parking, public realm and landscaping, basement car parking, refuse storage, servicing and access arrangements – Under consideration, and not yet determined by the LPA

4.0 SUMMARY OF KEY REASONS FOR RECOMMENDATION

- The principle of the redevelopment of this site has already been established, bringing forward the regeneration of a vacant site.
- A part 49/34 storey tower comprising residential and co-living accommodation with commercial uses at ground floor is supported in principle and aligned with the desire for growth on the Croydon Opportunity Area and the site allocation.
- Co-living is an emerging residential product, the principle of which is supported by emerging New London Plan Policy H16.
- The provision of C3 residential is fully supported, all of which would be in the form of London Shared Ownership (equating to 30.5% by habitable room) which has been independently assessed as the maximum reasonable provision. .
- The proposal includes a policy compliant number of family units in the C3 tower.
- Against the backdrop of the 14/01603/P permission, a tall building is supported. The height and mass of the two towers has been assessed in relation to its impact from a wide range of viewpoints and found acceptable, including in relation to its impact on heritage assets.
- The design, appearance and detailed façade treatment of the development is of high quality as required for tall buildings.
- The provision of a new pedestrian colonnade linking East Croydon Station through to Fairfield Halls is fully supported.
- The living conditions of adjacent occupiers would be protected from undue harm subject to conditions.
- The living standards of future occupiers are satisfactory (in terms of overall residential quality) and the C3 homes would comply with the Nationally Described Space Standard (NDSS).
- The level of parking and impact upon highway safety and efficiency would be acceptable, subject to conditions and s.106 agreement.

- The environmental impacts, including wind, noise, air quality, land contamination and flooding, are acceptable subject to mitigation proposed through a combination of conditions and s.106 agreement.
- Sustainability aspects have been properly assessed and their delivery can be controlled through planning obligations and planning conditions.

5.0 CONSULTATION RESPONSE

5.1 The views of the Planning Service are expressed in the MATERIAL PLANNING CONSIDERATIONS section below.

5.2 The following organisations were consulted regarding the application:

The Greater London Authority (Statutory Consultee)

5.3 The GLA have made the following comments:

- The proposals do not demonstrate an adequate quality of accommodation for future occupants, and cannot be supported in their current form. This relates largely to the quantum of and access to the communal amenity spaces, in particular the kitchen facilities. Officers therefore consider that units would become self-contained as a result, where the incentive to use the communal spaces would be undermined by the quantity and ease of access.
- The provision of on-site conventional affordable housing is supported in principle, however the single intermediate affordable tenure is not acceptable. The viability is being scrutinised by the GLA to ensure the maximum amount is provided.
- The overall approach to urban design is supported.
- The applicant has provided a fire safety statement that demonstrates that all features and materials would comply with Part B of the Building Regulations, which should be appropriately secured in any decision.
- An increase in the total amount of long-stay cycle parking, with further details of the shared approach should be provided. Facilities to support deliveries by cargo bike should be considered. A contribution towards public transport capacity enhancements is also requested.
- Water and energy matters require resolution.

[OFFICER COMMENT: Continual engagement with the GLA has taken place throughout the pre-application and application process. As a result of discussions with the GLA during the application, the applicant has proposed the following amendments in order to address their concerns:

- An additional floor of communal cooking and dining facilities has been provided increasing the amount of overall communal space from 2,104sqm to 3,016sqm, reducing journey times to the kitchens and a reduction in the number of co-living units from 836 to 817. This increases the communal amenity space per person from 2.5 to 3.7sqm per person and the space devoted to catering from 567sqm to 1,331sqm.
- Further justification for the size and facilities within the other communal spaces including the laundry have been provided.
- We have had the viability independently appraised and are confident the 120 London Shared Ownership homes is the maximum reasonable.
- Further details of the bike hire scheme have been provided, and amended water and energy details.

- Conditions and s.106 obligations are recommended].

Transport for London (TfL) (Statutory Consultee)

5.4 TfL requests the following points are addressed for the application to comply with the transport policies of the London Plan:

- TfL support the improvements the applicant proposes to make to the surrounding highways network in their Active Travel Zone assessment.
- The public realm to the north and south of the site could be enhanced further to support the Healthy Streets agenda, for example providing seating.
- The amount of long stay cycle parking for the co-living units should be increased to accord with the minimum London Plan standards for C3 accommodation.
- A public transport contribution of £425,000 towards service capacity enhancements is requested.
- Details to be provided including for cycle parking, parking management plan, EVCPs, DSP and CLP

[OFFICER COMMENT: additional transport information has been provided in response to the comments made by TfL, including for the cycle hire scheme. The transport impacts are discussed in detail below, and conditions and s.106 obligations imposed. Note the £425,000 figure has been disputed by the applicant and officers are working with TfL to resolve the final amount to be secured]

Lead Local Flood Authority (LLFA) (Statutory Consultee)

5.5 The LLFA have no objection (further information was received to address these initial concerns) and is satisfied that a detailed sustainable drainage scheme can be suitably secured through a condition [OFFICER COMMENT: condition imposed]

Environment Agency (Statutory Consultee)

5.6 No response from the Environment Agency was received.

Historic England – Archaeology

5.7 The archaeology team at Historic England have confirmed that no further assessment or conditions are necessary with regards to archaeology.

Natural England

5.8 Natural England had no comments to make on the application.

Thames Water

5.9 With regards to foul water and surface water network infrastructure capacity, Thames Water raised no objection. An informative is recommended to advise the developer that Thames Water underground water assets are located within 15m of the development, and water mains crossing or close to the development. Thames Water have requested a condition be imposed, requiring the developer to liaise with them to discuss the impact on the existing water network infrastructure, and whether upgrades are required to accommodate the development.

[OFFICER COMMENT: informatives and conditions imposed]

Network Rail

- 5.10 Network Rail requests the developer contact their Asset Protection team to discuss and agree an Asset Protection Agreement with them for any work in the vicinity of Network Rail land and infrastructure. Their generic comments relate to future maintenance, drainage, plant and materials, scaffolding, piling, fencing, lighting, noise and vibration and vehicle incursion and the need to contact them prior to works commencing would be attached as an informative to any planning permission granted [OFFICER COMMENT: informative imposed].

Metropolitan Police Service – Designing Out Crime Officer

- 5.11 The Designing Out Crime Officer has raised an objection to the scheme. As a result further information and amendments to the scheme have been provided. Final detailed matters can be resolved through the imposition of a condition, requiring liaison with the MPS once an operator has been secured and prior to occupation. This particularly relates to the security arrangements for the building (including size of the security team)
[OFFICER COMMENT: condition imposed]

National Air Traffic Services (NATS) Safeguarding

- 5.12 NATS Safeguarding have objected. To overcome this objection that have requested a condition be imposed requiring the submission and implementation (once approved) of a radar mitigation scheme, to be discharged in consultation with them. This is to mitigate impact of potential reflections of radar signals from Heathrow Airport and avoid false detections being sent to air traffic controllers.
[OFFICER COMMENT: condition imposed]

Gatwick Airport Safeguarding

- 5.13 No objection was raised.

Mid Croydon Conservation Area Panel

- 5.14 The Panel consider the proposal will be detrimental to the Conservation Area due to:
- Cumulative visual impact with the proposed scheme on the adjacent site
 - Hideous and uninteresting architecture
 - Detrimental impact through wind turbulence
 - Concern with the concept of co-living
- [OFFICER COMMENT: the points raised are covered within the material considerations below]

London Fire Brigade

- 5.15 London Fire Brigade requested an undertaking that access for fire appliances in accordance with the current Building Regulations and adequate water supplies for fire-fighting purposes will be provided.
[OFFICER COMMENT: additional information has been provided, conditions and informatives attached]

6.0 LOCAL REPRESENTATION

6.1 The application has been publicised by way of 19 letters of notification to neighbouring properties in the vicinity of the application site. Site notices were also erected in the vicinity of the site and a press notice published. Two re-consultations in the same terms took place following submission of (firstly) an updated wind and microclimate report, and (secondly) revisions to the scheme including the addition of an extra floor of amenity space and a reduction in the number of units. The number of representations received from neighbours in response to notification and publicity of the application are as follows:

No of individual responses: Objecting: 1 Supporting: 0 Comment: 2

6.2 The following issues were raised in representations. Those that are material to the determination of the application, are addressed in substance in the MATERIAL PLANNING CONSIDERATIONS section of this report:

Comment	Officer comment
Overdevelopment of the site	Addressed in Sections 1 and 4 of this report
Obtrusive and likely to overshadow public realm and Croydon College	Addressed in Section 5 of this report. In terms of the impact on the public realm and College, these are not residential properties and are therefore not subject to the same tests for amenity. Notwithstanding this, the principle of a tall building in this location has been accepted, and it is not considered that the impact on either the College nor public realm would be significantly worsened by the changes to the scheme, when compared with the extant consent. Adequate daylight/sunlight testing of an acceptable scope has been carried out.
Adverse effect on air flow (wind) in this location	Addressed in Section 8 of this report
The adjacent Fairfield Homes scheme is referenced as the “competition” scheme which is incorrect	This is a typographical error
The adjacent Fairfield Homes scheme is not listed in the wind and microclimate report under B3 on page 31	This is a typographical error
The wind assessment does not test the impact on the stairs between the podium and Hazeldean Bridge or at the entrance or corner of Block A of the Fairfield Homes scheme	The testing carried out is considered to be sufficient and reasonable

Queries on submitted Design and Access Statement	These are addressed in the relevant paragraphs below. Amendments and additional information received.
Application does not adequately represent the Fairfield Homes scheme	The surrounding consented and proposed development is understood and the information provided is considered sufficient to understand the relationships between the buildings.

6.3 Cllr Scott has referred the application to Planning Committee for a decision, in his capacity as Vice-Chair, raising the following issues:

- Potential to meet housing need through the provision of new homes, responding to local, regional and national housing targets
- Proposed affordable housing provision of 100% shared ownership with no social rented accommodation in contrary to policy

7.0 RELEVANT PLANNING POLICIES AND GUIDANCE

7.1 In determining any planning application, the Council is required to have regard to the provisions of its Development Plan so far as is material to the application and to any other material considerations and the determination shall be made in accordance with the plan unless material considerations indicate otherwise. The Council's adopted Development Plan consists of the Consolidated London Plan 2015, the Croydon Local Plan 2018 and the South London Waste Plan 2012.

7.2 Government Guidance is contained in the National Planning Policy Framework (NPPF), issued in February 2019. The NPPF sets out a presumption in favour of sustainable development, requiring that development which accords with an up-to-date local plan should be approved without delay. The NPPF identifies a number of key issues for the delivery of sustainable development, those most relevant to this case are:

- Delivering a sufficient supply of homes
- Promoting sustainable transport;
- Achieving well designed places;

7.3 The main policy considerations raised by the application that the Committee are required to consider are:

7.4 Consolidated London Plan 2015

- 3.3 Increasing housing supply
- 3.4 Optimising housing potential
- 3.5 Quality and design of housing developments
- 3.8 Housing choice
- 3.9 Mixed and balanced communities
- 3.11 Affordable housing targets

- 3.12 Negotiating affordable housing on individual private residential and mixed use schemes
- 3.13 Affordable housing thresholds
- 5.1 Climate change mitigation
- 5.2 Minimising carbon dioxide emissions
- 5.3 Sustainable design and construction
- 5.12 Flood risk management
- 5.13 Sustainable drainage
- 5.16 Waste net self sufficiency
- 6.3 Assessing effects of development on transport capacity
- 6.9 Cycling
- 6.13 Parking
- 7.2 An inclusive environment
- 7.3 Designing out crime
- 7.4 Local character
- 7.6 Architecture
- 7.14 Improving air quality
- 7.19 Biodiversity and access to nature
- 7.21 Woodlands and trees

Emerging New London Plan

7.5 Whilst the emerging New London Plan is a material consideration, the weight afforded is down to the decision maker linked to the stage a plan has reached in its development. The Plan appears to be close to adoption. The Mayor's Intend to Publish version of the New London Plan is currently with the Secretary of State and no response had been submitted to the Mayor from the Secretary of State. Therefore, the New London Plan's weight has increased following on from the publication of the Panel Report and the London Mayor's publication of the Intend to Publish New London Plan. The policies of most relevance to this application are as follows:

- SD6 Town centres and high streets
- D4 Delivering good design
- D5 Inclusive Design
- D6 Housing quality and standards
- D7 Accessible housing
- D8 Public Realm
- D9 Tall Buildings
- D11 Safety, security and resilience to emergency
- D12 Fire safety
- H5 Threshold approach to applications
- H16 Large-scale purpose built shared living

7.6 Croydon Local Plan (CLP) 2018

- SP2 Homes
- SP6.3 Sustainable Design and Construction
- DM1 Housing choice for sustainable communities
- SP4 Urban Design and Local Character
- DM10 Design and character
- DM13 Refuse and recycling

- DM16 Promoting healthy communities
- SP6 Environment and Climate Change
- DM23 Development and construction
- DM25 Sustainable drainage systems and reducing floor risk
- SP7 Green Grid
- DM27 Biodiversity
- DM28 Trees
- SP8 Transport and communications
- DM29 Promoting sustainable travel and reducing congestion
- DM30 Car and cycle parking in new development

7.7 There is relevant Supplementary Planning Guidance as follows:

- London Housing SPG March 2016
- Homes for Londoners: Affordable Housing and Viability SPG, August 2017
- Croydon Opportunity Area Planning Framework (adopted by the Mayor and Croydon), 2013
- Fair Field Masterplan, 2013

8.0 MATERIAL PLANNING CONSIDERATIONS

8.1 The main planning issues raised by the application that the Planning Committee are required are as follows:

1. Principle of development
2. Housing quality for future occupiers
3. Affordable housing, mix and density
4. Townscape and visual impact
5. Residential amenity of neighbours
6. Parking and highway safety
7. Trees, landscaping and biodiversity
8. Environment
9. Sustainability
10. Other planning matters

Principle of Development

8.2 At the heart of the National Planning Framework 2012 (NPPF) is a presumption in favour of sustainable development which meets social, economic and environmental needs, and attaches great importance to significantly boosting the supply of new housing.

8.3 The site is located within the Croydon Opportunity Area (COA). The Opportunity Area Planning Framework (2013) encourages new homes, the revival of the high street, and improved streets and amenity spaces. The site is also within the Croydon Metropolitan Centre, where Local Plan policy SP3.10 sets out a flexible approach to office, housing and retail uses.

8.4 The site is allocated (site 31) in the CLP (2018) for *“mixed use redevelopment comprising hotel and residential”*, with a projected 159 homes on site. The justification for the option is as follows:

“The site is to be used to fund improvements to the remaining parts of Croydon College, who do not need the car park. Residential development will help meet the need for new homes in the borough. The site lies within Croydon Metropolitan Centre close to East Croydon station but outside of the Primary Shopping Area so is suitable for all town centre uses except retail.”

- 8.5 The site was formerly a car park in use by the College. Planning permission was granted in 2014 for a mixed use hotel/residential redevelopment, which this allocation relates to. As part of this former application the car park was considered ancillary to the educational use of the College and therefore a ‘community use’, protected by policy. Its loss to accommodate the development was justified at the time through a demonstration by the College that the car park was no longer required, and the sale of the car park would partly fund a planned expansion of the College’s further education provision.
- 8.6 Current Policy DM19.1 of the CLP (2018) protects community facilities. Ownership of the land has formally changed since the previous application, and development has commenced in pursuance of this planning permission and therefore the site as it sits today is not a car park associated with the College. Therefore no objection is raised in respect of policy DM19.1 of the CLP; the re-use of this existing underutilised site in a highly sustainable location can be supported.
- 8.7 In terms of housing numbers, the allocation suggests 159 homes (linked back to the 2014 permission). This scheme propose 120 C3 homes which is 39 short of that target. Whilst there is no currently adopted policy that sets out how co-living accommodation should count towards housing numbers, it is important to note that the emerging New London Plan suggests that units of non-self-contained communal accommodation (such as co-living) should be counted at a ratio of 1.8:1, when compared with traditional C3 units. On this basis the 817 co-living units would equate to 453 C3 residential homes. In the absence of a methodology other than that in the emerging New London Plan (where the weight afforded is down to the decision maker and equates to 453 homes) combined with the fact all 120 C3 homes to be delivered are affordable and the allocation is simply the previous consent, officers view is that the site allocation is met. The development comprises mixed uses but is clearly residential-led, with shared living proposed alongside traditional residential flats and would deliver a significant number of new dwellings in the town centre. The site is in a central location with excellent access to public transport, local shops and services and is therefore well placed for residential-led development of a higher density than was previously consented, and indeed for a large scale shared living scheme.
- 8.8 Whilst the Croydon Local Plan (2018) and current London Plan are silent on co-living accommodation, Policy H16 of the emerging New London Plan recognises the contribution co-living and shared living in general can make towards housing supply. Co-living is an emerging type of housing, which does not fall within a traditional residential use class, but is classed as sui-generis use. Whilst Local Plan policy SP2.7 seeks to ensure that a choice of homes is available in the Borough that will address the need for homes of different sizes, there is no specific policy reference to co-living. There are limited precedents for this housing typology in the UK, with only a small number of operators and there are currently no space standards for co-living accommodation.

8.9 For co-living schemes to be supported, emerging New London Plan policy H16 requires the following criteria to be met (these criteria, along with how the scheme complies with these, are discussed in more detail further on in the report):

- “1) it is of good quality and design*
- 2) it contributes towards mixed and inclusive neighbourhoods*
- 3) it is located in an area well-connected to local services and employment by walking, cycling and public transport, and its design does not contribute to car dependency*
- 4) it is under single management*
- 5) its units are all for rent with minimum tenancy lengths of no less than three months*
- 6) communal facilities and services are provided that are sufficient to meet the requirements of the intended number of residents and offer at least:*
 - a) convenient access to a communal kitchen*
 - b) outside communal amenity space (roof terrace and/or garden)*
 - c) internal communal amenity space (dining rooms, lounges)*
 - d) laundry and drying facilities*
 - e) a concierge*
 - f) bedding and linen changing and/or room cleaning services.*
- 7) the private units provide adequate functional living space and layout, and are not self-contained homes or capable of being used as self-contained homes*
- 8) a management plan is provided with the application*
- 9) it delivers a cash in lieu contribution towards conventional C3 affordable housing. Boroughs should seek this contribution for the provision of new C3 off-site affordable housing as either an:*
 - a) upfront cash in lieu payment to the local authority, or*
 - b) in perpetuity annual payment to the local authority”*

8.10 The site sits within Croydon Metropolitan Centre, outside of designated retail frontage. Therefore, in terms of the ground floor uses, the principle of a community use (Class D1) on the ground/first floor of Tower B is acceptable, providing a link into the adjacent cultural quarter. A public café (Class A3) and co-living reception and cafe in the ground floor of Tower A (Sui-generis use as part of the co-living accommodation) is also supported, activating the ground floor frontage of this key public route. The applicant proposes to temporarily utilise the public café space for a marketing and leasing suite for the first year following occupation of Tower A, which is considered acceptable provided the use reverts to an A3 use following this period (to be secured by condition).

8.11 Policy DM4.3 of the CLP (2018) seeks to prevent ground floor units outside main and secondary frontages remaining empty. Whilst specific end users have not yet been identified for the ground floor units, the applicant will be required to market the units and provide fitting out for the eventual end occupier to ensure the unit is capable of occupation and operation. This is to be secured within a s.106 agreement. Overall the proposed ground floor uses are proposed to complement and signal entry into the Cultural Quarter, with the incorporation of a community/cultural space on the corner.

8.12 Local Plan policy SP4.5 encourages tall buildings in the Opportunity Area, subject to compliance with the Local Plan’s detailed policies and the Opportunity Area Planning Framework. Being located within the Edge Area of the Opportunity Area, the site is covered by policy DM38.4 of the Croydon Local Plan (2018), where tall buildings can

be acceptable subject to achieving a high quality form, design and treatment and where negative impact on sensitive locations is limited.

- 8.13 The scheme is for a mixed use development of up to 49 storeys comprising predominantly co-living (sui-generis) and residential accommodation (C3 use), with commercial and community floorspace on lower floors. Further to detailed consideration below, in building height and use terms the proposal accords with the policies applicable to the site and the principle of a tall building on the site has been established through the grant of the extant planning permission (14/01603/P).
- 8.14 Taking into account the above matters, it is considered that the erection of a high density residential focussed development within a tall building, incorporating A3 and D1 uses on lower floors would be acceptable in principle, subject to compliance with the other policies including meeting the criteria of emerging New London Plan policy H16. The scheme would result in the re-use of an existing underutilised site with the delivery of a significant number of new dwellings, which is supported.

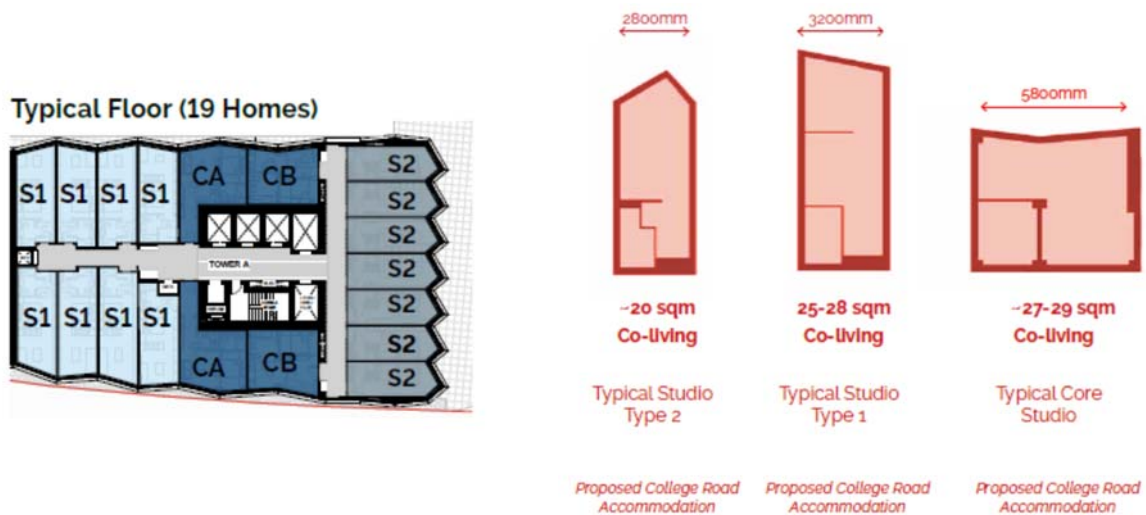
Housing Quality for Future Occupiers

Co-living – Tower A

- 8.15 Co-living is a sui-generis use and therefore not required to meet the minimum floorspace standards as required for traditional C3 homes. As noted earlier in the report, there are no specific standards for this type of housing product, in terms of the units themselves nor the amenity areas. Officers have referred to policy H16 of the emerging New London Plan for considering quality.

Unit sizes

- 8.16 Part 7 of this emerging policy states that whilst units must provide adequate functional living space and layout, they must also demonstrably not be self-contained homes nor be capable of being used as such – to remain distinct from traditional C3 residential accommodation.
- 8.17 Officers have worked extensively with the applicants to ensure the sizes and layouts of the co-living units are acceptable. From the outset the applicant has sought to deliver larger units, with research undertaken suggesting rooms within other operating co-living developments are too small, with rooms more comparable in terms of size and quality to student accommodation. For example rooms in developments operated by the Collective co-living are typically 11-16sqm. The applicant has also reviewed the Council's House in Multiple Occupation standards, which seeks a single bedroom of 10sqm. Comparison has also been made to the Nationally Described Space Standards minimum requirement for C3 residential studio unit of 37sqm and the ratio of person to area. The proposed layouts have sought to find an appropriate middle ground, with studio rooms ranging from 20sqm to 29sqm.



8.18 4 different types of unit size and layout would be available across each floor, maximising the choice for residents and enabling a more mixed community. Each unit would be fully furnished with integrated and adaptable storage, including a small kitchenette and en-suite bathroom. It is important to note that the kitchenettes would have limited cooking and food preparation facilities, ensuring the shared kitchen and dining facilities external to the unit are the primary amenities for residents (this is discussed further below).

8.19 Whilst there are no specific accessibility standards for co-living accommodation, there is level and lift access to all floors and provision has been made for two wheelchair accessible units per floor, which will be secured by condition.

8.20 The proposed units are considered to be of good quality and are functional in terms of size and layout, whilst remaining dependent on the communal facilities for primary living functions. Therefore officers are of the view the units are not self-contained homes nor are capable of being used as such.

Communal spaces

8.21 The second critical element is the amount, quality and arrangement of the shared communal areas in the scheme. There are no prescribed standards for the size or specification of communal facilities and services for co-living accommodation, nor for external amenity space. However emerging New London Plan policy H16 requires these to be sufficient to meet the requirements of the intended number of residents, and offer at least:

- a) Convenient access to a communal kitchen
- b) Outside communal amenity space (roof terrace and/or garden)
- c) Internal communal amenity space (dining rooms, lounges)
- d) Laundry and drying facilities
- e) A concierge
- f) Bedding and linen changing and/or room cleaning services

8.22 Officers have also assessed the above in terms of the likely success in enabling and encouraging social interaction between residents, which is a key part of this housing typology, and to mitigate for the smaller sizes of the co-living units (relative to C3).

8.23 Commonly, the approach to other co-living developments in operation (or those consented) is to offer amenity in the form of kitchens or lounges for residents on multiple floors throughout the building. The applicants have carried out extensive research into co-living and other shared housing typologies both in the UK and internationally, alongside undertaking dialogue with prospective operators. This research suggests that there are issues with maintenance and management of these dispersed spaces, particularly in larger buildings. Research suggests that such spaces are then often underutilised by residents, who then prefer to use the larger and maintained centralised spaces.

8.24 The applicants have provided a comparison of the amount of amenity space provided within other co-living developments and other housing typologies to justify the overall amount and level per person. This research into co-living as a typology was extensive, but is constrained by the limited existing examples of comparable high-rise developments. Officers requested that a behavioural specialist and/or social anthropologist be appointed as part of the design team to assist in justifying the success of this unique housing model. This was not achieved, but the applicants did carry out a literature review, including the historic study 'A History of Collective Living', which provides data and analysis which has informed the design in many aspects. Examination of the most comparable examples has taken place, e.g. Urby in Jersey City, a 69 storey shared living tower with 762 rooms and associated amenity space. This has mitigated to a certain extent the lack of behavioural analysis, but some reservations remain as to the adequacy of the amenity spaces provided, including the variety and arrangement of spaces in terms of supporting a diverse and mixed community in future, embedded in Croydon as a place. This is why it is critical to ensure that an updated management plan and full credentials of the future operator (once decided) are secured through the s.106 agreement for approval, to get a full understanding of the use and day to day functioning of the building.

8.25 The communal amenity is split into the following:

- Ground floor: Café and reception
- Mezzanine: Café, amenity balcony and snug
- First floor: Kitchens and club room
- Second floor: Gym, studio, laundry, maker space, co-working
- 46th floor: Kitchens
- 47th floor: Social room, lounge/cinema, library, kitchen, dining
- 48th level: Rooftop amenity and terraces

Communal spaces	Size (sqm)		
Café and reception	96.5sqm	Co-working	171.2sqm
Café and amenity balcony	114.5sqm	46 th floor kitchens	518.5sqm
Snug	28.5sqm	Social room	152sqm
First floor kitchens	260sqm	Lounge/cinema	137sqm
Club room	201sqm	Library	40sqm
Gym	149sqm	47 th floor kitchens/dining	88sqm
Gym studio	49sqm	47 th floor dining	52.5sqm
Laundry	69sqm	48 th level rooftop amenity	396sqm
Maker space	69.7sqm	48 th level rooftop terrace	160sqm
Total			3,016sqm

8.26 Firstly focussing on catering, the scheme would provide 1,361sqm of communal catering facilities with 170 communal cooking points, equating to 1.7sqm per person (note this does not include the kitchenette facilities within each room) in a variety of different dining options including large shared kitchens, a café and hireable banqueting rooms. The applicant has done a lot of work on the catering capacity of the scheme and it has been significantly increased during the course of the application. Whilst 20% of occupants are able to cook independently at any one time, this must be considered in the context of not all occupants needing to cook at the same time, occupiers eating out and the ability to use a kitchenette in their own rooms. The applicant has evidenced with calculations that there are enough covers to accommodate all residents in the catering spaces at peak times.

8.27 In terms of other spaces, the scheme provides essential facilities such as the laundry as well as a variety of other types of social spaces and communal facilities on the upper and lower floors, including a gym, co-working spaces, contemplation room, cinema and roof terrace. Circulation spaces and corridor widths on each floor have been maximised with seating adjacent to lifts and within corridors to encourage social interaction and windows provided where possible to provide natural light. Officers consider the capacity of community spaces to accommodate residents at peak times has been adequately evidenced.

8.28 To address the other requirements of emerging policy H16, residents would have access to outside communal amenity space in the form of a rooftop amenity and terrace area on level 48. Convenient access to communal kitchens has been demonstrated through analysis of lift capacities and maximum travel times for residents to catering facilities. The proposed number of units per core (19) would be high, but given that co-living would not be C3 accommodation (the standards set by the GLA - generally 8 per core would therefore not apply) and the development is considered acceptable in terms of fire risk (covered below). Floor to ceiling heights would be appropriate, providing adequate standards in terms of ventilation and overheating. A

concierge, along with bedding and room cleaning facilities are to be provided for residents, which is to be secured in the management plan.

8.29 Overall, in the absence of space or quality standards for this typology, officers are satisfied that the location, size and arrangement of the communal spaces strike the right balance between future occupier amenity and management/maintenance pressures for the future operator. Further to engagement with the GLA the applicant has provided an extra floor of shared amenity (45th floor); the development would now provide 3 sqm per person internal amenity space for occupiers (3.7sqm including external space). It is interesting to note that this is nearly 5 times the internal communal space per person in Urby in Jersey City, for example and similar levels of spaces to other comparable international examples. The quality of the co-living accommodation is considered acceptable, subject to compliance with the management plan.

Other H16 requirements

8.30 The scheme is of good quality and design as covered above and contributes to mixed and inclusive neighbourhoods. The site is located in an area well connected to local services and employment by walking, cycling and public transport and its design does not contribute to car dependency. The scheme would be under single management (secured through condition) and the co-living units would be for rent with minimum tenancy lengths of no less than 3 months (secured through s.106 agreement). A management plan was submitted with the application and agreement of operator details (and further detail on the operation of the accommodation) are required prior to occupation, included as a condition.

8.31 The overall internal amenity for the co-living units when combining both the floorspace of the units and the communal space averages out as 27.3sqm per person across the whole building (excluding roof amenity and circulation spaces). This compares favourably with the NDSS requirement; with the exception of 1 bedroom homes that demand 37sqm and 39sqm per person, this exceeds the space per person from 2 bedroom homes and larger (25sqm per person for a 1B2P and 20.3sqm for a 2B3P).

8.32 Whilst officers are satisfied that the accommodation provided would constitute co-living to an appropriate standard, given this is a new typology the applicant has provided layout plans to show how the floorplates could be converted to C3 residential flats. This is a matter which was raised by Members at pre-application stage. Whilst a conversion would require planning permission, the applicants have demonstrated that the layout could be changed into NDSS compliant C3 flats with relative ease, without having to re-configure or re-service the entire building. This added safeguard is supported.

C3 residential accommodation Tower B

8.33 All of the proposed new units would comply with the internal dimensions required by the NDSS and would have acceptable layouts and room sizes. The scheme has been designed to ensure accessibility and inclusivity, with level access and accessible lifts provided. The proposal would meet the requirement of 10% of units to be wheelchair accessible M4(3) and all others can meet the requirements of M4(2), which is to be secured by condition.

8.34 Whilst the flats would not have private amenity space, this could only reasonably be achieved with balconies and given the façade treatment and building height this would be challenging in both visual impact and amenity of the spaces terms. The units are

oversized to an appropriate degree to compensate for the lack of external space, which is considered an acceptable solution in this context.

- 8.35 External communal space and child playspace is limited to the roof garden at level 34, which would have a total area of 330sqm. It is not feasible to provide any other external space due to the limited site footprint and proposed modular construction.
- 8.36 The proposal generates a requirement for 225.6sqm of play-space, of which 113sqm should be for under 5's. This is proposed to be provided within the communal roof area as this is as close to the units as possible, and is the only feasible way this would be provided within a constrained tower site. A condition is recommended to control the specific details. This leaves a shortfall of 113sqm for children over the age of 5, which cannot be accommodated on site (given the type of play equipment which would be required, and allowing for 117sqm for communal amenity space). The applicant has agreed to provide a financial contribution, based on the costs of equipping an area of approximately 113sqm with suitable equipment and including an allowance for future maintenance. Given the site constraints, along with the proximity to the Fair Field which is envisaged to incorporate some play elements, it is considered this is an acceptable approach in this instance to make up for the shortfall of on-site play equipment.

Designing out crime

- 8.37 London Plan Policy 7.13 states that development proposals should contribute to the minimisation of potential risks, and development should include measures to design out crime that, in proportion to the risk, deter terrorism, assist in the detection of terrorist activity and help defer its effects. The Croydon SPD No. 3: Designing for Community Safety sets out guidance for minimising risk, including maximising natural surveillance; creating spaces which foster a sense of ownership; activity levels; and management and maintenance provisions.
- 8.38 Engagement with the Metropolitan Police Designing out Crime officers has taken place during the course of the application. In response to concerns raised, particularly arising from the co-living element, amended layouts and an updated management plan have been received, as well as a detailed response. Conditions are recommended to require the applicant to continue engagement with the Police prior to occupation and discuss Secured by Design accreditation.

Daylight/sunlight and outlook for future occupiers

- 8.39 A daylight and sunlight assessment has been provided to assess the living conditions of future occupiers. See Appendix 1 for Daylight and Sunlight BRE Guidance terms.

Tower A (co-living)

- 8.40 The entire floorspace of each co-living unit (excluding the bathrooms) were tested to understand daylight/sunlight penetration through to the rear of the rooms. As with the daylight/sunlight testing for adjoining occupiers, the assessment takes into account the varying future surrounds scenarios to appreciate the realistic standard of accommodation as surrounding development comes forward.
- 8.41 This is of particular importance given that there are north facing single aspect co-living homes adjacent to 101 George Street. Whilst the west facing units have generally unrestricted outlook over the College, access to daylight is challenging on the northern and eastern elevations on the lower floors (particularly the units on the north eastern

corner) which are closest to surrounding development (including the consented scheme for 102 George Street to the east). The results show that 83% of the co-living units would comply with BRE guidance for access to daylight and 60% of the units would meet the BRE targets for sunlight. Many of the units have a relatively deep footprint with a central kitchenette, limiting sunlight penetration into the inner section of the unit. However, the layouts are such that the living space and kitchenettes are located closest to the windows with the bedspaces further into the footprint, which is considered to mitigate this to some degree.

- 8.42 A small sample of communal areas were tested for access to daylight, given the dependence of the co-living units upon these spaces. Both rooms tested (the contemplation room and one of the breakout spaces) achieved good ADF results demonstrating they will be well lit, although it should be noted this is a small sample. The roof terrace would achieve at least 2 hours of sunlight on 21st March, meeting BRE sunlight targets for outdoor spaces.
- 8.43 It must be noted that co-living as a sui-generis housing typology is not strictly subject to the guidelines for access to daylight and sunlight as traditional C3 homes are. The units are defined as non-self-contained, and as such residents are not dependent on the unit for their sole living area and would utilise the communal internal and external areas as an extension to their home. The applicants have taken steps to design in larger and angled windows on the northern façade to maximise access to daylight for residents, in an already constrained area in a town centre location with dense surrounding development. Officers are satisfied with the quality and layout of the units in other respects. Taking a balanced view when noting the requirements of emerging New London Plan policy H16, and weighing the other merits of the scheme, as well as the 2014 permission, flexibility to be afforded to the BRE guidance and town centre location where high density development will inevitably impact on daylight and sunlight for future occupiers, this is considered acceptable.

Tower B (C3 accommodation)

- 8.44 In terms of the units within Tower B, the corner units on each floor are dual aspect with a southern outlook. The west facing flats have unrestricted outlook over the College, whilst the east facing 1b2p flats face the lower height of the consented scheme at 102 George Street. Therefore daylight/sunlight levels are less challenging than for the co-living accommodation, however the testing still identifies lower lighting for rooms on the eastern flank in particular on the lower floors. This is particularly apparent for the living/kitchen/dining space for the 1b2p unit, which has a relatively deep footprint and a north east facing window towards the tallest element of the 102 George Street scheme. Overall, 98% of rooms would meet BRE targets for daylight and 79% would achieve target levels of sunlight. This percentage is marginally lower (98% daylight and 73% sunlight) within a scenario where the consented Fairfield scheme is built, compared with the Fairfield scheme currently proposed. The roof terrace would achieve at least 2 hours of sunlight on 21st March, meeting BRE sunlight targets for outdoor space. BRE guidance does recognise that sunlight criteria cannot be fully achieved in flats due to orientation constraints and density. Given the very good levels of internal daylight through the development and the recognised constraints for developments such as this in achieving high internal sunlight levels, it is considered overall that the daylight and sunlight levels afforded to future occupiers of this development would be acceptable.

Affordable Housing, Mix and Density

Affordable housing

- 8.45 The CLP (2018) states that to deliver affordable Class C3 housing in the Borough on sites of ten or more dwellings, the Council will negotiate to achieve up to 50% affordable housing, subject to viability and will seek a 60:40 ratio between affordable rented homes and intermediate (including shared ownership) homes unless there is an agreement with a Registered Provider that a different tenure split is justified.
- 8.46 In terms of co-living, the above policy would not apply as the use class is sui-generis and not C3. The only policy covering co-living schemes (or purpose-built shared living) is policy H16 of the Emerging New London Plan, which requires co-living accommodation to deliver a cash in lieu contribution towards conventional C3 affordable housing. This is because C3 standards do not apply to co-living (there are no standards) and a requirement of registered providers is for homes to meet the national space standards, which they do not. The policy directs that this should be sought either as an upfront cash in lieu payment to the Local Planning Authority (LPA), or by way of an in perpetuity annual payment to the LPA. In both cases the contribution provided is expected to be the equivalent of 35% of the units (to be provided at a discount of 50% of the market rent). This envisages the scenario that a scheme is entirely for co-living.
- 8.47 A key benefit of this scheme is that traditional affordable residential accommodation can be delivered on site, as opposed to a cash in lieu payment towards off site delivery, which would be the case for a wholly co-living scheme. The 817 co-living units within Tower A cannot be secured as affordable housing. However the entirety of Tower B (120 units) would be provided as C3 affordable housing (intermediate tenure, in the form of London Shared Ownership), equating to 30.5% by habitable room across both towers. The application was subject to a viability appraisal, which was scrutinised independently for the LPA. This included scenario testing specific to the requirements for co-living set out in the draft policy H16 of the emerging New London Plan. The results of the appraisal review and scenario testing is that there would be a viability deficit, even with the 30.5% offered.
- 8.48 The proposed affordable housing offer is therefore considered the maximum reasonable, providing 120 shared ownership homes with a mix of sizes including family homes, alongside 817 homes for co-living, catering to a different need within the housing market suitable for this highly sustainable location. No additional affordable housing (in percentage or affordable rented terms) could be viably provided, and early stage and late stage review mechanisms are recommended for inclusion within the s.106 agreement to account for any potential uplift. Taking this into account, alongside the key public benefits from the scheme (including delivery of the pedestrian route through to Fair Field) the level of affordable housing is supported.

Mix of accommodation

- 8.49 Policy SP2.7 seeks to ensure that a choice of homes is available to address the borough's need for homes of different sizes and that this will be achieved by setting a strategic target for 30% of all new homes up to 2036 to have three or more bedrooms. Policy DM1.1 requires a minimum provision of homes designed with 3 or more bedrooms on sites of 10 or more dwellings. In central settings with high PTAL ratings, the requirement is 20% of units to have 3 bedrooms or more (a minimum of 5% in Retail Core Area of the Croydon Opportunity Area and 10% in 'New Town' and East Croydon as defined by the Opportunity Area Planning Framework).

- 8.50 Each typical floor comprises a 1b1p, 1b2p, 2b3p and a 3b4p unit, equating to 30 x 1 bedroom 1 person units, 30 x 1 bedroom 2 person units, 30 x 2 bedroom 3 person units and 30 x 3 bedroom 4 person units. Therefore 25% of the C3 residential units are for family occupation, which would meet the associated policy requirement.
- 8.51 As the co-living element is sui-generis Policy DM1.1 does not comply. The scheme would deliver a large number of high quality co-living units to be occupied by single persons. Overall, officers are satisfied with the unit mix provided within the scheme.

Density

- 8.52 Based on the public transport accessibility level (PTAL 6b) and the site's central characteristics, the London Plan density matrix suggests a residential density of between 650-1100 habitable rooms per hectare and 140-405 units per hectare for the application site.
- 8.53 The residential density of the proposal would be 5,885 habitable rooms per hectare which would far exceed the upper limit of the indicative range within the London Plan for a central area. It is important to note that the density matrix was designed for C3 residential and not co-living. Officers have taken the worst case and included each of the 817 co-living units as a habitable room in the density above. Excluding the co-living units results in a purely C3 density of 1,800 habitable rooms per ha or 600 units per hectare. Whilst this is still an exceedance of the guidelines, the site is within the centre of the Opportunity Area, where significant growth is expected to be accommodated and the supporting text of Policy 3.4 of the London Plan confirms that the density matrix should not be applied mechanistically. Critically, the site allocation of 159 homes would also have exceeded the density matrix. The London Plan Examination in Public: Panel Report (October 2019) confirms that about half of developments permitted since 2004 have been outside the matrix ranges, thereby casting doubt over its effectiveness. It goes on to state that enforcing a strict upper limit on density runs the risk of stymying otherwise acceptable development. The correct approach is to set density on the basis of local context.
- 8.54 The proposed development has been designed to deliver new homes within a building that responds to its local context, taking into account both the physical constraints of the site and its relationship with neighbouring properties and the nearby townscape. Furthermore, the Mayor's Housing SPG and emerging New London Plan removes the density matrix and requires higher density development to have greater scrutiny, endorsing design review panels as an appropriate method. As discussed above, the scheme was presented to our Place Review Panel.

Townscape and Visual Impact

Massing and townscape

- 8.55 The extant scheme comprised a part 16 part 38 storey tower. The proposed scheme would retain a similar stepped building form, but would increase the height of the taller element (Tower A) to 49 storeys and the lower element (Tower B) to 33 storeys. It is worthy of note that modular construction methods proposed allow depth efficiencies, resulting in each storey being shorter in height than a typical build-up. Therefore the literal increase in storeys is not so significant.



8.56 The building heights would step up from George Street with the increase in height to Tower A from the consented scheme. Whilst this was not envisaged within the Fair Field masterplan, this is considered to be beneficial in massing terms (particularly in long range views) to differentiate from the immediate neighbouring scheme to the north at 101 George Street (there would be a difference in 5 storeys). The height of Tower B would be visually level with Block A of the proposed Fairfield scheme. As such, the towers would sit within a medium to tall cluster at a gateway location into the cultural quarter.

8.57 Initial concerns were raised by officers regarding the additional height proposed to Tower B. This related to the potential for the additional massing to detract from the slenderness of the taller tower, and the potential for the scheme to coalesce with surrounding buildings (in both existing and emerging context). However, design work has taken place to incorporate differentiation between the two towers and balanced against the provision of affordable C3 housing, the massing is considered acceptable.

Elevational Design

8.58 The concept for the scheme is to create two conjoined towers that are related but different, through façade articulation, texture and materiality. The starting point for the design approach was to seek to draw upon and articulate a contemporary reinterpretation of the mid-century heritage of Croydon in its expression. Officers have worked extensively with the applicant to ensure this is articulated robustly on the façade both in form and materiality, and is distinct from surrounding design approaches to give the building its own identity within this cluster.

8.59 An angled, pleated undulation has been applied across both of the tower elevations and helps to provide visual slenderness, whilst referencing some of the forms found within mid-century buildings within Croydon such as Corinthian House and No.1 Croydon. This is successfully reinforced through the use of a texture applied to alternating facets of the angled elevation 'pleats'.



- 8.60 The proposal employs two design moves in order to differentiate the two towers, a concern during the pre-application process. Firstly, through the design of the fenestration openings, Tower A takes the approach of elongated windows which are exaggerated through a darker toned cladding. These become increasingly foreshortened to the base of the tower to provide greater elevational variety. On Tower B, these windows are squarer and read as punched openings, leaving greater areas of white cladding. This ensures that Tower B's façade reads as visually more solid than Tower A and avoids the mass merging into one, particularly when viewed from a distance. This is clear in east and west views which have been tested, where the towers appear as two conjoined forms. The second design move is the design of the textured treatment to the terracotta (proposed for the façade), which is in a finer grain on Tower A to further differentiate between the two towers. This has the potential to achieve this, however the exact articulation of this requires further development to ensure it is effective in strengthening the differentiation between the two forms. This will be controlled by condition.
- 8.61 The articulation of the top of the tower distinguishes it from adjacent buildings, in particular the crown of 101 George Street which has a close relationship to the site in mid and long range views. The proposal has begun to develop the detailed articulation of this with some success. However, the design will require additional development through condition to ensure the proportions and articulation of the crown further strengthen the design.
- 8.62 Similarly the articulation of the podium levels of accommodation are expressed reasonably clearly in this elevational treatment. However, as with the crown, officers feel there is a requirement for this to undergo further design refinement to ensure that it reads as clearly distinct from the two tower forms. Refining the termination of the

Tower A and B junction detail and the height of the cladding and glazing bands will need to form part of this further work, to be secured by condition.

Materiality

- 8.63 The predominant material for the building façade is terracotta, which is supported in principle as it is robust and can achieve a variety of finishes and forms. It also has the ability to incorporate texture, tone and finish as well as colour variety, and the design has begun to explore several means of taking advantage of these qualities, for example the proposed alternating fluted texture of the white terracotta on the façade pleats. Given this is the same material as is being utilised on the modular 101 George Street development to the north, it is critical that the terracotta used is different to ensure these adjoining schemes retain separate identities.
- 8.64 The predominant finishes are a matt white across the majority of the facades and glossy black within the reveals. This creates a distinct contrast in tones which in principle is supported. There remain some concerns that the contrast of these may be too stark, however, whilst work has begun on exploring the variety of finishes and detailed articulation of the façade forms, it is recommended to impose a detailed set of conditions to continue this work post-determination. The cladding will also need to work on both a micro and macro scale and so further work will include rigorous material testing and will be done in partnership with a ceramicist to ensure, particularly the matt white cladding, is not too flat in appearance.
- 8.65 The approach the proposal takes to delivering public art is divided in three ways; firstly it is to engage a ceramicist to develop the finishes of the cladding above the podium level; secondly, is through the lighting strategy; and thirdly, the applicant has committed to work with a ceramicist to develop a piece of public art in a ceramic material pallet for the colonnade. This includes the large areas of blank façade, the columns and the soffits. The applicant was directed to draw inspiration from the mid-century sculptures within Leon House by the artist William Mitchell.
- 8.66 The applicant has identified a ceramicist to work with already, who has experience with working on tall building facades (specifically Mapleton Crescent, a 27 storey building in Wandsworth). Officers are satisfied with this approach for the upper levels. The colonnade will require a more in-depth selection process given the large extent of area and huge potential to create a significant piece of public art along a key route into the emerging Cultural Quarter. Officers' strong preference is for the applicant to write a public art strategy and implementation plan before tendering to a selection of artists based on the criteria set. Officers request they are able to participate within this process and review final designs and samples of the selected artist. The lighting will have to work with and compliment all elements of the architectural expression and will be developed in collaboration with the emerging façade designs. Conditions are recommended accordingly.

Public realm

- 8.67 An absolutely critical element of the delivery of this scheme is the key portion of the level access pedestrian route from East Croydon Station through to College Green. This is identified within the Fair Field Master Plan and its implementation is crucial in providing a high quality, easy and legible link from the Station into the Cultural Quarter through to Fairfield Halls, the Fair Field (formerly College Green) and beyond through to Mid-Croydon and Park Hill Park. This has been considered by the applicants with officer input from initial pre-application discussions and throughout. This has included joint workshops which have taken place to ensure co-ordination of the applicant's

designs with the public realm design for the Cultural Quarter, including managing the land level change and interfaces between the sites, in particular 101 George Street to the north and Fairfield Homes site to the south, as well as the Fair Field competition scheme.



8.68 As part of these workshops the applicant has committed to continuing the work with the adjoining developments to ensure the delivered public realm is as coordinated and consistent as possible. In particular a commitment has been made to carrying through the emerging public realm designs of the Fair Field public realm scheme to achieve a consistent treatment and reinforce the route into the Cultural Quarter.

8.69 The route would be provided through a colonnade, as was proposed through the 2014 consent, hence establishing this as an acceptable approach. Work has focussed on making the route as wide and visually accessible as possible for a high quality legible route, and making this as safe and active as possible, rationalising the number and positioning of columns with a cylindrical shape to ease access through and improve legibility. The route has also been widened by removing one of the footpaths down the service ramp. Views have been tested along the route which are considered acceptable.

8.70 To support the functioning of the public route, active frontages are being implemented along the colonnade with a D1 use proposed at the southern end of the building as you enter into the Cultural Quarter. An A3 use is proposed to the northern end of the building, providing public access into the building. Further discussions regarding the specific end users of the units will be held during condition discharge to ensure they are appropriate. Public access to rooftop level once a year is further being secured through the Section 106 agreement to maximise public benefit. In addition, the entrance lobby and residents café are located to the centre of the colonnade for maximum activation along the whole route.

Heritage

8.71 The Planning (Listed Buildings and Conservation Areas) Act 1990 requires (at section 66) with respect to listed buildings, that special regard is paid to the desirability of

preserving the building or its setting or any features of special architectural or historic interest which it possess. With regard to conservation areas (at section 72), it requires special attention to be paid to the desirability of preserving or enhancing their character or appearance.

- 8.72 The NPPF places strong emphasis on the desirability of sustaining and enhancing the significance of heritage assets, and affords great weight to the asset's conservation. At paragraph 193 it states that:

“great weight should be given to the asset's conservation (and the more important the asset, the greater the weight should be)... irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm”

- 8.73 Any harm to a designated heritage asset, including from development within its setting requires “clear and convincing justification” (paragraph 194), with less than substantial harm weighed against the public benefits delivered by the proposed development (paragraph 196). The NPPF requires a balanced judgement to be made in regard to the effects of a development proposal on the significance of non-designated heritage assets (paragraph 197).
- 8.74 Policy DM18 of the Local Plan permits development affecting heritage assets where the significance of the asset is preserved or enhanced. Policy SP4 requires developments to respect and enhance heritage assets, and Policy DM15 permits tall buildings which relate positively to nearby heritage assets.
- 8.75 The setting of a building is defined as ‘the surroundings in which a heritage asset is experienced’ in the glossary to the NPPF. ‘It’s extent is not fixed and may change as the asset and its surrounding evolve. Elements of a setting may make a positive or negative contribution to the significance of an asset, may affect the ability to appreciate that significance of may be neutral.’
- 8.76 The site is not within a Conservation Area (CA) and there are no designated heritage assets either on or immediately adjacent to the site. The development will join a cluster of consented and emerging tall building but it will be visible in the setting of nearby heritage assets due to its height and form.
- 8.77 A heritage statement was submitted as part of the Design and Access Statement. This assesses the impacts of the proposal on a range of nearby heritage assets, accompanied by views. Officers have also undertaken their own assessment of the impacts and are of the view that there would be some less than substantial harm to heritage assets, assessed in more detail below.

Chatsworth Road CA

- 8.78 Chatsworth Road CA, to the south of the site, contains a substantial grouping of late Victorian and Edwardian houses, many of which are of high architectural quality and fifteen of which are Locally Listed. Whilst the additional height of both towers would be visible from the Conservation Area, this would be comparable to the extant scheme in terms of visual impact and would be seen against the backdrop of 101 George Street, and beyond the consented Fairfield scheme in the foreground. The view from the south would be of the most slender proportions of the towers, with the distinction between the heights of the two elements clearly visible. The Chatsworth Road CAAMP acknowledges that the CA is located in close proximity to the town centre with existing and proposed tall buildings visible in the longer views, which forms part of the setting

of the CA. Overall it is considered the harm to this heritage asset would be less than substantial.



Central Croydon CA

8.79 Central Croydon CA is the commercial and civic heart of Croydon. Its street layout is largely medieval in original and it retains much of its plan form and historic fabric. Key views are identified along its length including a locally designated long view of the NLA tower from George Street, where the Tower A would be visible. The additional height is considered to result in minimal additional visual impact on the designated view when compared to the extant scheme and in the context of the surrounding area and tall buildings. To the south, Queen's Gardens is also included in the CA. From here, the enlarged massing of the development is at its most apparent and coalesces to some extent with 101 George Street. The proposed increases the dominance of the development on the setting of the CA in this location. It is however viewed in combination with existing and consented tall buildings. The proposal is considered to cause less than substantial harm to the Central Croydon CA.

8.80 From Queen's Gardens and Park Lane, the development is viewed across the roofscape of Croydon College and Fairfield Halls which are locally listed buildings immediately outside the CA. Both locally listed buildings are significant pieces of mid-20th century architecture which form an important part of the development of Croydon at this time. The enlarged massing of the development similarly increases the dominance of the development on these locally listed buildings causing some modest harm to their significance.



8.81 The CA includes the 16th century Grade 1 listed Whitgift Almshouses which have outstanding national historical and architectural significance. As per the extant scheme, the development will be visible in views of this building from North End, as a momentary glimpse in the valley between the western gable and the former Alders building, albeit with minor increased prominence given the additional height to Tower A. Taking this into account, with the overall view of the roof form remaining unimpacted, the proposal is considered to cause less than substantial harm to the setting of the Whitgift Almshouses.

Croydon Minster

8.82 The scheme will be visible in views of the Grade I Listed Croydon Minster. This building has high historic interest and community value. Only the very top of the tower will be visible near to the north aisle of the church and without coalescing with 101 George Street. Whilst it will affect the skyline and setting, this will be a momentary glimpse of the building and the church tower silhouette in the foreground will remain the prominent feature. This impact is considered to result in less than substantial harm to the setting of the Minster.

Harm and Public Benefits

8.83 No direct harm to the fabric of any heritage assets would occur as a result of the proposal, however it would cause some less than substantial harm to the settings of the key heritage assets identified above.

8.84 The extant proposal is a benchmark for the level of harm caused to the heritage assets. A scheme within the massing parameters of this extant permission would result in less harm to the assets, but critically would not deliver the scheme's key benefits in terms of affordable housing and realising the Council's aspirations for a high quality pedestrian route through from East Croydon station into the Cultural Quarter. Officers are of the view that the benefits of the proposal could not be achieved, without that level of harm. Those benefits, accompanied by the minimisation of the accompanying harm, offer clear and convincing justification for the harm to heritage assets identified above.

8.85 Having concluded that the scheme gives rise to "less than substantial harm" it is necessary to weight that harm against the public benefits. As set out above, a reduced scheme would be less beneficial in terms of affordable housing delivery and provision

of high quality pedestrian route and public realm. Therefore, the public benefits weight against the scheme are as follows:

- The delivery of a significant quantum of housing in this highly sustainable location, contributing positively to the borough's housing stock
- A significant proportion of affordable housing
- Delivery of high quality public realm including the critical pedestrian link from George Street to Fair Field
- The opportunity to make optimal use of land which is currently derelict and underutilised

8.86 Officers are of the view that those public benefits would outweigh the harm caused to the various heritage assets. Officers are satisfied that the approach adopted by the applicant in terms of design, heritage and townscape is sound and can be supported.

8.87 The planning application site lies in an area of archaeological interest. Historic England have advised that no further archaeological investigations are needed.

Residential Amenity of Neighbours

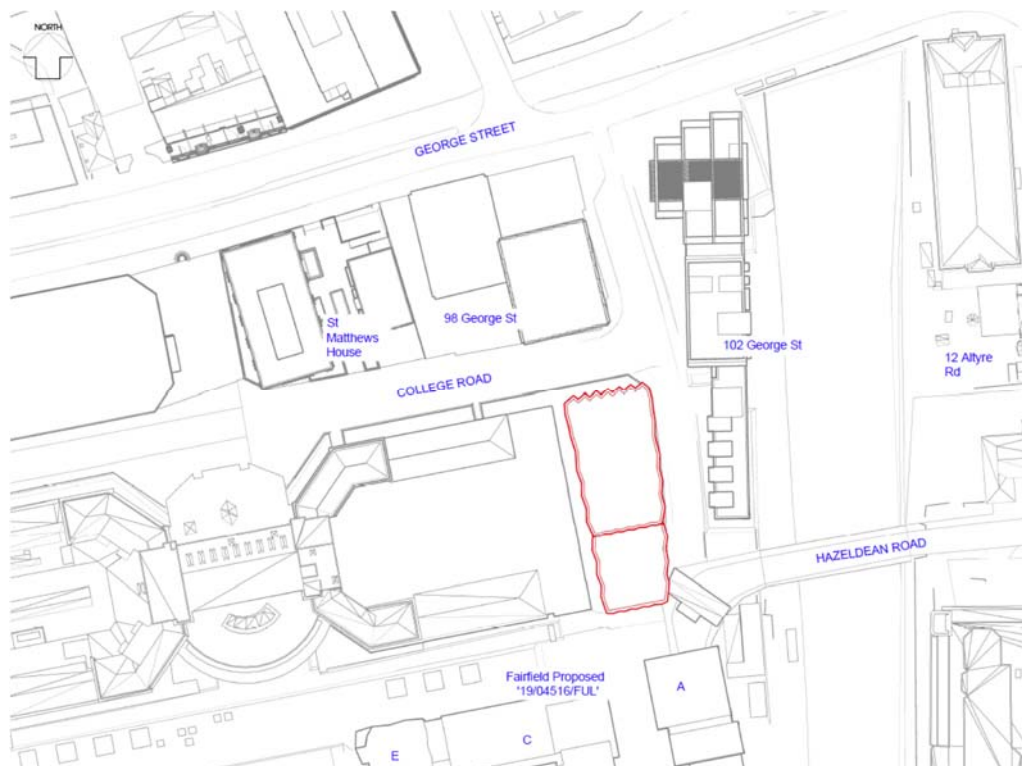
Outlook and privacy

8.88 In terms of outlook and privacy, the most critical relationships to consider are the adjoining occupiers at 101 George Street, 102 George Street and the consented and proposed Fairfield schemes. There would be approximately an 18m separation distance to 101 George Street, and direct overlooking would be avoided through the angled fenestration. Block A of the proposed Fairfield scheme would be approximately 13m from the southern elevation of Tower B. The adjacent balconies proposed within the consented scheme for 102 George Street would be approximately 12m from the flank elevation, at the closest point. Whilst these are relatively close distances, these are to be expected in high density town centre schemes and has already been established with the permission in place, including this site and the hybrid application to the south.

8.89 Overall, separation distances remain similar to the consented scheme (albeit the uses are different), and given the density of this cluster of developments in a town centre location it is expected that there will be a degree of mutual overlooking and visual impact for occupiers, so is acceptable.

Daylight and Sunlight Impacts

8.90 Policy DM10.6 states that the Council will not support development proposals which would have adverse effects on the amenities of adjoining or nearby properties or have an unacceptable impact on the surrounding area. This can include a loss of privacy, daylight, sunlight, outlook or an increased sense of enclosure. There are a number of buildings surrounding the site, along with a number of planning consents granted (and schemes coming forward) in close proximity.



- 8.91 A daylight and sunlight assessment has been provided with the application, assessing the development's impacts on existing and future residents. This in general has considered the impact on the neighbouring buildings from the extant planning permission as a fallback position (as suggested by BRE guidance, 0.8 times the former value for the permitted scheme). This is an acceptable approach, although overall standalone tests for Vertical Sky Component (VSC), Daylight Distribution (DD) and Annual Probable Sunlight Hours (APSH) have also been analysed, as set out below.
- 8.92 To clarify, guidance suggests that if the VSC percentage difference is less than 27% and less than 0.8 times its former value then there would be an adverse effect. However, the BRE guidance allows alternative target values; given the town centre location an 18% threshold guideline has been used.

101 George Street (Formerly Essex House)

- 8.93 At the time of the previous planning application, a residential proposal for the site immediately to the north was under consideration (planning reference 14/01594/P). Whilst planning permission was granted, this was not implemented and a later planning permission was granted for redevelopment (planning reference 17/04201/FUL) for a part 39 and part 44 storey tower comprising 546 residential units with commercial units at ground floor. This is nearing completion on site and thus consideration has been had to these future occupiers, where there are habitable rooms facing the site.
- 8.94 The VSC results show that out of the 684 windows assessed, 651 (95%) would fully comply with BRE standards when considering the consented scheme as a baseline. The remaining 5% constitutes 33 windows, but the difference between the consented and proposed VSC is within only 1% and almost all of these windows would achieve over 18% VSC. In terms of DD, 91% of the 432 rooms would meet BRE guidelines when compared with the extant scheme. Of the 38 rooms that fail, the margins are generally small and 29 are bedrooms where daylight is less important given the primary function of the room is to sleep. All rooms would achieve adequate sunlight levels in accordance with BRE guidance.

8.95 Taking all factors into account, it is considered the impact would be defined as minor adverse, taking into account the proportion of rooms affected. However in the context of the extant scheme having such a close relationship with the site, and the dense urban surrounds this impact is acceptable.

102 George Street

8.96 102 George Street is located to the east and north east of the site, and is currently occupied by an office block. A planning consent was granted in 2016 for redevelopment with a part 11-35 storey building providing 220 flats. The impact on daylight/sunlight has been tested for these units with outlook on the west and south facing elevations towards the site, which comprise a mix of bedrooms and living areas.

8.97 Those most affected in terms of daylight are west facing towards the site. 78% of rooms tested continue to comply with BRE targets for VSC, with the extant scheme as a fallback position. There are limited changes between the results for the extant and proposed schemes. 90% of rooms tested for DD meet the guidelines when compared against the consented scenario – of those failures (14 rooms), 9 are bedrooms (where daylight is less important) and the other 5 are marginal failures. For sunlight impact, generally the same rooms are most impacted upon on the west and southern elevation of the building (closest to the site). 81% of the windows meet the BRE targets for APSH. Out of these some would be poor and would likely experience a noticeable impact e.g. W13 serving a living room/kitchen/diner would lose 50% of its APSH, but this room is served by other windows.

8.98 Pass rates are generally 81-86% across all tests in comparison to the extant scheme, which would be as a result of the increased height of both towers. It is worthy of note that the planning permission for these units includes recessed balconies and projecting wings surrounding some windows as part of the design, which will already affect access to light within these units. It is considered the transgressions from the consented scheme would result in a minor adverse daylight/sunlight impact on the occupiers of these buildings but that these are within acceptable tolerances.

12 Altyre Road

8.99 12 Altyre Road is a residential property located on the other side of the railway lines approximately 95m to the east. In carrying out the assessment, the internal layouts are unknown, but all rooms with rear aspect have been assumed to be habitable as a worst case scenario. This is considered to be an acceptable approach. For the VSC test, 13 windows to the rear were analysed for daylight impact, and only 2 were compliant with BRE guidelines. W1 on the ground floor would achieve the lowest VSC value (6.51). However, these values were already low when compared against the consented scheme. Therefore although the impact would be worsened from the proposed scheme, the values are not significantly worse (generally around 1-1.5% worse) so this is considered to be a minor increased impact. In terms of daylight distribution, all windows would pass and this would not result in a change from the extant consent.

8.100 In terms of impact on sunlight, all windows would fail to meet BRE guidelines. However it should be noted that the failure is considered to be marginal, and this impact is not significantly worse when compared with the extant consent.

8.101 Overall it is considered this property will be the most affected, given the size of the dwelling and proportion of windows impacted for both daylight and sunlight which fail to meet BRE guidelines. Whilst this minor adverse impact from the additional height would be noticeable, it would only be marginally worse than that resulting from the

extant scheme coupled with the future surrounding consented/built development. This impact, when taking into consideration the town centre location, that the proposal is far enough away to avoid being unduly overbearing or causing privacy issues and the public benefits resulting from the scheme, is considered acceptable.

St Matthews House

8.102 To the north-west of the site on the northern side of College Road is St Matthew's House, a low rise building containing residential accommodation at first and second floor levels. This building is approximately 40-50m away to the north east. The residential units have windows in the eastern and southern flank walls. It is not known whether the rooms are habitable or non-habitable, but all have been assessed on the basis of being habitable as a worst case scenario.

8.103 For daylight impact considering VSC, 60% of the rooms would meet the targets set out in the BRE guidelines when compared with the extant scheme. Where the rooms would fail to meet the guidelines, the differences are generally marginal (around 0.3% difference). This is a similar scenario when considering DD, where 17 out of 19 rooms would meet BRE standards, with minor differences between the extant and proposed scheme. With regards to sunlight impact and testing APSH, all schemes would pass when compared with the extant scheme.

8.104 Overall, the adverse impact on daylight/sunlight, when compared with the formerly approved scheme and building surrounds, is considered to be marginal in the scope of the scheme. It should also be noted that the proposed scheme being built at 101 George Street, which sits directly adjacent to and projects beyond the rear of the affected windows, post-dates the extant consent and would have a more significant impact on this building than that proposed by the additional height of Tower A.

Fairfield Homes site to the south – consented and proposed

8.105 The impact on both the consented Fairfield scheme (16/00944/P) and the currently proposed Fairfield scheme (19/04516/FUL) has been tested for both daylight and sunlight impacts. Testing excluded blocks B and D, as these are furthest away from the development site and unlikely to be impacted upon, for e.g. Block B being set behind Block A which was tested. Similarly, not all floors have been tested – alternate floors have been tested which is common practice for tall buildings as it provides a representative sample of the effects without the requirement to model and analyse every room. The testing carried out is considered to be sufficient and an appropriate approach.

8.106 The consented Fairfield Scheme could form part of the future surrounds for development in this area. Across blocks A-C within the consented scheme, an average of 90% of windows tested would meet BRE targets for daylight (block B being the most affected – but still with 83% of windows meeting the guidelines). The two windows tested for sunlight levels (in block C) would achieve BRE targets.

8.107 For the proposed Fairfield scheme, 88% of windows would meet BRE target values for VSC. Those windows most effected would be closest to the site. For those falling short, it should be noted that the proposed results for block A would all achieve over the 18% urban target for VSC and those results for block C would constitute a minor difference to those reported for the consented scheme. Across blocks A-C, 98% of rooms tested would meet BRE guidance (3 rooms fail to pass) when considering the consented scheme as a benchmark, and all windows tested (taking into account orientation) would meet guidelines for sunlight.

8.108 Overall whilst there are some minor reductions when compared against the extant scheme in terms of BRE targets, it is considered either scheme, if built out, would achieve adequate levels of daylight and sunlight.

Daylight and sunlight conclusions

8.109 Whilst the proposed development would result in some daylight and sunlight impacts for surrounding properties, in the vast majority of instances where impacts beyond BRE guidelines occur, these are only minor in nature and where these impacts occur, good levels of daylight and sunlight are generally still maintained, especially considering the central location of the affected properties. It should be noted that daylight and sunlight impacts for surrounding properties beyond BRE guidelines are inevitable in an urban context such as this, which is why notable weight should be given to the assessment of the proposed development against the alternative baseline and target. The summary of the results given above confirms that a notable proportion of the impacts which occur are as a result of the most recent extant planning permission (the 2014 consent) and given the notable additional benefits proposed by this scheme (in particular the considerably higher proportion of affordable housing and delivery of a more generous public route) the harm of these additional impacts are considered to be outweighed by these additional benefits. As such the daylight and sunlight implications of the proposed development for surrounding properties are acceptable.

Microclimate

8.110 Paragraph 6.71 of the Croydon OAPF states that new buildings, in particular tall buildings, will need to demonstrate how they successfully mitigate impacts from micro-climate conditions on new and existing amenity spaces. In particular, new tall buildings in the COA will need to show how their designs do not have a negative impact on wind (downdrafts and wind tunnelling).

8.111 A wind tunnel assessment of the impact on the local microclimate has been undertaken. Given the number of consented and/or proposed developments in the vicinity of the site (including the extant consent on the site itself) and to fully understand the implications of the scheme in conjunction with all surrounding built form, the wind testing covers a number of different scenarios.

8.112 Modelling of the existing site identified no safety issues in terms of wind, and found that the environment was generally suitable for existing pedestrian uses. Testing of the proposed scheme within both the existing and proposed surrounds without any provision for wind mitigation identified a number of safety and comfort issues, in particular at the southeast corner of the development within the colonnade and at the northeast corner and upwards towards 101 George Street. Discussions on the appropriate level, design and location of mitigation to combat has taken place during the course of the application. The current scheme proposes mitigation in the form of planters surrounding the building, incorporating trees of varying heights. The planters at the southern end of the building are flush with the deck level, to maximise the openness of the pedestrian route through from the colonnade into Fair Field. Whilst the trees in terms of number and height would be sizeable in this area of the public realm, wind conditions within and immediately surrounding the site would meet the safety criteria (including in the critical southeast corner) and would in general achieve acceptable levels of comfort for pedestrian access to and passage through the site and onto George Street. Some localised areas would be marginally windier in the winter months, but where these occur the exceedance is relatively minor and so would be at least tolerable for proposed pedestrian usage. The proposed rooftop terraces would

provide some space suitable for sitting, with full details of the arrangement of the space (e.g. finalising the layout and potential to create more sheltered areas), to be agreed.

8.113 It is considered the design of the wind mitigation would ensure a sufficiently comfortable environment within the surrounds of the development. Implementation and maintenance of the wind mitigation is to be secured through the s.106 agreement, as well as the detailed design of the tree pits and their management to ensure establishment of and longevity of the planting to perform its function within the wider public realm.

8.114 Joint workshops are taking place to ensure co-ordination of the applicant's designs with the public realm design for the Cultural Quarter and the Fair Field competition scheme. As part of these workshops the applicant has committed to continuing the work with the adjoining developments to ensure the delivered public realm is as coordinated and consistent as possible; this includes continued design development of the wind mitigation and soft landscaping which will be secured through the s.106 agreement.

Highway Safety, Access and Parking

8.115 The site has a Public Transport Accessibility Level (PTAL) of 6b (on a scale of 0 – 6b, where 6b is the most accessible). The site therefore has an excellent level of accessibility to public transport links.

Car parking

8.116 The proposal is predominantly car-free, with the exception of the 15 dedicated disabled bays (11 for Tower A and 4 for Tower B) proposed in a lower basement level which form the extent of the total parking provision. This level of provision (3% of dwellings) is considered to be acceptable and would provide a satisfactory level for the wheelchair accessible units proposed.

8.117 The structural columns required for the towers mean some spaces are partially obstructed which is not ideal, however this was similarly the case for the extant planning consent and this has been designed out as much as possible. It is noted the emerging New London Plan requires a demonstration of disabled spaces for a further 7% of dwellings, in case these are needed at a later date. This further provision cannot be achieved on site given the small site area, and any further excavation to provide additional sub-basements (if this was feasible) would impact significantly on scheme viability. In any case, taking into account that the majority of units are for co-living, it is not considered further parking provision is necessary in such an accessible location. Resident's eligibility for parking permits would be restricted by the s.106 agreement and a Car Park Management Plan relating to the allocation of spaces would be secured through planning condition.

8.118 The parking spaces would be accessed via the ramp from College Road (extending partially under the College) similarly to the extant scheme. Sections have been provided to show that access can be achieved for the largest necessary vehicles under the sunken planters forming part of the public realm. Part of the existing ramp (approximately a footpath width) is proposed to be incorporated into the colonnaded public realm, in order to increase capacity for pedestrians on such a key route and maximise openness of the colonnade. The applicants have demonstrated this would not have a significant impact on pedestrian or vehicular safety, with access critically being retained into the adjacent Fairfield Homes site (19/04516/FUL) and the UKPN

power station to the south east. Detailed arrangements for the access and ramp are to be agreed as part of the s.278 works and public realm works to be secured within the s.106 agreement.

- 8.119 Local Plan Policy DM30 states that 20% of parking bays should have EVCP with future provision available for the other bays. The applicants have agreed to this in principle, with full details and provision of the EVCP to be secured by conditions.

Cycle parking

- 8.120 For the co-living accommodation, the applicant has applied the principle of shared living to the cycling facilities. 283 cycle parking spaces are to be provided within the upper basement for co-living residents of Tower A (equating to around 1 per 3 residents), with 130 of these to be for cycle hire spaces. The applicant has undertaken initial discussions with companies who provide shared cycling facilities for residential schemes who indicate this is feasible, and have analysed likely length, timings and frequency of rentals. Taking into account the limitations of the site area, the type of accommodation and the proximity of the site to public transport, it is considered this proportion of cycle parking for Tower A is appropriate. It is important that this hire scheme is only for residents, which will be secured by condition along with the full details once agreed, including provision for retaining the hire scheme but adapted for public use if necessary in future.

- 8.121 180 cycle parking spaces for Tower B would be provided at second floor level. Whilst not ideal, this is the largest area available on this tight site (allowing for plant and refuse storage in the basement) and a separate cycle lift has been provided from the basement up to the cycle store. The spaces provided would be in accordance with the adopted London Plan standards, which is considered acceptable and the maximum that could be accommodated on the site. Short stay visitor parking for the commercial uses is to be provided within the public realm area, along with staff cycle parking and changing facilities within the ground floor of Tower A. This is considered acceptable and would be secured by condition.

Car club

- 8.122 Policy DM30 of the Croydon Local Plan (2018) requires 5% of the total number of spaces to be provided as on-site car club spaces, with additional spaces at a rate of 1 space for every 20 spaces below the maximum overall number of car parking spaces as set out in the London Plan. In this particular case, it is considered more beneficial to secure improvements to existing car club spaces within the basement car park adjacent to the site (considered to fulfil current demand and which would still be easily accessible for residents). A financial contribution is to be secured through the s.106 agreement for 4 electric vehicle charging points given the number of units proposed, signing and lining and a signal booster, as well as car club membership being paid for the occupiers of the units, normally for 3 years. This is considered to be in accordance with the intentions of the policy, contributing to sustainable transport infrastructure.

Delivery and servicing

- 8.123 A Delivery and Servicing plan has been provided, stating all deliveries and servicing are to take place from the basement. Officers have carefully considered the likely transport and access impacts specific to a mixed co-living and residential scheme of this size, with public uses on the ground floors. For example, a high demand for deliveries and servicing in and around the building from vehicles, visitor cyclists and pedestrians as a result of a high number of residents. Pedestrian comfort level analysis has been undertaken taking account of full use and occupation of all the immediate

surrounding schemes. Given that there are limited comparative UK examples of co-living on this scale, it is considered necessary to require a bond to be secured against the projected delivery and servicing movements anticipated by the applicant. This will be monitored by the Council for a year, and can be reimbursed following expiry of this period if the projections are in line with what was envisaged. This is to ensure no adverse impacts on the local highways network from movements resulting from the co-living use, particularly given the cumulative number of developments and existing uses accessing from College Road. This is to be secured within the s.106 agreement.

Construction Logistics

- 8.124 Given the scale of the development, a condition requiring the submission of a detailed Construction Logistics Plan is imposed to ensure that the construction phase of development does not result in undue impacts upon the surrounding highway network. This is of particular importance given that there are a number of developments consented or proposed surrounding the site, and site logistics and build programmes will therefore need to be co-operative between developers to manage the potential for multiple schemes to be delivered simultaneously.

Refuse collection and storage

- 8.125 The proposal includes bin storage in the basements of both towers, with a refuse collection bay and access points accessed from the ramp. Refuse is proposed to be collected 3 times a week for Tower A, and once a week for Tower B. Tower A would utilise a bin chute linking to the basement; full details of how this will be managed and maintained successfully (i.e. for residents with accessibility issues) along with detailed arrangements are to be required by condition, prior to occupation. This will also feed into the co-living management plan and tenancy guide to be given to residents on arrival, which will also be assessed prior to occupation.

Pedestrian flows

- 8.126 A Pedestrian Comfort Level (PCL) assessment has been carried out for the proposed walkway, to assess the cumulative impacts of the development (and the proposed Fairfield scheme) on the public space around the base of the development, in particular the colonnade. In accordance with guidance produced by TfL, this confirms that the 3.53m footway width along the colonnade would provide an acceptable level of comfort for pedestrians at peak times generated from both the proposed development and the Fairfield scheme.

Sustainable transport

- 8.127 Given that the development would be car-free (aside from disabled spaces) and taking into account the nature of the development, increased walking, cycling and public transport use is expected. The impact of additional development within the Croydon Opportunity Area, including the proposed development, is expected to require upgrades to existing services and therefore a sustainable transport contribution is to be secured in the s.106 agreement to mitigate the impacts of the development and secure improvements to include highway, tram or bus infrastructure.
- 8.128 The applicants have proposed upgrades to the local highways network to support the forthcoming development, including increased street lighting between Fairfield Path and The Avenue, improved signage and lighting at Park Lane/George Street crossing and improving signage across George Street and around the tram lines. Financial contributions to transport improvements including improving the town centre cycle network in the immediate vicinity of the site are also to be secured in the s.106 agreement.

Travel Plan

- 8.129 In order to ensure that the identified modal shift is adequately supported, and barriers to uptake of more sustainable transport modes can be addressed, a Travel Plan and monitoring for three years is to be secured through the s.106 agreement.

Environmental impact and sustainability

Flooding and drainage

- 8.130 The site is within Flood Zone 1 (low risk) and an area of surface water flood risk. There is limited potential for groundwater flooding to occur. The applicant has provided a Flood Risk Assessment and Drainage Strategy. This sets out a strategy for managing runoff from the various parts of the site using an attenuation tank and hydrobrake control. There are no external areas which could be used for SuDS as the building covers the entirety of the site, excluding hardstanding of road areas.
- 8.131 The Lead Local Flood Authority assessed the proposed scheme and following submission of additional information raise no objection. This is subject to a condition securing detailed design.
- 8.132 With regards to foul water and surface water network infrastructure capacity, Thames Water raised no objection. An informative is recommended to advise the developer that Thames Water underground water assets are located within 15m of the development, and water mains crossing or close to the development. Thames Water have requested a condition be imposed, requiring the developer to liaise with them to discuss the impact on the existing water network infrastructure, and whether upgrades are required to accommodate the development. This is included within the recommendation.

Contamination

- 8.133 The submitted contaminated land report concluded that there were previous potentially contaminative onsite land uses including a railway line, gravel pit, workshops and warehouses. Ground investigation works have been carried out and a remediation strategy for contamination proposed. A condition is recommended to ensure the development is carried out in accordance with the submitted documents, and require submission of a validation report detailing evidence of remedial works carried out.

Air quality

- 8.134 The entire borough of Croydon is an Air Quality Management Area (AQMA) and therefore careful consideration to the air quality impacts of proposed development is required. The submitted air quality assessment demonstrates that the development will be better than air quality neutral and thus compliant with policy 7.14 of the London Plan. Mitigation measures to minimise dust emissions during construction works are proposed, to ensure that any residual effects will be 'not significant'. Emissions from the generated traffic are considered to have a negligible impact on the local road network. Emissions from the proposed Boiler Plant will lead to impacts at the south-facing properties on levels 36 and 37 and one east-facing property on level 36 in Tower A, however this will be mitigated with mechanical ventilation to prevent impacts at these properties. Residual effects of road traffic emissions are not envisaged to be significant. A contribution of £19,380 towards air quality improvements to mitigate against these impacts will be secured via the s.106 agreement, with recommended conditions.

Construction Impacts

- 8.135 A Construction Environmental Management Plan is to be secured by a condition, to ensure adequate control of noise, dust and pollution from construction and demolition activities, and to minimise highway impacts during the construction phase.

Ventilation

- 8.136 Prior to use of any food and drink uses (including the kitchen and dining areas within the co-living accommodation) commencing on site, details of ventilation will be required by planning condition.

Light pollution

- 8.137 To avoid excessive light pollution, a condition is recommended requiring details of external lighting, including details of how it would minimise light pollution.

Sustainable design

Carbon emissions

- 8.138 Policy SP6.3 requires new development to minimise carbon dioxide emissions and seeks high standards of design and construction in terms of sustainability in accordance with local and national carbon dioxide reduction targets. This requires new build, non-residential development of 1000sqm and above to achieve a minimum of 35% CO₂ reduction beyond the Building Regulations Part L (2013), and new build residential development over 10 units to achieve the London Plan requirements or National Technical Standards (2015) for energy performance (whichever is higher).
- 8.139 The policy also requires the development to incorporate a site wide communal heating system and to be enabled for district energy connection.
- 8.140 The applicant has provided justification that Tower A should be assessed against the non-domestic non-residential targets for carbon emissions. This is largely on the basis that the use class for co-living accommodation is sui-generis and the units are not self-contained, being dependent on the spaces external to the unit e.g. kitchen and dining areas. Officers consider this to be a reasonable assumption, with the traditional residential accommodation within Tower B assessed against the standard domestic methodology.
- 8.141 Overall, across the whole development, a reduction in regulated CO₂ emissions of 40% over current Part L Building Regulations (2013) is expected to be achieved. The remaining regulated CO₂ emissions shortfall would be covered by a carbon offset payment which would be secured through the s.106 agreement.
- 8.142 Whilst no existing district heating networks currently exist, the site is within an area where one is planned. Consideration was given to the use of a CHP (Combined Heat and Power) system but this was discounted in favour of alternative low carbon solutions including an air source heat pump and a low emissions gas-fired boiler. However, such a system would not be compatible with a District Heating System which is planned for the future. Drawings of the plant room have been provided showing how the building would be designed to connect into a future network, including space for the future substation and ducts for incoming pipes. A s.106 obligation is also recommended requiring connection to the District Heating System if the Council has appointed an operator before commencement on site, or a feasibility into connection to a future system on first replacement of the heating plant. On this basis, as the proposal

complies with the above requirements regarding carbon reduction and a CO2 offset payment, subject to a condition requiring the above standards to be achieved, the proposal is considered acceptable.

- 8.143 Both blocks can achieve the on-site carbon dioxide reductions as required by policy. Sustainable design and construction measures have been designed in where feasible, including measures to address overheating within the units. These matters are to be secured by condition.

Water use

- 8.144 Policy SP6.3 requires all new build residential development to meet a minimum water efficiency standard of 110 litres/person/day as set out in Building Regulations Part G. A planning condition is recommended to secure compliance with this target to ensure sustainable use of resources in Tower B. There is no policy requirement relating to water efficiency standards in non-domestic buildings such as Tower A.

Other planning issues

- 8.145 A health impact assessment was submitted which identifies that the proposal will have a positive impact on the majority of health factors including provision of homes, improving connectivity to the public realm and local services, and would have a neutral impact in terms of highway safety, construction impact and noise. Planning obligations and conditions are recommended restricting car use and securing highway improvements, noise and disturbance from commercial units and construction, along with other measures to avoid unacceptable health impacts. The development is liable for a Community Infrastructure Levy (CIL) payment to ensure that development contributes to meeting the need for physical and social infrastructure, including educational and healthcare facilities.
- 8.146 Although fire safety is predominantly a building regulations issue, draft policy D12 of the emerging New London Plan requires developments to achieve the highest standards of fire safety for all building users. The policy sets out a number of requirements, with the submission of a Fire Statement (an independent fire strategy produced by a third party suitably qualified assessor) setting out how the development has been designed and will function to minimise fire risk. Both buildings will be fully sprinklered (including the co-living amenity spaces), have comprehensive fire detection and alarm systems and incorporate features such as a 120-minute fire-rated enclosed stair and fire-fighting lift. As a managed building, Tower A will be subject to a fire evacuation strategy with a designated assembly point, details of which will be provided to residents on agreement of tenancy. It is considered the submitted details are sufficient to address, at this stage, the development's fire safety implications from a planning perspective. The development should comply with the fire statement as an approved document, with a condition recommended to secure full details of fire safety measures once the next stage of design work is complete, including materials and construction methods, evacuation points and any requirements incorporated as a result of discussions with the London Fire Brigade (e.g. specific locations for fire appliance access points and wet riser outlet locations). Furthermore, the GLA confirmed the Fire Statement was acceptable and should be secured by condition.
- 8.147 An EIA Screening Opinion was issued prior to the submission of the planning application. Consistent with the former planning permission, the development was not

considered to require an EIA, taking account of its location, nature, scale and characteristics.

- 8.148 A TV and Radio signal impact assessment was submitted, which identified the potential for minor short-term localised interference to satellite television users adjacent to the site, within 306m to the immediate north-northwest of the site. This could be mitigated by repositioned satellite dishes, to be secured by the s.106 agreement.
- 8.149 In order to ensure that the benefits of the proposed development (including those required to mitigate the harm caused) reach local residents who may be impacted indirectly or directly by the proposal's impacts, a skills, training and employment strategy (both operational and construction phases) and a contribution towards training are to be secured through the s106 agreement.
- 8.150 Emerging New London Plan policy D9 states that tall buildings, including their construction, should not interfere with aviation, navigation or telecommunication. NATS Safeguarding have requested a condition be imposed requiring the submission and implementation (once approved) of a radar mitigation scheme, to be discharged in consultation with them. This is to mitigate impact of potential reflections of radar signals from Heathrow Airport and avoid false detections being sent to air traffic controllers and forms part of the recommendation.

Conclusions

- 8.151 The proposed development would introduce a significant amount of new housing, including a mix of uses with co-living accommodation, affordable residential units and active ground floor uses linking into the Cultural Quarter. The proposed development would be well designed and deliver a key piece of public realm, making use of an existing underutilised site. There would be a good standard of accommodation for new residents, with an acceptable level of impact on neighbours. There would be harm to heritage assets, but that harm is considered to be minimised and necessary to deliver the development's benefits (and therefore is justified), and the harm caused would be outweighed by the development's public benefits. With conditions and mitigation, the proposal would be sustainable and acceptable in terms of its impact on the highway network. Residual planning impacts would be adequately mitigated by the recommended s.106 obligations and planning conditions.
- 8.152 All other relevant policies and considerations, including equalities, have been taken into account. It is recommended that planning permission is granted in line with the officer recommendation for the reasons summarised in this report.

Appendix 1: Drawing numbers

Location Plan TID_CRC_HTA-A_DR_0100, Level 00 TID_CRC_HTA-A_DR_0200 D, Level 0M TID_CRC_HTA-A_DR_0201 B, Level 01 TID_CRC_HTA-A_DR_0202 B, Level 02 TID_CRC_HTA-A_DR_0203 B, Level 03-32 – Typical Lower TID_CRC_HTA-A_DR_0204 B, Level 33-45 – Typical Upper TID_CRC_HTA-A_DR_0237 B, Intermediate Amenity TID-CRC_HTA-A_DR_0245, Level 46 TID_CRC_HTA-A_DR_0246 B, Level 47 TID_CRC_HTA-A_DR_0247 B, Roof Plan TID-CRC_HTA-A_DR_0248 A, Level 34 – General Arrangement Plan HTA-L_DR_0911, Level 34 – Planting Strategy HTA-L_DR_0912, Level 34 – Landscape Masterplan HTA-L_DR_0910, Level 48 – General Arrangement Plan HTA-L_DR_0916, Level 48 – Planting Strategy HTA-L_DR_0917, Level 48 – Landscape Masterplan HTA-L_DR_0915, Level B1 TID_CRC_HTA-A_DR_0290 B, Level B2 TID_CRC_HTA-A_DR_0291 A, General Arrangement Plan HTA-L_DR_0901 B, North Elevation TID_CRC_HTA-A_DR_0400 B, East Elevation TID-CRC_HTA-A_DR_0401 B, South Elevation TID_CRC_HTA-A_DR_0402 B, West Elevation TID_CRC_HTA-A_DR_0403 B, Levels Strategy Plan HTA-L_DR_0902 B, Planting Strategy HTA-L_DR_0903 B, Landscape Masterplan TID-CRC_HTA-L_DR_0900 B, 4239-5-3-M401, 4329-5-8-SK100, Sketch Section Through Southern Tree Pit 03.02.20, Fire Safety Statement OF-OFS-3692-01-A, Fire Submission Supplementary Part B Information 3rd February 2020, Wind Microclimate Report 0080034rep1v3 25 November 2019, Wind Microclimate Supplementary Report to 0080034rep1v4 30 January 2020, Civil Engineering Infrastructure Report 19.732-IT-01 22.01.20 + appendices, Energy Strategy – response to comments December 2019, Air Quality Assessment J3845A/1/F2 22 October 2019, Delivery and Servicing Management Plan October 2019, Desk Study/Preliminary Risk Assessment report P2169J1690/AMM Final v1.1 10 October 2019, Ecology Report 17/9/2019 V2.0, Geo-Environmental & Geo-Technical Assessment (Ground Investigation) Report P2169J1690/AMM V1.2 10th October 2019, Acoustic Design Statement DJB/7264/A 16 October 2019

Appendix 2: BRE Guidance Terms

Daylight to existing buildings

The BRE Guidelines stipulate that the diffuse daylighting of the existing building may be adversely affected if either:

- the vertical sky component (VSC) measured at the centre of an existing main window is less than 27%, and less than 0.8 times its former value (or reduced by more than 20%), known as “the VSC test” or
- the area of the working plane in a room which can receive direct skylight is reduced to less than 0.8 times its former value known as the “daylight distribution” (DD) test.

Sunlight to existing buildings

The BRE Guidelines stipulate that the sunlight of an existing window may be adversely affected if the centre of the window:

- receives less than 25% of annual probable sunlight hours (APSH), or less than 5% of annual winter probable sunlight hours between 21 September and 21 March (WPSH); and
- receives less than 0.8 times its former sunlight hours (or a 20% reduction) during either period; and
- has a reduction in sunlight received over the whole year greater than 4% of annual probable sunlight hours.

If one of the above tests is met, the dwelling is not considered to be adversely affected.

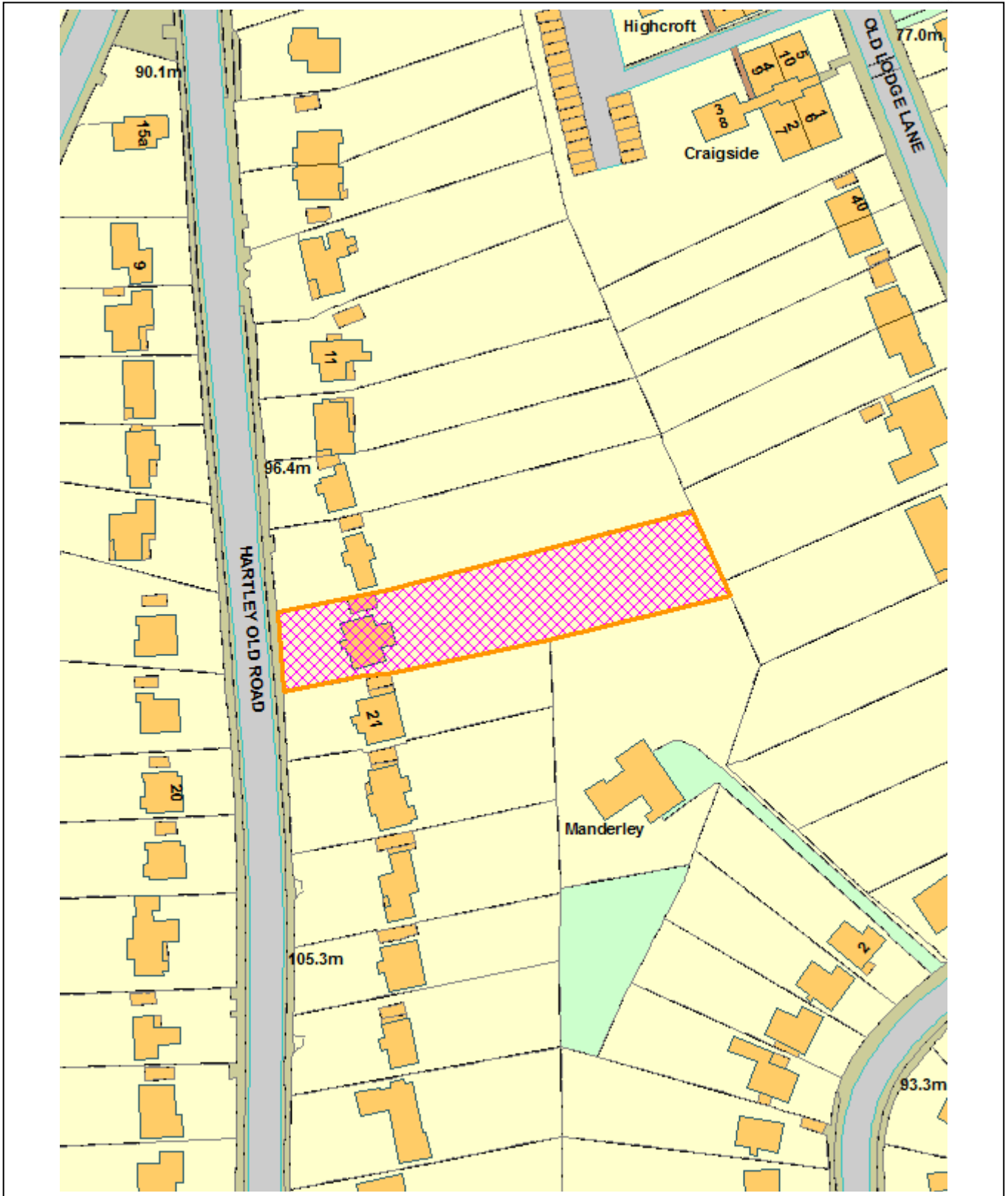
Daylight to new buildings: Average Daylight Factor (ADF)

The ADF test calculates the average illuminance within a room as a proportion of the illuminance available to an unobstructed point outdoors, under a sky of known illuminance and luminance distribution.

The BRE Guidelines stipulate that kitchens should attain at least 2% ADF, living and dining rooms at least 1.5% ADF and bedrooms at least 1% ADF.

Sunlight to gardens and outdoor spaces

The BRE guidelines look at the proportion of an amenity area that received at least 2 hours of sun on 21st March. For amenity to be considered well sunlight through the year, it stipulates that at least 50% of the space should enjoy these 2 hours of direct sunlight on 21st March.



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PART 6: Planning Applications for Decision

Item 6.2

1.0 SUMMARY OF APPLICATION DETAILS

Ref: 18/06068/FUL
 Location: 19 Hartley Old Road, Purley, CR8 4HH
 Ward: Purley and Woodcote
 Description: Demolition of a single-family dwelling and erection of a 3 storey block containing 9 flats with associated access, car parking, cycle and refuse storage (Amended plans).
 Drawing Nos: CX15-101D, CX15-102, CX15-103G, CX15-104F, CX15-105G, CX15-106D, CX-107B, CX15-108A, CX15-109A, CX15-110C, CX15-111B, CX15-112B, CX15-113A, CX15-114, CX15-115A, CX15-116, SUDS, Flood report, Transport statement, M4(2) statement, Energy report, Ecological survey, Badgerland fence design details, Hard landscape rev B, Soft landscape rev B, 5 year landscape management plan, Planting schedule A, Tree specification A, Arboricultural report, Bat survey report 001, Reptile survey 001, Phase 1 habitat survey plan.
 Applicant: Mr Rafael Porzycki of Aventier Ltd
 Case Officer: Nathan Pearce

	1B 2P	2B 3P	2B 4P	3B 4P	4B+	Total
Existing Provision				1		1
Proposed Provision		5	1	3		9

1.1 This application is being reported to Planning Committee because objections above the threshold in the Committee Consideration Criteria have been received and it has been referred by a ward Councillor, been referred by the Hartley & District Residents' Association (HADRA) and a petition has been received.

2.0 RECOMMENDATION

- 2.1 That the Planning Committee resolve to GRANT planning permission
- 2.2 That the Director of Planning and Strategic Transport has delegated authority to issue the planning permission and impose conditions and informatives to secure the following matters:

Conditions

- 1. Development to be carried out in accordance with the approved drawings and reports except where specified by conditions
- 2. Construction Logistics Plan

3. Details of facing materials
4. Landscaping
5. Cycle and refuse stores
6. Play space
7. 19% reduction in CO2 Emissions and 110l Water Restriction
8. Car parking
9. Permeable forecourt material
10. Arboricultural report
11. Trees – Details in accordance with tree report
12. Visibility splays
13. Sustainable urban drainage details
14. Windows restrictions
15. Building maintenance strategy
16. Ecology conditions
17. Time limit of 3 years
18. Any other planning condition(s) considered necessary by the Director of Planning and Strategic Transport

Informatives

- 1) CIL
- 2) Code of practise for Construction Sites
- 3) Construction Logistics Plan
- 4) Trees and shrubs
- 5) Refuse
- 6) Any other informative(s) considered necessary by the Director of Planning and Strategic Transport

3.0 PROPOSAL AND LOCATION DETAILS

3.1 The proposal includes the following:

- Demolition of existing detached house
- Erection of a three storey building including accommodation in roof-space.
- Provision of 5 x 2 bedroom flats (3 person), 1 x 2 bedroom (4 person) flat, 3 x 3 bedroom flats.
- Provision of 7 off-street parking spaces including one disabled bay.
- Provision associated refuse/cycle stores.

3.2 Amended plans were received on 04/12/2019 showing a redesigned front elevation, additional private amenity provision, revised landscaping, revised internal cycle and refuse stores and one 2 bed unit being redesigned to a 3 bed unit. A re-notification of neighbours was undertaken on 14/01/2020.

Site and Surroundings

3.3 The application site is a large detached property situated on the east side of Hartley Old Road. The topography of the site is a sloping site. The land rises from the lower level on the north side where no.17 is, to a higher level on the south of the site.

3.4 The surrounding area is mainly residential in character. Whilst there is no distinct style in regard to the properties along Hartley Old Road, the majority of properties appear to be detached family dwellinghouses. The site has a Public Transport Accessibility Level (PTAL) of 1b.



Fig 1: Aerial street view highlighting the proposed site within the surrounding street-scene

Planning History

3.5 None relevant

4.0 SUMMARY OF KEY REASONS FOR RECOMMENDATION

- The proposed development would create good quality residential accommodation that would make a positive contribution to the borough's housing stock and would make a small contribution to the Council achieving its housing targets as set out in the London Plan (2016) and Croydon Local Plan (2018) and emerging housing targets. The proposed development would provide an appropriate mix of units including 3x three-bed houses and 1x four-person, two-bed flat.
- The proposed development would be of an appropriate mass, scale, form and design that would be in keeping with its context, thus preserving the appearance of the site and surrounding area.
- The proposed development would not cause unacceptable harm to the amenities of neighbouring residential occupiers.
- The proposed development would not have an adverse impact on the operation of the highway.
- Subject to the imposition of conditions, the proposed development would not cause unacceptable harm to visual amenity of trees.

- Subject to conditions, the proposals would not have an adverse impact on flooding.
- Sustainability aspects can be controlled by conditions.

5.0 CONSULTATION RESPONSE

5.1 The views of the Planning Service are expressed in the MATERIAL PLANNING CONSIDERATIONS section below.

5.2 East Surrey Badger Society: The Society objects to the proposed planning application on the grounds that the development will result in the loss of badger habitat and could lead to the already stressed badger group disappearing completely. [OFFICE COMMENT: Further advice has been received from the Council's ecological advisors who recommend that, subject to conditions, the impact on badgers is acceptable]

6.0 LOCAL REPRESENTATION

6.1 The application has been publicised by 12 letters of notification to neighbouring properties in the vicinity of the application site plus a re-notification of the amended plans. The number of representations received from neighbours, a petition, a Residents' Association, local ward Councillors and Local MP in response to notification and publicity of the application are as follows:

No of individual responses:164 Objecting: 161 Supporting: 0
 Comment: 2

No of petitions: 1 (objecting)
 Signatures: 52

6.2 The following issues were raised in representations. Those that are material to the determination of the application, are addressed in substance in the MATERIAL PLANNING CONSIDERATIONS section of this report:

Summary of objections	Response
<i>Principle of development</i>	
Overdevelopment and intensification	Addressed in the report at paragraphs 8.2 – 8.6
Loss of family home	Addressed in the report at paragraph 8.2 – 8.6
Poor quality development	Addressed in the report at paragraphs 8.2 – 8.6
<i>Design</i>	
Out of character	Addressed in the report at paragraphs 8.7 – 8.12
Massing too big	Addressed in the report at paragraphs 8.7 – 8.12
Over intensification – Too dense	Addressed in the report at paragraph 8.7 – 8.12
Visual impact on the street scene (Not in keeping)	Addressed in the report at paragraphs 8.7 – 8.12

Accessible provision	Addressed in the report at paragraphs 8.23
Number of storeys	Addressed in the report at paragraphs 8.9
<i>Amenities</i>	
Negative impact on neighbouring amenities	Addressed in the report at paragraphs 8.13 – 8.18
Loss of light	Addressed in the report at paragraphs 8.13 – 8.18
Loss of privacy	Addressed in the report at paragraphs 8.13 – 8.18
Overlooking	Addressed in the report at paragraphs 8.13 – 8.18
Disturbance (noise, light, pollution, smells etc.)	Addressed in the report at paragraphs 8.13 – 8.18
Refuse store	Addressed in the report at paragraphs 8.31
<i>Traffic & Parking</i>	
Negative impact on parking and traffic in the area	Addressed in the report at paragraphs 8.25 – 8.32
Not enough off-street parking	Addressed in the report at paragraphs 8.25 – 8.32
Negative impact on highway safety	Addressed in the report at paragraph 8.25 – 8.32
Refuse and recycling provision	Addressed in the report at paragraph 8.25 – 8.32
<i>Other matters</i>	
Construction disturbance	Addressed in the report at paragraph 8.41
Impact on wildlife	Addressed in the report at paragraphs 8.33 – 8.38
Impact on flooding	Addressed in the report at paragraph 8.40
Local services cannot cope	Addressed in the report at paragraph 8.43
Lack of affordable homes	Addressed in the report at paragraph 8.42
Impact on trees	Addressed in the report at paragraphs 8.33 – 8.35

6.3 Cllr Simon Brew (Purley & Woodcote Ward) has referred the application to committee and raised the following issues:

- Contrary to local plan paragraph 11.122 in failing to respect existing residential character and local distinctiveness
- Lack of affordable homes
- Side windows overlook neighbouring property
- Insufficient waste capacity

- Increased density is contrary to DM10 paragraph 2a
- Insufficient car parking given low PTAL
- Insufficient protection of badger sett

6.4 The Kenley & District Residents' Association has referred the application to committee and raised the following issues:

- The development pattern, layout and siting is out of character
- The scale, height, massing, density and increased height is out of character
- The appearance, existing materials and built and natural features are out of character.
- Insufficient car parking
- Poor quality living environment for future occupiers
- Impact on privacy and light of neighbouring occupiers
- Overintensification of the site
- Too close to side boundaries
- Does not comply with sustainable design principles
- Incorrect accessibility statement
- Insufficient Construction Logistics Plan
- Inadequate wildlife protection

7.0 RELEVANT PLANNING POLICIES AND GUIDANCE

7.1 In determining any planning application, the Council is required to have regard to the provisions of its Development Plan so far as is material to the application and to any other material considerations. Such determination shall be made in accordance with the Plan unless material considerations indicate otherwise. The Council's adopted Development Plan consists of the Consolidated London Plan 2015, the Croydon Local Plan (February 2018), and the South London Waste Plan 2012.

7.2 Government Guidance is contained in the National Planning Policy Framework (NPPF) revised in February 2019. The NPPF sets out a presumption in favour of sustainable development, requiring that development which accords with an up-to-date local plan should be approved without delay. The NPPF identifies a number of key issues for the delivery of sustainable development, those most relevant to this case are:

- Promoting sustainable transport;
- Delivery of housing
- Promoting social, recreational and cultural facilities and services the community needs
- Requiring good design.

7.3 The main policy considerations raised by the application that the Committee are required to consider are:

7.4 Consolidated London Plan 2015

- 3.3 Increasing housing supply
- 3.4 Optimising housing potential
- 3.5 Quality and design of housing developments
- 3.8 Housing choice
- 3.9 Mixed and balanced communities
- 5.1 Climate change mitigation
- 5.2 Minimising carbon dioxide emissions
- 5.3 Sustainable design and construction
- 5.7 Renewable energy
- 5.10 Urban greening
- 5.12 Flood risk management
- 5.13 Sustainable drainage
- 5.14 Water quality and wastewater infrastructure
- 5.15 Water use and supplies
- 5.16 Waste net self sufficiency
- 5.18 Construction, Demolition and excavation waste
- 6.3 Effects of development on transport capacity
- 6.9 Cycling
- 6.10 Walking
- 6.11 Smoothing traffic flow and tackling congestion
- 6.12 Road Network Capacity
- 6.13 Parking
- 7.6 Architecture
- 8.3 Community infrastructure levy

7.5 Croydon Local Plan (adopted February 2018)

- SP1 – The places of Croydon
- SP2 – Homes
- DM1 – Housing choice for sustainable communities
- SP4 – Urban Design and Local Character
- DM10 – Design and character
- DM13 – Refuse and recycling
- SP6 – Environment and Climate Change
- DM23 – Development and construction
- DM24 – Land contamination
- DM25 – Sustainable drainage systems and reducing flood risk
- SP7 – Green Grid
- DM27 – Biodiversity
- DM28 – Trees
- SP8 – Transport and Communications
- DM29 – Promoting sustainable travel and reducing congestion
- DM30 – Car and cycle parking in new development
- DM42 – Purley

7.6 Suburban Design Guide Supplementary Planning Document (SPD) 2019

The SPD is a Housing Design Guide that provides guidance on suburban residential developments and extensions and alterations to existing homes across the borough. The SPD is a design guide for suburban developments likely to occur on windfall sites where existing homes are to be redeveloped to provide for several homes or proposals for building homes in rear gardens.

7.7 Other relevant Supplementary Planning Guidance as follows:

- London Housing SPG, March 2016
- National Technical Housing Standards, 2015
- National Planning Practice Guidance

7.8 Emerging New London Plan

Whilst the emerging New London Plan is a material consideration, the weight afforded is down to the decision maker linked to the stage a plan has reached in its development. The Plan appears to be close to adoption. The Mayor's Intend to Publish version of the New London Plan is currently with the Secretary of State and no response had been submitted to the Mayor from the Secretary of State. Therefore, the New London Plan's weight has increased following on from the publication of the Panel Report and the London Mayor's publication of the Intend to Publish New London Plan. The Planning Inspectors' Panel Report accepted the need for London to deliver 66,000 new homes per annum (significantly higher than existing adopted targets), but questioned the London Plan's ability to deliver the level of housing predicted on "small sites" with insufficient evidence having been presented to the Examination to give confidence that the targets were realistic and/or achievable. This conclusion resulted in the Panel Report recommending a reduction in London's and Croydon's "small sites" target.

The Mayor in his Intend to Publish New London Plan has accepted the reduced Croydon's overall 10 year net housing figures from 29,490 to 20,790 homes, with the "small sites" reduced from 15,110 to 6,470 homes. Crucially, the lower windfall housing target for Croydon (641 homes a year) is not dissimilar to but slightly larger the current adopted 2018 Croydon Local Plan target of 592 homes on windfall sites each year.

It is important to note, should the Secretary of State support the Intend to Publish New London Plan, that the overall housing target in the New London Plan would be 2,079 new homes per annum (2019 – 2029) compared with 1,645 in the Croydon Local Plan 2018. Therefore, even with the possible reduction in the overall New London Plan housing targets, assuming it is adopted, Croydon will be required to deliver more new homes than our current Croydon Local Plan 2018 and current London Plan (incorporating alterations 2016) targets.

For clarity, the Croydon Local Plan 2018, current London Plan (incorporating alterations 2016) and South London Waste Plan 2012 remain the primary consideration when determining planning applications.

8.0 MATERIAL PLANNING CONSIDERATIONS

8.1 The principal issues of this particular application relate to:

- The principle of the development;
- Impact of the development on the character and appearance of the area;
- Impact on residential amenities;
- Standard of accommodation;
- Highways impacts;
- Impacts on trees and ecology;
- Sustainability issues; and
- Other matters

The Principle of Development

- 8.2 Both the London Plan and the NPPF place significant weight on housing delivery and focus on the roles that intensification and small sites in particular can play in resolving the current housing crisis. It is acknowledged that windfall schemes which provide sensitive renewal and intensification of existing residential areas play an important role in meeting the demand for additional housing in Greater London, helping to address overcrowding and affordability issues. Furthermore the Croydon Local Plan 2018 anticipates that roughly a third of housing delivery over the plan period will come from District Centres and windfall sites. The impact of the emerging London Plan is set out in paragraph 7.8 above.
- 8.3 The site is a windfall site which could be suitable for sensitive renewal and intensification.
- 8.4 Policy 3.3 of the London Plan 2016 recognises the pressing need for more homes in London and Policy 3.8 states that Londoners should have a genuine choice of homes which meet their requirements for different sizes and types of dwellings in the highest quality environments. The impact of the draft London Plan is set out in paragraph 7.8 above.
- 8.5 The proposal, whilst incorporating flatted accommodation, has been designed to appear as a large house which would maintain the overall character of neighbouring properties. The residential character of Hartley Old Road consists of detached houses.
- 8.6 The existing unit is a 4-bed house and the proposal would provide 3 x 3 bed and 1 x 2 bed (4 person) units which would provide adequate floorspace for families. Policy SP2.7 sets a strategic target of 30% of new homes to be 3-bedroom homes and CLP acknowledges that 2-bed, 4-person homes can be treated as family homes (in line with DM1.1) during the first 3 years of the Plan. The target of 30% of 3-bed and 4-bed units has been met. 2-bed, 4-person units are considered family units also, therefore it is considered that more units could be achieved from the site given the high amount of larger units that are proposed. The overall mix of accommodation would be acceptable and would result in a net gain in family accommodation.

8.7 Representations have raised concern over the intensification of the site and overdevelopment. The site is in a suburban setting with a PTAL rating of 1b and as such, the London Plan indicates that a suitable density level range is between 150-200 habitable rooms per hectare (hr/ha). Whilst the proposal would be within this range (153 hr/ha), it is important to note that the London Plan indicates that it is not appropriate to apply these ranges mechanistically, and also provides sufficient flexibility for higher density schemes (beyond the density range) to be supported where they are acceptable in all other regards. In this instance the proposal is acceptable, respecting the character and appearance of the surrounding area, and does not demonstrate signs of overdevelopment (such as poor quality residential units or unreasonable harm to neighbouring amenity). As such the scheme is supported.

The effect of the proposal on the character of the area and visual amenities of the streetscene

8.8 The existing property is not protected from demolition by existing policies and its demolition is acceptable subject to a suitably designed replacement building coming forward. The proposal seeks to replace it with 9 units within a single building. The scheme has been specifically designed to resemble a large house, rather than a block of flats. Officers are satisfied that the scheme respects the street-scene.

8.9 The Croydon Local Plan has a presumption in favour of three storey development and the application seeks to provide a three-four storey property providing a high quality built form that respects the land level changes, pattern, layout and siting in accordance with Policy DM10.1.

8.10 The height, scale and massing of the scheme would be acceptable, given that the site works well with the topography and would sit well with the adjoining properties.



Fig 2: Elevational view highlighting the proposal in relation to neighbouring properties.

8.11 The design of the building would incorporate a traditional styled appearance consisting of gables and bays to the front elevation, maintaining the overall street scene with use of an appropriate materials palette with an adequate balance between render, soldier course brickwork, mock tudor frame, glazing and appropriate roof proportions. The main front element would present a traditional architectural response, consisting of gabled bays. It is noted that half timbered detailing is predominantly found on the buildings in the immediate area and that the majority of these buildings are fully rendered or use render above a brick ground floor, with hung tile roofs also being characteristic. As such, whilst there are concerns about the appearance of render over time, a rendered building is considered to be an appropriate design response in this situation. A condition is recommended to require details of a building maintenance strategy so that the appearance of the building can be adequately controlled.

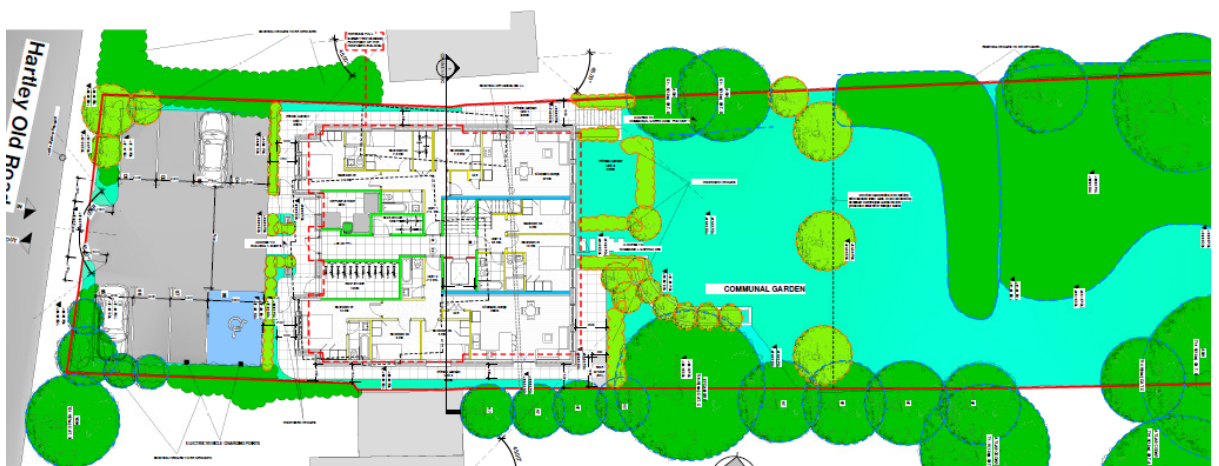


Fig 3: Proposed first floor site plan showing proposal in relation to neighbouring properties

8.12 Policy DM10.2 seeks to create well defined and designed public and private spaces and advises that forecourt parking should only be allowed where it does not cause undue harm to the character or setting of the building and is large enough to accommodate parking with sufficient screening to prevent vehicles encroaching on the public highway. Whilst the frontage would be given over to hard-standing to allow for off street parking there would be some soft landscaping surrounding it, along with a section of soft landscaping along the boundary. Given the overall scale of the development and number of forecourt hardstanding areas in the vicinity, the extent of hardstanding would not be excessive. The site would offer sufficient opportunities for soft landscaping to the rear.

8.13 The application site is a substantial plot within an established residential area. The scale and massing of the new build would generally be in keeping with the overall scale of development found in the immediate area whilst sensitively intensifying it and the layout of the development would respect the streets' pattern and rhythm.



Fig 4: CGI of site showing proposal in relation to neighbouring properties

Having considered all of the above, against the backdrop of housing need, officers are of the opinion that the proposed development would comply with the objectives of the above policies and the Suburban Design Guide SPD 2019 in terms of respecting local character.

The effect of the proposal upon the amenities of the occupiers of adjoining properties

8.14 Policy DM10.6 states that the Council will not support development proposals which would have adverse effects on the amenities of adjoining or nearby properties, or have an unacceptable impact on the surrounding area. This can include a loss of privacy, a loss of natural light, a loss of outlook or the creation

of a sense of enclosure. The properties with the potential to be most affected are the adjoining properties at 17 & 21 Hartley Old Road, dwellings opposite on Hartley Old Road and to the rear on Old Lodge Lane.



Fig 5: Surrounding properties

17 Hartley Old Road

8.15 This dwelling is to the north and is at a lower level than the proposal site. It has 1 obscure glazed side facing window that is considered to serve a non-habitable room. It has a front facing and a rear facing ground floor habitable room windows for which the development would break the horizontal and vertical 45 degree lines. A Vertical Sky Component Analysis under BRE guidelines has concluded that there would be a negligible impact on the window receptors for these windows.

The height, forward and rear projections of the proposed dwelling are considered to have an acceptable relationship to no.17 in terms of the impact of daylight & sunlight on the habitable room windows.



21 Hartley Old Road

8.16 There is a side facing ground floor habitable window that faces the proposed development. This is considered to be one of two windows serving that room, with the other on the front façade. A Vertical Sky Component Analysis under BRE guidelines has concluded that there would be a moderate impact on the window receptors. This is considered to be acceptable because it is one of two windows serving that room, the impact on the front facing window is considered to be negligible. It also has 1 first floor side facing window that is considered to serve a non-habitable room (bathroom).

This dwelling is to the south and is at a higher level than the proposal site. The rear of the proposed building would not break a 45 degree line drawn from rear habitable room windows, it is considered that given the separation distances and the angles that there would not be a significant impact on these dwellings in terms of loss of light, outlook, privacy or sense of overbearing. This is considered to be an acceptable relationship in a suburban setting such as this.

Dwellings opposite on Hartley Old Road and to the rear on Old Lodge Lane

8.17 It is considered that given the separation distances that there would not be a significant impact on these dwellings in terms of loss of light, outlook, privacy or sense of overbearing. This is considered to be an acceptable relationship in a suburban setting such as this.

General

8.18 As regards noise and disturbance, the proposed development would not result in undue noise, light or air pollution as a result of an increased number of occupants on the site. The increased number of units would increase the number of vehicle movements to and from the site, but this would not be significant and would not be overly harmful.

The effect of the proposal upon the amenities of future occupiers

8.19 The Nationally Described Space Standards (NDSS) provide minimum technical space standards for new dwellings in terms of the gross internal floor areas and

storage. All of the proposed units would meet the minimum required gross internal floor area.

- 8.20 The units would have access to private and communal amenity space which meets the required standard.
- 8.21 The private amenity space for unit 1 would consist of a thin strip along the north side of the building. This area measures 35 sqm, which exceeds the minimum standards and, whilst narrow, would be usable. It also provides direct access to the communal amenity space and is on balance acceptable.
- 8.22 Unit 3 would be on a split level with two bedrooms on the ground floor and one bedroom & kitchen/lounge on the basement level. The basement windows would look out on to their private amenity space and rear garden beyond it that is at a lower level.
- 8.23 The local plan also requires all flatted development to provide new child play space as well as the amenity space to be provided. In terms of the child play space, this can be secured through use of planning conditions.
- 8.24 In terms of accessibility, a lift is proposed to the block of flats. As such all units can meet M4(2) standard and one two-bedroom unit meets M4(3), in compliance with policy.
- 8.25 Overall the development is considered to result in a high quality development, including an uplift in family accommodation, and will offer future occupiers a good standard of amenity, including the provision of communal amenity space and child play space, and thus accords with relevant policy.

Traffic and highway safety implications

- 8.26 The Public Transport Accessibility Level (PTAL) rating is 1b which indicates poor accessibility to public transport. The London Plan and Policy DM30 of CLP2018 sets out that maximum car parking standards for residential developments based on public transport accessibility levels and local character. This states that 1-2 bedroom properties should provide a maximum of up to 1 space per unit, with up to 1.5 spaces per unit being provided for 3 bedroom properties. In line with the London Plan, the proposed development could therefore provide up to a maximum of 10.5 spaces. It is important to note however that it is not necessarily desirable to provide car parking up to the maximum standards given the requirements of both the London Plan and Croydon Local Plan which seek to reduce reliance on car usage and promote/prioritise sustainable modes of transport. As such a lower level of car parking can be supported and is encouraged in line with the ambitions of the Development Plan.
- 8.27 A parking survey has been provided which shows the stress occupancy over the two beats ranged from 8 cars parked in the closest 64 spaces to 11 cars parked. This would give a parking stress level of 13%-17% respectively, with plenty of available local parking spaces close to the site, and as such accords with the policy requirements for a development of this nature in this location. The

proposed car parking provision is considered acceptable when taking into account the site constraints, the need to provide high quality multi-functional spaces whilst preserving the existing trees on-site and ensuring the best use of land.

- 8.28 There are a number of representations that refer to the parking provision, on-street parking and highway safety at the site. In respect to highway safety, the scheme provides 8 off-street parking spaces including 1 disabled space and these will need to adhere to the parking visibility splays and parking standards to ensure that safety requirements are adhered to and these have been secured through conditions.
- 8.29 The parking layout arrangement has been amended and the refuse store has been located within the building allowing an additional parking space to be provided. A swept path plan shows that vehicles will be able to enter and exit in first gear.
- 8.30 In compliance with the London Plan, electric vehicle charging points should be installed in the parking area and this can be secured by way of a condition. Cycle storage facilities would comply with the London Plan (which would require 18 spaces) as these are located in a secure and covered cycle store within the rear communal amenity space. Vertical cycle parking would not be appropriate. This can be secured by way of a condition.
- 8.31 The refuse arrangements would be acceptable and for a nine units scheme would require 1 x 1100ltr landfill receptacle; 1 x 1280ltr for dry recycling and 1 x 140ltr food recycling, which has been accommodated within the site. The refuse store would be located in front of the building with access to the hardstanding. It can be secured by condition.
- 8.32 A Demolition/Construction Logistic Plan (including a Construction Management Plan) will be needed before commencement of work and this could be secured through a condition.

Ecology and Biodiversity

- 8.33 The site is bordered by established trees and shrubs adding to the overall amenity value and also providing a good degree of screening. The proposed landscape design protects most of the existing trees at the rear of the site and provides a large variety of bushes and hedges. A landscaping and planting plan has been submitted and can be conditioned.
- 8.34 The works should be undertaken in accordance with the tree protection plan; Arboricultural Report and Impact Assessment recommendations and this has been conditioned.



Fig 7: Extract from submitted soft landscaping scheme

- 8.35 There is a very large active main badger sett which extends from Hartley Hill through to 17 Hartley Old Road. Badgers and other wildlife have thrived in the woodland areas at the bottom of the long gardens. This strip of unmanaged habitat has been a wildlife corridor since before Old Lodge Lane was developed.
- 8.36 A Habitat Survey has been included with the application which shows evidence of a badger sett. A bat survey and reptile presence/absence survey have also been submitted. The Council's ecology consultant has reviewed the submissions and has concluded that the sett is well used and that there is a small population of slow worms on the site and that a summer bat roost was present in one of the buildings to be demolished.
- 8.37 The population of slow worms is considered to be low. Some concerns were raised as to the timing of surveys for reptiles, but advice has confirmed that the surveys are adequate to assess the population size and necessary remediation. The proposed remediation would require undergrowth to be cleared in a sensitive manner and slow worms to be moved to the area of the site (rear of garden) which would not be disturbed by the development, and that the landscaping scheme should include areas of habitat improvement suitable for slow worms. This can be controlled by condition.
- 8.38 The bat roost is considered likely to be used by 1 or 2 Pipistrelle bats (a common form of bat) in summer months and so, whilst of local importance, its removal is not unacceptable, with suitable mitigation. The mitigation is likely to consist of the creation of a temporary replacement roost and supervision of demolition works, with the full details being set out under a Natural England license to carry out works. With conditions to secure that a copy of the license is provided and mitigation undertaken, the impact is considered to be acceptable.
- 8.39 The impact of the development on the badger sett has been carefully assessed and further surveys have been undertaken. The sett is well used but the proposed building would be located at some 30m from the sett, The Council's ecological advisor has recommended that the impact on the sett is acceptable given suitable mitigation to ensure that it is not significantly disturbed during

construction and that foraging routes are not disturbed. The scheme originally proposed to fence off the area of the badger sett to prevent its disturbance by residents, but this element of the scheme has been amended to provide a gate for residents to access for maintenance. The fencing would only be erected between the communal gardens and the protected area, this would prevent badgers from entering the communal gardens but would not stop them from being able to cross into neighbouring gardens for foraging. The exclusion of the communal garden from their foraging area is not considered to adversely impact on the sett. With suitable mitigation to ensure that construction activity does not significantly affect the sett area the impact on badgers is considered to be acceptable. In order to ensure that no further impact would occur should the usage of the sett change, a condition requiring further supplementary ecological surveys for badgers prior to commencement of development has been.

- 8.40 The Council has certainty of the likely impacts on protected species and sites. Through the imposition of planning conditions and work undertaken to date, the local planning authority has operated in accordance with its statutory duties relating to biodiversity and national and local policy requirements.

Sustainability Issues

- 8.39 Conditions can be attached to ensure that a 19% reduction in CO2 emissions over 2013 Building Regulations is achieved and mains water consumption would meet a target of 110 litres or less per head per day.

Other Matters

- 8.40 The site is not located in any designated flood risk area. The applicants have submitted a Surface Water and SuDS Assessment which is based on a desktop study of underlying ground conditions. It is likely that infiltration of surface water runoff following redevelopment may be feasible. The parking area will incorporate permeable paving which will provide capacity for surface water runoff from hardstanding areas in up to the 1 in 100 years plus 40% climate change event. This can be secured through a condition.
- 8.41 Representations have raised concern that construction works will be disruptive and large vehicles could cause damage to the highway. Whilst the details submitted to date might well be acceptable, it would be prudent to condition a Construction Logistics Plan to be approved, as appointed contractors may have an alternative approach to construction methods and the condition ensures that the LPA maintains control to ensure the development progresses in an acceptable manner.
- 8.42 Representations have been made in respect to a lack of affordable homes being provided at the site, however the scheme is for nine units and as such is under the threshold where the provision for affordable homes would be required.
- 8.43 Representations have raised concerns that local schools and other services will be unable to cope with additional families moving into the area. The development will be liable for a charge under the Community Infrastructure Levy (CIL). This

payment will contribute to delivering infrastructure to support the development of the area, such as local schools.

Conclusions/planning balance

8.44 The principle of development is acceptable within this area. The design of the scheme is of an acceptable standard given the proposed and conditioned landscape and subject to the provision of suitable conditions the scheme is acceptable in relation to residential amenity, transport, sustainable and ecological matters. Thus the proposal is considered in general accordance with the relevant policies.

8.45 All other relevant policies and considerations, including equalities, have been taken into account.

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1 SUMMARY OF APPLICATION DETAILS

Ref: 19/03628/FUL
 Location: 5 Croham Valley Road
 Ward: Selsdon & Ballards
 Description: Demolition of the existing property and erection of 6 houses (3 houses fronting Croham Valley Road and 3 houses fronting Ballards Farm Close), gardens, car parking, new accesses, refuse and recycling.
 Drawing Nos: 31-P2-4H, 31-P2-5F, 31-P2-6C, 31-P2-7E, 31-P2-8C, 31-P2-9C, 31-P2-11C, 31-P2-14B, 31-P2-15A, 31-P2-16B, 31-P2-17B
 Applicant: Sterling Rose
 Case Officer: Pete Smith

	3 bed	4 bed
Existing houses	1	
Proposed Houses	3	3

All units are proposed for private sale

Number of car parking spaces	Number of cycle parking spaces
4	12

1.1 This application is being reported to Planning Committee as objections above the Committee Consideration Criteria have been received and as a consequence of referral requests from Councillor Michael Neal and the Croham Valley Residents Association.

2 RECOMMENDATION

2.1 That the Committee resolve to GRANT planning permission subject to the completion of a S.106 Agreement to secure the following heads of terms:

- A financial contribution of £6,000 towards highway management measures and the delivery of sustainable transport initiatives in and around Croham Valley Road and neighbouring streets.
- The costs associated with the removal and replanting of street trees (including on-going maintenance) to facilitate the formation of the crossover

- 2.2 That the Director of Planning and Strategic Transport has delegated authority to negotiate the legal agreement indicated above.
- 2.3 That the Director of Planning and Strategic Transport has delegated authority to issue the planning permission and impose conditions and informatives to secure the following matters:

Conditions

- 1) Development to be carried out in accordance with the approved drawings and reports except where specified by conditions.
- 2) Materials to be submitted with samples.
- 3) Details of refuse storage and cycle storage enclosures
- 4) Landscaping scheme to be submitted including hard/soft landscaping, retaining walls, the retention of hedging between 5 and 7 Croham Valley Road, boundary treatments and SUDs techniques.
- 5) Details of groundwater flooding mitigation to be submitted and agreed
- 6) Construction Logistics Plan and Environmental Management Plan to be submitted for approval.
- 7) No occupation to take place until such time as the access onto Croham Valley Road and minor changes to the existing Ballards Farm Road access, on site car parking, electric charging points, refuse and cycle storage have been provided as specified in the application and subsequently approved by condition
- 8) Removal of permitted development rights for enlargements and outbuildings
- 9) Side windows to be obscurely glazed
- 10) No additional windows in the flank elevations
- 11) 19% reduction in carbon emissions
- 12) Water usage restricted to 110 litres per person per day
- 13) Commencement of development within three years of consent being granted
- 14) Any other planning condition(s) considered necessary by the Director of Planning and Strategic Transport

Informatives

- 1) CIL liability
 - 2) Code of Practice for Construction Sites
 - 3) Ecological Informative
 - 4) Any other informative(s) considered necessary by the Director of Planning and Strategic Transport
- 2.4 That the Planning Committee confirms that adequate provision has been made, by the imposition of conditions, for the preservation or planting of trees as required by Section 197 of the Town and Country Planning Act 1990.

3 PROPOSAL AND LOCATION DETAILS

Proposal

3.1 The proposal comprises the following:

- Erection of a three storey building with accommodation in the roof-space fronting onto Croham Valley Road, comprising 3x4 bedroom (7 person) houses,
- Erection of a two storey building with accommodation in the roof-space, fronting Ballards Farm Road, comprising 3x3 bedroom (4 person) houses,
- New vehicular access onto Croham Valley Road and utilisation of existing access onto Ballards Farm Road.
- Parking for four vehicles; two spaces accessed from Ballards Farm Road and two from Croham Valley Road
- Associated cycle storage, excavation, retaining walls and soft landscaping.

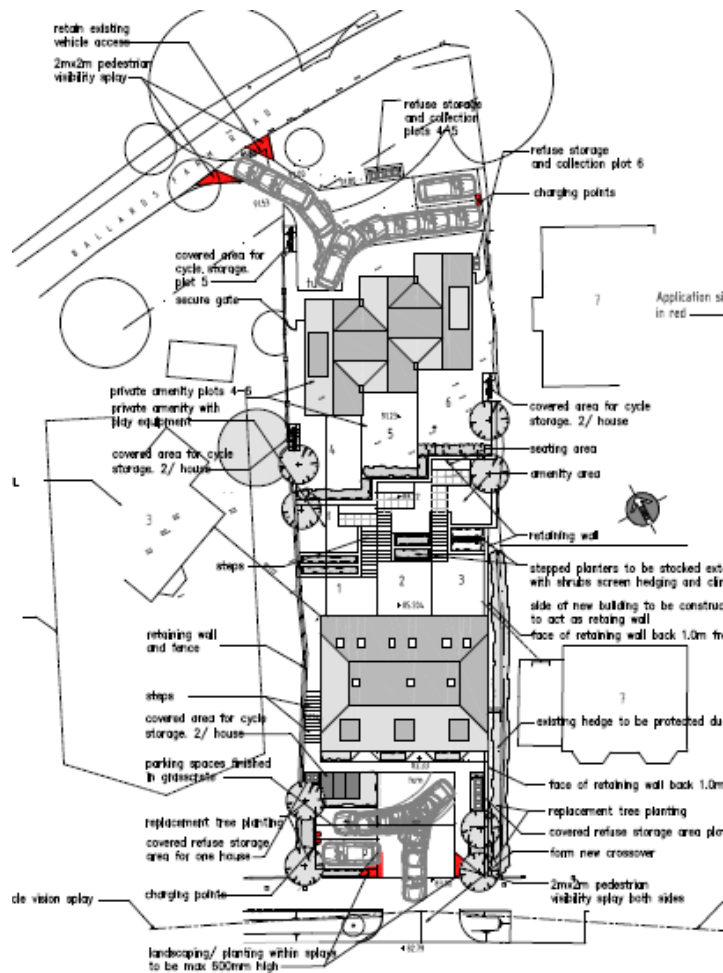


Figure 1 Proposed Site Layout

- 3.2 Amended drawings have been received during the course of the application, amending the appearance and materiality of the proposed building, providing additional sections through the site and reducing on site car parking to ensure that cars are able to turn on site and to enter and exit onto Ballards Farm Road in forward gear. Neighbours were re-notified in November 2019. A further amendment was made to the proposed retaining wall (between 5 and 7 Croham Valley Road) which has been set back by 1 metre (off the boundary) to ensure the retention of an existing hedged boundary condition. This amendment did not require re-notification but the most relevant party (the adjoining neighbour) was notified.

Site and Surroundings

- 3.3 The site is currently occupied by a traditional two storey detached dwelling located towards the northern (highest) part of the site. It is angled within the plot, with main windows facing onto the garden attached to 3 Croham Valley Road. The plot and surrounding area is steeply sloping and falls significantly in height from north to south – down towards Croham Valley Road. The main entrance and vehicular access to the existing property is off Ballards Farm Road, which is a relatively narrow un-adopted lane with no pedestrian footways, although there is an existing pedestrian access – leading up to the property off Croham Valley Road.



Figure 2 View from Croham Valley Road – looking north

- 3.4 To the west is a two storey detached property situated to the top of the slope and accessed off Ballards Farm Road (3 Croham Valley Road). This property is the subject of separate planning application which is pending consideration (LBC Ref 18/06067/FUL) for the demolition of the existing house and erection of a five storey

building (two storey fronting Ballards Farm Road with basement levels fronting onto Croham Valley Road) comprising 7 flats with creation of new access off Croham Valley Road, 7 car parking spaces, refuse storage and landscaping.

- 3.5 To the east are two residential properties; 7 Croham Valley Road which is a two storey detached property (with rooms in the roof-space) which was granted planning permission in March 2015 on appeal (LBC Ref 14/05093/P) and 7 Ballards Farm Road, which is a two storey detached property.
- 3.6 The application site falls within a surface water critical drainage area with a low risk of surface water flooding and potential for groundwater flooding to occur at the surface. Land to the north of the site is a Site of Nature Conservation Importance and 7 Ballards Farm Road has trees protected by way of a tree preservation order TPO (No.8 1997) – although this tree is not located adjacent to the application site.
- 3.7 The site has a PTAL of 2. Footways are present on both sides of Croham Valley Road (with the highway having a 30 mph speed limit). The road is also part of the local cycle network and connects to South Croydon. Moreover, Croham Valley Road links (via Farley Road) to Addington Road which is a further cycle route connecting Sanderstead with Gravel Hill and beyond. There are two bus stops close by (serving the 64 and 433 bus routes).

Planning History

- 3.8 The following applications are of relevance to the consideration of this application:
- 19/00281/FUL: Demolition of the existing dwelling and erection of nine residential units (six residential apartments and three houses) with associated refuse, recycling, cycle storage and car parking, and new vehicular crossover onto Croham Valley Road

Permission refused (18/04/2019) on the following grounds:

- 1) By reason of its mass, depth and height, the proposed development would have an unduly overbearing impact on 7 Croham Valley Road, causing an unacceptable loss of outlook and feeling of enclosure, particularly from the rear garden. The proposal is therefore contrary to the requirements of Policy DM10 of the Croydon Local Plan (2018), Suburban Design Guide Supplementary Planning Document (2019) and Policy 7.6 of the London Plan 2015 (consolidated with amendments since 2011).
- 2) The development would result in sub-standard accommodation for future occupiers by reason of poor access to daylight/sunlight and outlook and would provide communal amenity space of inadequate quality by reason of its scale, layout and topography. The proposal is therefore contrary to the Policies 3.5 and 7.4 of the London Plan 2015 (consolidated with alterations since 2011) Policy DM10 of the Croydon Local Plan (2018) and the London Housing SPG.

- 3) The development would create a hazard to pedestrians, cyclists and vehicular traffic using the highways by reason of inadequate visibility splays and unsafe access and parking arrangements and would thereby conflict with Paragraph 109 of the NPPF, Policies DM29 and DM30 of the Croydon Local Plan (2018) and Policies 6.3 and 6.12 of the London Plan 2015 (consolidated with amendments since 2011).

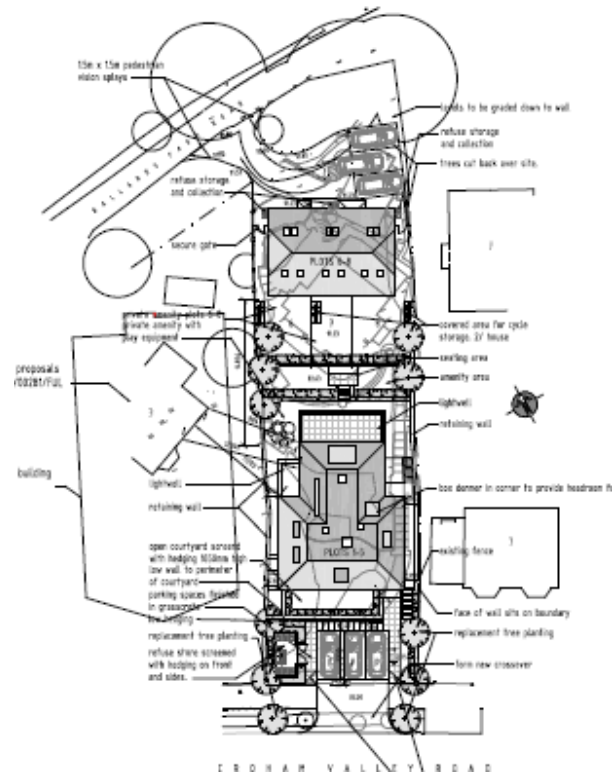


Figure 3 Previously Refused Site Layout

3.9 The current application seeks to overcome these previous reasons for refusal through a reduction in the number of units proposed, a greater focus towards houses (rather than flats), a reduced footprint (specifically the proposed houses fronting onto Croham Valley Road) and a reduced impact on the garden amenities of the neighbouring occupiers – specifically 7 Croham Valley Road.

3.10 The following applications at nearby sites are also of relevance:

10 Croham Valley Road (diagonally opposite the application site)

16/04907/OUT: Demolition of existing dwelling, erection of a building comprising 8 two bedroom flats, formation of vehicular access and provision of parking area with 8 spaces, cycle and refuse stores.

Permission granted

17/06096/RSM: Reserved matters associated with the above application

Approved The development has now been substantially completed

3.11 Three further schemes are currently under consideration and are listed below:

3 Croham Valley Road (next door property)

18/06067/FUL Demolition of existing building and the erection of a five storey building (two storey fronting Ballards Farm Road with basement levels) comprising 7 flats with creation of new access off Croham Valley Road, parking areas (7 spaces) refuse storage and landscaping

Application under consideration

6 Croham Valley Road

19/05034/FUL Demolition of existing house; erection of a two storey building plus roof space to provide 9 apartments; provision of 8 car parking spaces, refuse store and new landscaping.

A report into this proposal appears elsewhere on this agenda.

R/O 31-33 Croham Valley Road

19/04615/FUL) Erection of 2 two storey detached buildings with accommodation within the roof-space comprising 8 flats, bin store, the formation of vehicular access and provision of 7 parking spaces.

A report into this proposal also appears elsewhere on this agenda.

4 LOCAL REPRESENTATION

4.1 The application has been publicised by way of letters sent to the occupiers of 6 adjoining properties around the application site. The number of representations received from neighbours in response to notification and publicity of the application were as follows:

No of individual responses: 33 Objecting: 33

No of petitions received: 0

4.2 The following issues were raised in representations. Those that are material to the determination of the application, are addressed in substance in the MATERIAL PLANNING CONSIDERATIONS section of this report:

Summary of objections	Response
Density of Development – and Quality of Accommodation	
<p>Gross overdevelopment of the site Cramped development</p> <p>Inadequate amenity space for future occupiers</p> <p>Schemes proposed at 3, 5, 6, 8 and 10 Croham Valley Road – possibly 150 new residents and their cars replacing 5 houses. Cumulative impact needs to be considered</p>	<p>The proposed density of development would equate to around 303 habitable rooms per hectare and 67 units per hectare. Whilst the habitable rooms per hectare figure would marginally exceed the range specified in the London Plan, the units per hectare would comply with the suggested range (albeit at the top of the range)</p> <p>All future gardens/amenity spaces would comply with required space standards</p> <p>Officers are satisfied that cumulative impact has been properly considered (and will continue to be considered). Further commentary is included below.</p>
Character and Appearance	
<p>Mass, depth and height would be overbearing on 7 Croham Valley Road.</p> <p>Significantly larger than neighbouring houses</p> <p>Bulk, mass and design out of keeping and will cause harm to local character. Design is just plain ugly. The scale of development means that the neighbouring property would be dwarfed –cutting out the only sunlight captured by the rear garden to 7 Croham Valley Road. Bins, bikes parking spaces not in character with the appearance of Croham Valley Road and will add clutter to the Croham Valley Road frontage.</p> <p>Terraced houses are not a feature of the area</p>	<p>Officers are satisfied that the scale and mass reflects the policy approach – and seeks to identify with the character of neighbouring properties – especially as the form, character and appearance of neighbouring properties is quite varied. Further commentary is included in later paragraphs.</p>

<p>Ballards Farm Road is a semi-rural lane well used by local people for walking, jogging, cycling and horse riding. The proposal will cause harm to the rural nature of the area</p> <p>Ridge line higher than adjacent properties on both roads</p>	<p>Ballards Farm Road is used by vehicular traffic – providing access and the existing house is accessed off Ballards Farm Road.</p> <p>The proposed building (fronting onto Croham Valley Road) has a higher ridge and eaves height (compared to 7 Croham Valley Road) this does not necessarily render the scheme unacceptable.</p>
<p>Neighbour Amenity</p>	
<p>Loss of privacy</p> <p>Query over the boundary position between the site and 7 Croham Valley Road – with the development affecting the existing boundary condition – detrimental to the neighbour’s residential amenity.</p>	<p>The degree of overlooking would be within acceptable limits, in view of the changes in topography and the angled nature of surrounding properties.</p> <p>The immediate neighbour raised concern about the future of an existing Laurel hedge, in place on the boundary with 5 and 7 Croham Valley Road. The application has subsequently been modified – setting back a proposed retaining wall off this boundary. Officers are now satisfied that there should be sufficient growing medium to ensure that the Laurel hedge is suitably protected and maintained in the future. Planning conditions are recommended to deliver high quality landscaping along with the retention of the Laurel hedge.</p> <p>The proposed building would be sited north of due west and therefore whilst it is appreciated that there will be a loss of late afternoon sun into the rear garden attached to 7 Croham Valley Road, BRE Guidance advises that under such circumstances, sunlight cannot be relied upon.</p>
<p>Highway Impacts and Car Parking</p>	

<p>Ballards Farm Road is a private road and the development would increase costs for owners of the road.</p> <p>Inadequate space for turning Inadequate parking provision resulting in increased on-street parking pressure</p> <p>Vehicles manoeuvring and parking in Croham Valley Road will cause serious highway danger Vehicles parked on Croham Valley Road will impede the safe passage for buses</p> <p>No scope of overspill parking on Ballards Farm Road</p> <p>Ballards Farm Road is not suitable for extra traffic as it's a single track lane</p> <p>How will construction vehicles access Ballards Farm Road without causing damage?</p>	<p>There are no changes to existing access arrangements off Ballards Farm Road and officers are satisfied that turning on site (following submission of amended drawings) is adequate to allow vehicles to exit parking areas in forward gear</p> <p>The level of car parking (66%) might well lead to some overspill onto the public highway – although officers are satisfied that there is spare capacity on street (excluding Ballards Farm Road and Croham Valley Road) to manage this effect (even after taking into account cumulative impact). It is recommended that contributions be made to ensure that any highway impacts are satisfactorily managed, alongside a contribution towards sustainable transport measures (including opportunities for a localised car club operation).</p> <p>The scheme would be subject to the agreement of a construction logistics plan – which will manage how demolition, ensuring that construction activities can safely take place on site – with areas set aside for delivery of materials to site (including loading and unloading) and a secure site compound.</p>
<p>Impact of Excavation</p>	
<p>Significant excavation required, concern over flooding, vegetation and ecological impact</p>	<p>The level of excavation is likely to be relatively extensive (especially towards the bottom of the site – fronting onto Croham Valley Road). The scheme would be expected to accommodate sustainable drainage measures (with water attenuation likely within car parking areas) in order to manage any drainage impacts.</p>

<p>Significant level changes and retaining walls will increase flood risk</p> <p>Considerable excavation is required which led to concerns regarding land stability</p>	<p>Details of any retaining walls and the effect of the scale of excavation would be a matter for the developer to satisfy neighbours (through party wall negotiations) – with such matters being a private issue between neighbouring landowners.</p> <p>The proposals have been supported by a Flood Risk Assessment and mitigation (in terms of surface water drainage and ground water flood risk) has been covered through the use of planning conditions</p> <p>The site is not included in an area of importance for nature conservation with the site being a well maintained/manicured residential garden.</p>
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4.3 Croham Valley Resident’s Association has objected to the proposal (referring the matter to Planning Committee). Their concerns are listed below:

- Significant increase in the scale of development and in terms of occupancy, is likely to be higher compared to the previously refused scheme
- Scheme would have an over-bearing impact on the neighbouring occupier at 7 Croham Valley Road
- The proposed development would be totally out of character with the immediate areas – characterised by detached houses –with the eaves and ridge heights being substantially higher than neighbouring houses
- Lack of car parking. There should be space for 6 parking spaces on site as a minimum – and there will be overspill parking onto Croham valley Road which is on a bus route
- Loss of privacy with overlooking to neighbouring properties
- Need to take into account other proposed developments in the area and especially the proposals for 3 Croham Valley Road
- Extent of excavation and construction of retaining walls which will significantly affect the water table and surface water flooding

4.4 Councillor Neal has objected to the scheme (referring the matter to Planning Committee) raising the following issues:

- Overdevelopment, out of character in size and design
- Inadequate parking
- Overbearing to other residents

5 RELEVANT PLANNING POLICIES AND GUIDANCE

5.1 In determining any planning application, the Council is required to have regard to the provisions of its development plan so far as is material to the application and to any other material considerations; the determination shall be made in accordance with the plan unless material considerations indicate otherwise. The Council's adopted development plan consists of the Consolidated London Plan 2015, the Croydon Local Plan 2018 (CLP) and the South London Waste Plan 2012.

5.2 Government Guidance is contained in the National Planning Policy Framework (2019), The NPPF sets out a presumption in favour of sustainable development, requiring that development which accords with an up-to-date local plan should be approved without delay. The NPPF identifies a number of key issues for the delivery of sustainable development, those most relevant to this case are:

- Requiring good design.
- Permission should be refused for development of poor design that fails to take the opportunities available for improving the character and quality of an area and the way it functions

5.3 The main policy considerations raised by the application are:

Consolidated London Plan 2015 (LP):

- 3.3 Increasing housing supply
- 3.4 Optimising housing potential
- 3.5 Quality and design of housing developments
- 3.8 Housing choice
- 3.9 Mixed and balanced communities
- 5.2 Minimising carbon dioxide emissions
- 5.3 Sustainable design and construction
- 5.13 Sustainable drainage
- 6.9 Cycling
- 6.11 Smoothing traffic flow and tackling congestion
- 6.13 Parking
- 7.2 Designing out crime
- 7.4 Local Character
- 7.6 Architecture
- 7.14 Improving air quality
- 7.19 Biodiversity and access to nature
- 7.21 Trees and woodland

Emerging New London Plan

5.4 Whilst the emerging New London Plan is a material consideration, the weight to be afforded is down to the decision maker, linked to the stage the Plan has reached in

its development. The Plan appears to be close to adoption. The Mayor's Intend to Publish version of the New London Plan is currently with the Secretary of State and at the time of drafting this report, no response had been submitted to the Mayor. Therefore, the New London Plan's weight has increased following on from the publication of the Panel Report and the London Mayor's publication of the Intend to Publish New London Plan. The Planning Inspectors' Panel Report accepted the need for London to deliver 66,000 new homes per annum (significantly higher than existing adopted targets) but questioned the London Plan's ability to deliver the level of housing predicted on "small sites" with insufficient evidence having been presented to the Examination to give confidence that the targets were realistic and/or achievable. This conclusion resulted in the Panel Report recommending a reduction in London's and Croydon's "small sites" target.

- 5.5 The Intend to Publish version of the New London Plan has accepted the reduction of Croydon's overall 10 year net housing figures - from 29,490 to 20,790 homes, with the "small sites" target reduced from 15,110 to 6,470 homes. Crucially, the lower windfall housing target for Croydon (641 homes a year) is not dissimilar to but slightly higher than the current adopted 2018 Croydon Local Plan target of 592 homes set for windfall sites each year.
- 5.6 Should the Secretary of State support the Intend to Publish New London Plan, the overall housing target in the New London Plan would be 2,079 new homes per annum (2019 – 2029) compared with 1,645 in the Croydon Local Plan 2018. Therefore, even with the possible reduction in the overall New London Plan housing targets (assuming it is adopted) Croydon will be required to deliver more new homes than our current Croydon Local Plan 2018 and current London Plan (incorporating alterations 2016) targets.
- 5.7 For clarity, the Croydon Local Plan 2018, current London Plan (incorporating alterations 2016) and South London Waste Plan 2012 remain the primary consideration when determining planning applications.

Croydon Local Plan 2018 (CLP 2018):

- SP2 on homes
- SP4 on urban design and local character
- SP6 on environment and climate change
- SP8 on transport and communications
- DM1 on housing choice for sustainable communities
- DM10 on design and character
- DM13 on refuse and recycling
- DM16 on promoting healthy communities
- DM19 on promoting and protecting healthy communities
- DM23 on development and construction
- DM24 on land contamination
- DM25 on sustainable drainage systems and reducing flood risk

- DM27 on biodiversity
- DM28 on trees
- DM29 on promoting sustainable travel and reducing congestion
- DM30 on car and cycle parking in new development

5.8 The relevant Supplementary Planning Guidance is as follows:

- London Housing SPG (March 2016)
- The Nationally Described Space Standards (October 2015)
- Croydon Suburban Design Guide Supplementary Planning Document April 2019

6 MATERIAL PLANNING CONSIDERATIONS

The main planning issues raised by the application that the Planning Committee is required to consider are as follows:

- Principle of development
- Townscape and visual impact
- Residential amenity
- Living conditions of future occupiers
- Parking and highway safety
- Flood risk and sustainability
- Trees and biodiversity
- Other planning matters

Principle of Development

- 6.1 The London Plan and Croydon Local Plan identify appropriate use of land as a material consideration, to ensure that opportunities for development are recognised and housing supply optimised. It is acknowledged that windfall schemes, which provide sensitive renewal and intensification of existing residential areas, play an important role in meeting demand for housing across London, helping to address overcrowding and affordability issues. The impact of the emerging London Plan on this policy approach is set out above.
- 6.2 Policies seek to prevent the net loss of 3 bedroom homes (as originally built) or homes less than 130m² and ensure that 30% of new homes are suitably sized for families.
- 6.3 The site is located within an existing residential area. The application proposes two buildings (each containing 3 family houses) which would provide additional family homes within the borough, which the Council is seeking to promote. Therefore, whilst the existing house is protected, as a three-bed home there would be a significant uplift in family accommodation, helping meet the strategic objective of 30% of all new homes being for families.

- 6.4 A number of residents have argued that the proposed development would constitute over-development of the site – in excess of density guidelines. The London Plan density matrix (where suburban PTALs are in the region of 2-3) suggests a density range of 150-250 habitable rooms per hectare and 35-65 units per hectare. In this case, the density of development would equate to around 300 habitable rooms per hectare and 67 units per hectare. Whilst this exceeds density ranges (in terms of habitable rooms) it is very close to the top end of the range (in terms of number of units) and in any case, consideration of the density matrix over the years has become a lesser consideration with London Boroughs expected to thoroughly test the elements that help inform the appropriateness of a particular density (neighbour impact, residential quality - including landscaping and amenity space, highway considerations, car parking and design quality). In any case, the emerging London Plan, which is a material consideration, is seeking to remove reference to the density matrix (which was accepted by the Panel Report – mentioned earlier in this report). In short, officers feel that this proposal sits well within its context and properly mitigates the various impacts.

Townscape and Visual Impact

- 6.5 This part of Croham Valley Road is mostly characterised by large detached dwellings with traditional forms of two storeys with pitched roofs. In more recent times, infill has occurred on the northern side of Croham Valley Road. More recently, a flatted development (10 Croham Valley Road – LBC Ref 17/06096/RSM) has received planning permission and is nearing completion.
- 6.6 Ballards Farm Road differs in character from Croham Valley Road; being a single track access lane with a far more rural appearance. The road has landscaped verges with no dedicated footpaths with heavily treed/landscaped site boundaries. There are detached outbuildings to the front of the buildings/dwellings with some detached garages. Whilst these structures are traditionally designed, they all have differing appearances and relationships with the public realm.
- 6.7 Croydon Local Plan (CLP) (Policy DM10.1) states that proposals should seek to achieve a minimum height of 3 storeys. The Suburban Design Guide (SDG) suggests appropriate ways of accommodating intensified use of sites in different scenarios. Where surrounding buildings are predominantly detached dwellings of two (2) or more storeys, new developments may be three (3) storeys with an additional floor contained within the roof space or set back from the building envelope below.



Figure 2.10c: Where surrounding buildings are predominantly detached dwellings of two (2) or more storeys, new developments may be three (3) storeys with an additional floor contained within the roof space or set back from the building envelope below.

- 6.8 In accordance with the abovementioned policy and guidance, the proposed building fronting onto Croham Valley Road comprises three full storeys (one partly set below the level of the highway) with accommodation contained in the roof space. Whilst it is accepted that the scheme would be a larger development compared to the adjacent dwelling at 7 Croham Valley Road (with eaves and ridge lines being higher) the proposed building would have a traditional form with a half hipped and pitched roofs, respecting the general character of built form in the area. By reason of the careful levelling of the building, the eaves height of the proposed new building would be approximately 0.5 metres above the neighbouring eaves height (7 Croham Valley Road) and an increase of around 1 metre at ridge height. Officers are satisfied that this variation is acceptable in this particular instance bearing in mind the general variation in built form. The SDG also advises that developments need not necessarily step down in height towards neighbouring buildings of a lesser height.
- 6.9 Whilst it is appreciated that the scheme proposes a residential terrace rather than a single detached house, the footprint of the composite terrace would be similar to the building footplates found elsewhere in Croham Valley Road. The scheme works and responds effectively to the changing ground levels – albeit with the need for retaining walls and a terraced rear garden arrangement (similar to the arrangements found at 7 Croham Valley Road). Landscaping, boundary treatment and the details of retaining walls would need to be considered in detail as part of the planning conditions discharge process – to ensure that the scheme responds well to its surrounding context.
- 6.10 The western side element would be visible within the street scene along Croham Valley Road. The building would have greater depth (compared to 7 Croham Valley Road) but would only exceed the depth of this neighbour by approximately 3.5 metres (taken from the neighbouring rear garage wall) and 2.5 metres from the main rear elevation of this neighbouring house. The planning application for 3 Croham Valley Road remains under consideration although officers are comfortable with the appearance of the side flank elevation of the proposed development which in any case, would be partially masked should redevelopment of 3 Croham Valley Road comes forward in the future. Overall, the building would not appear overly deep or bulky.

- 6.11 The building is proposed to have traditional external materials with stock brick under a plain tiled roof. The proposed materials are commonly found in the area and should help the development to respect its setting and surroundings. The scheme would involve extensive excavation at the foot of the slope with terraced gardens to the rear which mirrors the arrangement in place at 7 Croham Valley Road (which arose out of the previous subdivision of 7 Ballards Farm Road a few years ago)
- 6.12 A vehicle crossover and elements of hardstanding would be provided – accessed off Croham Valley Road. Access driveways and forecourt parking are features found along Croham Valley Road and there is no objection to these interventions from a visual viewpoint. Sufficient space should be accommodated on site for two car parking spaces (with easy manoeuvring on site) and space made available for bikes and bin storage, replacement tree planting and visibility splays. Space would be available within this area to accommodate these required facilities without resulting in an overly cluttered appearance.



Figure 4 Elevations of Croham Valley Road Frontage Building and CGI

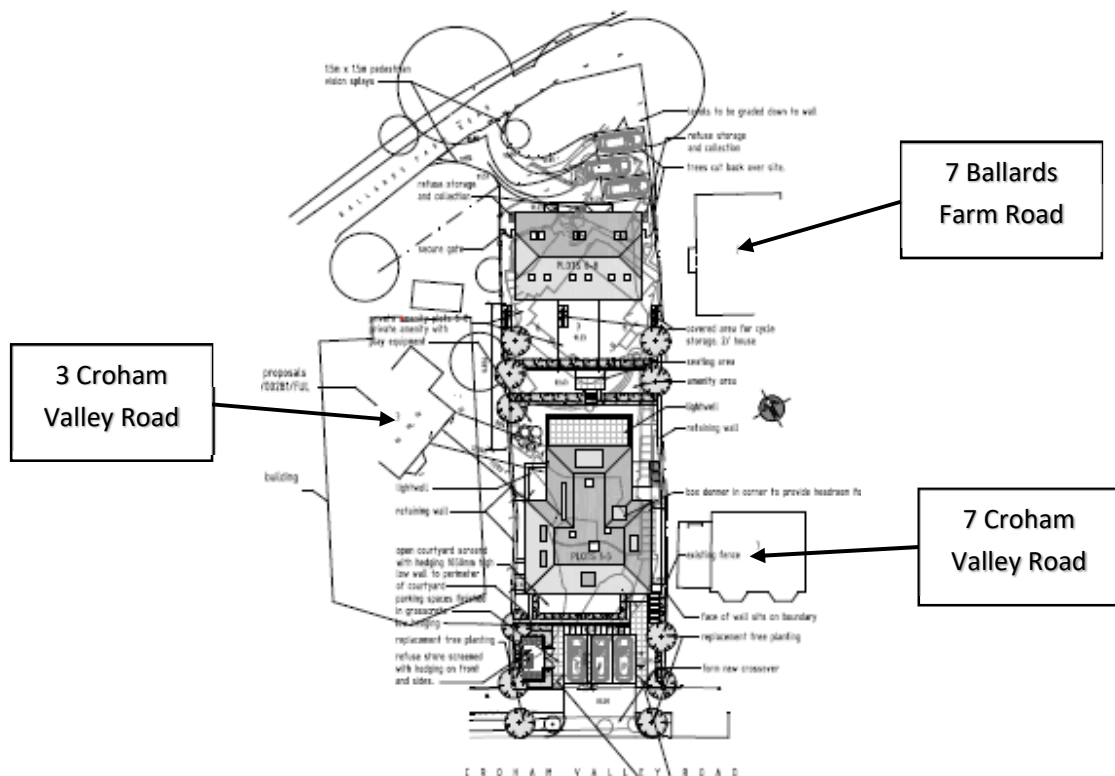
- 6.13 The same policy and guidance within the SDG apply would apply to the proposed development fronting onto Ballards Farm Road. The scale of development (being located at the top of the slope and on more level ground) would present itself as a part two/part 3 storey terrace with accommodation in the roof void with side dormers and gable features. Policy DM10 advises that development within rear gardens should be subservient to the existing property on site and this approach is followed through in the Suburban Design Guide. Officers feel that this approach is not especially relevant in this particular case as the existing house (situated at the top

Impact on Neighbouring Residential Amenity

6.16 The properties most affected by the development would be the immediate neighbours (3 and 7 Croham Valley Road and 7 Ballards Farm Road).

3 Croham Valley Road

6.17 3 Croham Valley Road is situated to the north-west of the application site, towards the middle of the plot, and the dwelling is angled so that the rear windows face into the garden that slopes down towards Croham Valley Road. The ridge height of the proposed houses fronting onto Croham Valley Road would roughly align with the eaves line of this neighbouring property and as such, the proposed rear facing windows would be at a lower level and angled away from the rear elevation of this neighbouring property. The windows in the side elevation of the proposed building are shown to be obscurely glazed and are also at an angle to the neighbouring house.



6.18 Whilst the building would be readily visible from and impact on views enjoyed by the residents of 3 Croham Valley Road (with a minimum separation of 10 metres) given the change in levels and angled relationship, the proposed building would not be

unduly intrusive or cause any harmful loss of light. 3 Croham Valley Road would still enjoy an open outlook to the south.

- 6.19 The terrace fronting onto Ballards Farm Road would be located adjacent to the parking area (attached to 3 Croham Valley Road) and would be separated by established boundary planting. The proposed building would be over 10 metres from the closest point to 3 Croham Valley Road and would be situated (again) at an oblique angle. Windows in the side elevations of the proposed building would again serve bathrooms and would be obscurely glazed and should therefore respect privacy. The rear/south facing windows would again be positioned at an oblique angle to 3 Croham Valley Road. Some windows are located in the side elevation of 3 Croham Valley Road. The SDG sets out that little protection is afforded to side facing windows in these circumstances. Given the orientation of the buildings and angled relationship/gap, the proposal would respect light and outlook.
- 6.20 A planning application (LBC Ref 18/06067/FUL) has been submitted (yet to be determined) for the redevelopment of 3 Croham Valley Road involving the demolition of the house and redevelopment to form 8 flats. The proposed development proposes a contemporary response to appearance and layout, with the development cascading down the slope. All proposed units would enjoy a southerly aspect with no proposed side windows. Officers are satisfied that the current proposal for 5 Croham Valley Road would not prejudice the redevelopment potential of 3 Croham Valley Road and any subsequent determination of the associated planning application. It is hoped that the proposals for 3 Croham Valley Road will be ready to present to Planning Committee in the next month or so. .

7 Croham Valley Road

- 6.21 This dwelling is an infill development that was granted planning permission in 2015 (on appeal). It has an attached garage situated adjacent to the boundary with a bathroom above. The previous 2019 refused scheme raised significant concerns around impact on this immediate neighbour; the previously proposed block of flats were shown to project 7 metres beyond the rear elevation of this neighbouring property which would have been overly intrusive and over-bearing.
- 6.22 The current proposal seeks to deal with this previous issue and in comparison to the previously refused scheme, the proposed building has now been reduced in depth (by 4.7 metres). However, following on from the removal of the previously stepped projection and the introduction of a revised built form, the north eastern corner of the proposed building would now be sited slightly closer to 7 Croham Valley Road. That said, the proposed building would not encroach into the 45 degree angle (highlighted in the SPD) taken from any main habitable window. Moreover, the relationship with the bathroom window of 7 Croham Valley Road (over the side garage) would also be acceptable. The ground floor level to 7 Croham Valley Road, is set down into the rear garden with the patio already enclosed by a relatively high retaining wall which in itself, encloses the rear patio and restricts light from entering the ground floor rooms (which in any event is dual aspect). Consequently, given the

significant reduction in depth and taking into account SPD advice, the amenities of this immediate neighbour (in terms of daylight, sunlight, outlook and enclosure) would be acceptable.

- 6.23 The rear garden (associated with to 7 Croham Valley Road) is tiered – with an overall depth of around 7 metres. Existing ground floor accommodation has only limited outlook (in view of the change in level within the rear garden) with steps leading to the grassed upper terrace, which is the only area which receives mid/late afternoon sun. The proposed building would extend approximately 2.5 metres beyond the rear elevation of this neighbouring property and part of the upper level and roof would be visible from the garden

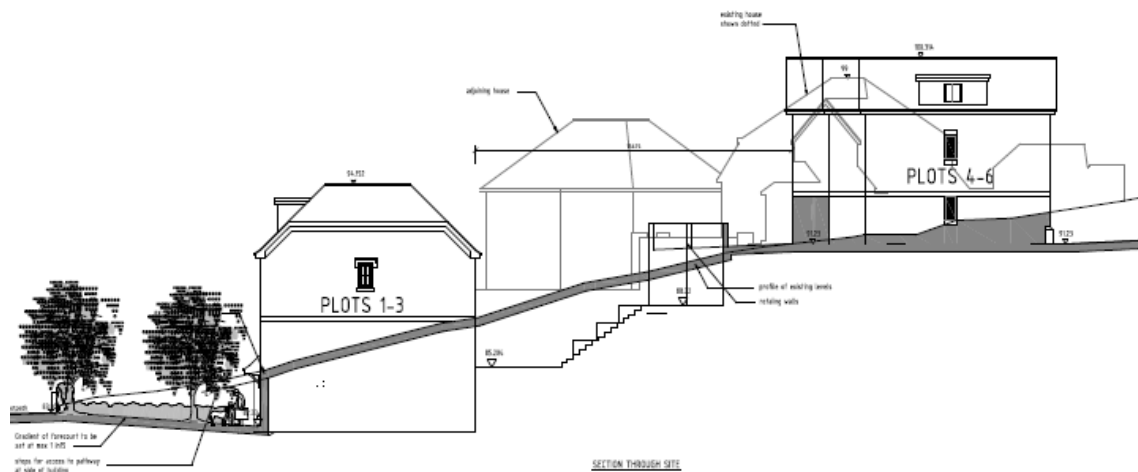


Figure 6 Section through the Site – Viewed from 7 Croham Valley Road and 7 Ballards Farm Road

- 6.24 This neighbour has raised a number of concerns as regards the proposed development but specifically the extent of excavation required close to their boundary and the details of any retaining wall between the properties; concerns over the future of an existing Laurel hedge which currently forms a verdant boundary between the two properties. In response to these concerns, the scheme has been modified by setting back the retaining wall off the existing site boundary, which should provide sufficient space for the existing hedge to flourish – whilst still providing a landscaped boundary between the two properties. A small portion of the retaining wall might well be visible (albeit set back from the common boundary and behind the existing Laurel hedge) but officers are satisfied that the amendment helps to overcome this valid concern.

- 6.25 The terrace fronting onto Ballards Farm Road would be located over 20m from the rear elevation of 7 Croham Valley Road and given the site layouts, levels, orientation and distance, this element of the proposal would not cause any harmful loss of light, outlook or privacy.
- 6.26 The owners of 7 Croham Valley Road dispute the location of the eastern side boundary as shown on the submitted plans; arguing that the boundary does not align with the boundary that was granted planning permission back in 2015 or the subsequent plans submitted as part of the subsequent conditions discharge process. The applicant has been made aware of this apparent disparity and apart from modifying the location of the retaining wall, has not amended the plans and/or served notice on the neighbouring owners. That said, this remains a private matter between parties and does not represent a reason to refuse planning permission.

7 Ballards Farm Road

- 6.27 This property is located to the north east of the site and has a number of side facing windows (lighting day rooms, bedrooms, an en-suite bathroom and a study) with a heavy tree screen along the western boundary. The existing dwelling and garage is already sited in close proximity to the site boundary.
- 6.28 Whilst at some points, the proposed terrace would be closer to the boundary compared to the existing building, given the tree screen, it is not considered that the proposed building would cause any harmful loss of light or outlook. Apart from the study window, all other side windows are secondary (with rooms having dual aspect) or serve bathrooms and the proposed building would not extend beyond the front or rear elevation of this neighbouring property. The proposed side windows would serve bathrooms or stair cores and would be conditioned to be obscure-glazed should planning permission be forthcoming.
- 6.29 The rear most element of the proposed block fronting Croham Valley Road would be 15 metres from the rear elevation of 7 Ballards Farm Road but would be situated at a significantly lower ground level – which would provide appropriate levels of mitigation.
- 6.30 In view of the sloping topography and the challenging rear garden levels, it is recommended that permitted development be limited to allow the local planning authority to further consider the effect of householder extensions and any rear outbuildings.

The Standard of Accommodation for Future Occupiers

- 6.31 All of the proposed residential units would meet the Nationally Described Space Standards (NDSS).
- 6.32 The units in the block facing onto Croham Valley Road would be single aspect (at lower ground floor level) due to the levels of the site and the desire to limit the extent of excavation. However, all of the upper floors of the building would benefit from

dual or triple aspect and would therefore have a satisfactory outlook. The lower-ground floor is shown as a bedroom with a front facing window. There would be some space available in the front forecourt to provide some defensible planting – to maintain some degree of privacy for these bedrooms.

- 6.33 All houses would benefit from private gardens to the rear. In view of the sloping nature of the site, the gardens would have to be terraced with an initial level area leading from the upper ground floor level – stepping up to a higher level garden area, enclosed by the retaining wall (highlighted above). Overall, there would be sufficient useable space within the garden. In addition, the stepping of the garden levels and the height of the retaining walls in relation to the ground floor windows of the houses would be sufficiently shallow/low to ensure that the outlook would not be unduly obstructed.
- 6.34 The terraced dwellings to the north of the site would be either dual or triple aspect with private rear gardens of adequate size and privacy. Overall, all of the proposed units would be of an acceptable quality and would benefit from sufficient private amenity space. In terms of accessibility, the block fronting Croham Valley Road could provide level access through the front door.
- 6.35 London Plan Policy 3.8 'Housing Choice' requires 90% of dwellings to meet M4(2) 'accessible and adaptable dwellings' building regulations requirement. Full compliance with this can be difficult to secure with steeply sloping sites and the London Plan does recognise that full compliance can be difficult to secure in various circumstances. In this case, the houses proposed onto Croham Valley Road have the option to install a lift (between the lower ground floor bedrooms and the upper floor main reception room – to enable a wheelchair occupier to move between the lower ground and upper ground levels – and into the lower terraces of the rear garden areas. This would allow for some elements of Part M4(2). Similarly, the houses fronting onto Ballards Farm Road would have level access at ground floor level – with a reasonably sized ground floor WC and access into the rear garden which should be sufficient to comply with M4(1).
- 6.36 Overall, officers are satisfied that the proposals would result in a good standard of accommodation for future occupiers.

Parking and Highways

- 6.37 The site has a PTAL rating of 2 and is served by two bus services (64 and 433 bus routes); linking Thornton Heath Pond with New Addington via Selsdon and Central Croydon and linking Addington Village with East and West Croydon Stations.
- 6.38 The applicants have submitted a detailed transport statement alongside a parking stress survey which has considered overnight on-street parking availability – whilst taking into account cumulative impact should all current applications (highlighted above) be granted planning permission.

- 6.39 The London Plan sets out that the maximum car parking standards for residential developments – which are based on public transport accessibility levels and local character. It states that 1-2 bedroom properties should provide a maximum of up to 1 space per unit, with up to 1.5 spaces per unit being provided for 3 bedroom properties. Based upon this ratio and the unit mix proposed, a maximum of 8 parking spaces should be provided. However, using 2011 Census data the proposed development is likely to generate demand for around 5 car parking spaces.
- 6.40 Officers have reviewed all current Croham Valley Road applications and commented accordingly. Officers are satisfied that there is adequate capacity on street to accommodate any overspill car parking; up to 4 car parking spaces (as worst case) might need to be accommodated on-street and the applicant has identified spare capacity on Croham Road and Manor Way (in close proximity of the application site) – with spare capacity for around 30 cars. Officers concur that there is spare capacity. Other developments proposed in and around Croham Valley Road have on site car parking capacity (8 car parking spaces for 9 flats in the case of 6 Croham Valley Road and 7 car parking spaces for 8 flats in the case of land to the R/O 31-33 Croham Valley Road) and even with these other sites, there should be capacity for additional on-street car parking should the need arise.
- 6.41 Policy SP8 and DM29 seeks to manage use of the private car and promote sustainable travel. DM30 requires a car club space to be provided on nine unit schemes, where there is likely to be interest from an operator. Whilst there is sufficient parking on site and on street to ensure that the impact on the network is satisfactory, sustainable travel should still be promoted in accordance with these policies. Therefore, a contribution is recommended, and has been agreed with the applicant, towards the provision of sustainable travel measures, most likely to be traffic management measures on Croham Valley Road (to ensure that the bus route is not affected by any overspill parking) and the provision of a car club space in the local area, which will help mitigate overspill parking as well as encouraging sustainable travel. Similar arrangements have been agreed with the developers of 6 Croham Valley Road and the R/O 31-33 Croham Valley Road. Taking into account the site's accessibility to public transport, relevant car ownership data and capacity for on-street are parking to accommodate any overspill, the proposal would provide for an appropriate number of parking spaces which would not detrimentally impact highway safety within the surrounding area.
- 6.42 There is scope to provide the required visibility splays and vehicles should be able to turn on site and enter and exit the car parking bays (from both Croham Valley Road and Ballards Farm Road) in forward gear. Moreover, the degree of change at the entrance of Ballards Farm Road would be minimal. There are some staked street trees/bushes (in Croham Valley Road) that would need to be removed and replaced and this could be agreed when the crossover works are submitted for consideration by the highway authority. The costs of tree replacement would be secured through the S.106 Agreement. The change in the number of vehicles using Ballards Farm Road would be relatively minimal and existing highway conditions would be suitably maintained.

- 6.43 It is recommended that a Demolition, Construction Logistics and Environmental Management Plan will be required by a condition before commencement of work, given the site's location in a residential area and to deliver a managed solution to highway impact associated with the demolition and construction process.

Cycle and Refuse Storage

- 6.44 The proposed cycle stores are proposed within the forecourt areas along with refuse storage areas. The location of these facilities are considered acceptable (for storage and collection) although detailed design of the various enclosures would need to be controlled and approved through the use of planning conditions to be discharged at a later date.

Trees and Biodiversity

- 6.45 The site is not covered by any tree preservation orders and there are no trees of any particular merit on the site although the importance of the existing hedge boundary is acknowledged. The plans show some replacement landscaping although full details of hard and soft landscaping, including boundary treatment would be secured by condition should planning permission be forthcoming. There is a protected tree within the front garden of 7 Ballards Farm Road – but the proposed development would not interfere with this protected tree.
- 6.46 As raised above, there is an immature street tree to the front of the site on Croham Valley Road and some shrubs that would need to be removed to facilitate the new vehicular access. Due to the young age of the tree currently in situ the Trees and Woodland Officer has no objection to its removal, provided at least another two street trees of a suitable approved species are planted post development works and the costs of supply, planting and maintaining (regular watering, weeding and tie adjustments) for a minimum two years post works are fully covered by the developer. There is no objection to the removal of the shrubs. These arrangements can be agreed with the highway authority when the crossover is installed.
- 6.47 Whilst there is no reason to believe that any protected species would be affected by the proposed development, an informative should be included on any decision making the applicant aware that it is an offence to harm protected species or their habitat and in the event that protected species are found on site the applicant should refer to Natural England standing advice.

Environment and Sustainability

- 6.48 Conditions can be attached to ensure that a 19% reduction in CO2 emissions over 2013 Building Regulations is achieved and mains water consumption would meet a target of 110 litres or less per head per day.
- 6.49 The site is located within an area some risk of surface water flooding and potential for groundwater flooding to occur. Policy DM25 requires all development to incorporate sustainable drainage measures (SuDS). A flood risk assessment was

submitted as part of the planning application and in many ways the re-profiling of the land/terracing might well allow for reduced surface water flows and might well provide capacity for water attenuation and sustainable urban drainage measures (especially within the car parking and garden areas). Given the location of part of the development at the bottom of a slope, partially dug in, there is some risk from groundwater flooding. The proposed mitigation of raising floor levels by 0.15m internally and installing non-return valves and covers to air bricks would suitably ameliorate this risk and a condition is recommended to secure full details

- 6.50 A planning condition is recommended requiring site specific SuDS measures as well as groundwater flood risk resistance and resilience measures.

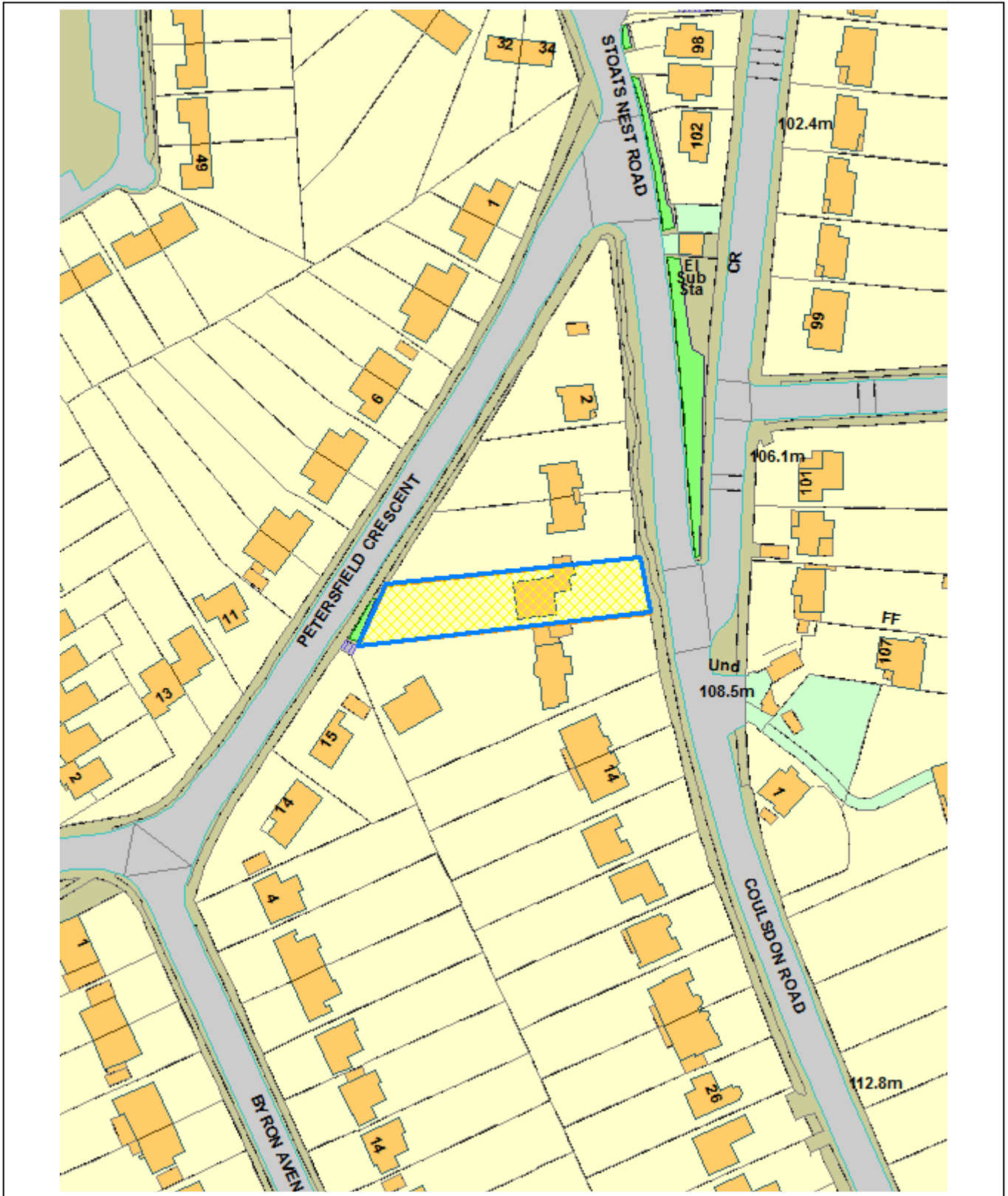
Other Planning Matters

- 6.51 The development would be liable for a charge under the Community Infrastructure Levy (CIL). This payment would contribute to delivering infrastructure to support the development of the area, such as local schools.

Conclusions and the Planning Balance

- 6.52 This scheme seeks to make optimum use of the site, delivering a net gain in the number of family houses – contributing not only to housing targets but also the strategic 30% target for new family accommodation. The PTAL is relatively low and there is likely to be some pressure being placed on existing on street car parking conditions, but there is capacity on street to accommodate additional car parking demand associated with this development and other schemes currently in the development pipeline. The developer has accepted the need to work with the Council to encourage more sustainable forms of transport – as an alternative to the private car. The appearance of the site and the immediate area will change and evolve, but there is nothing intrinsically wrong with the form of development proposed, with buildings proposed fronting onto Ballards Farm Road as well as Croham Valley Road. The proposed houses would all comply with Nationally Prescribed Housing Standards and the impact of development on immediate neighbours would be acceptable

- 5.53 Taking all of the above planning considerations into account, the application is recommended for approval.



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1.0 SUMMARY OF APPLICATION DETAILS

Ref: 19/03965/FUL
 Location: 8 Coulsdon Road, Coulsdon, CR5 2LA
 Ward: Old Coulsdon
 Description: Demolition of the existing property and erection of six new apartments and 2 houses (houses to front Petersfield Crescent), with associated new access, parking, refuse and cycle stores and landscaping.
 Drawing Nos: 33-P2-3, 33-P2-4 Rev O, 33-P2-5 Rev E, 33-P2-6 Rev G, 33-P2-7 Rev D, 33-P2-8 Rev F, 33-P2-9 Rev G, 33-P2-10 Rev G, 33-P2-11 Rev K, 33-P2-12 Rev F, 33-P2-14 Rev A and 33-P2-16 Rev D
 Applicant: Sterling Rose
 Agent: Sterling Rose
 Case Officer: Samantha Dixon

	1 bed	2 bed	3 bed	4 bed	5 bed
Existing			1		
Proposed flats	3	3	2		

All units are proposed for private sale

Number of car parking spaces	Number of cycle parking spaces
4	14

1.1 This application is being reported to committee because the ward councillor (Councillor Margaret Bird) and Hartley & District Resident’s Association have made representations in accordance with the Committee Consideration Criteria and requested committee consideration and objections above the threshold in the Committee Consideration Criteria have been received.

2.0 RECOMMENDATION

2.3 That the Director of Planning and Strategic Transport has delegated authority to issue the planning permission and impose conditions and informatives to secure the following matters:

Conditions

1. Time limit of 3 years
2. Development to be carried out in accordance with the approved drawings and reports except where specified by conditions
3. Construction Logistics Plan to be submitted
4. Details of site specific SuDS to be submitted
5. Protection measures for retained hedge and street trees to be submitted
6. Submission of Drainage Strategy as required by Thames Water
7. Details of materials to be submitted

8. Hard and soft landscaping including boundary treatment, retaining walls and maintenance to be submitted
9. Details of electric vehicle charging point to be submitted
10. Details of children's playspace to be provided
11. Accessible units to be provided
12. Accesses to be provided and existing reinstated prior to occupation
13. Car and cycle parking provided as specified
14. Obscured glazing to flank windows
15. No other openings in flank elevations
16. 19% Carbon reduction
17. 110litre Water usage
18. Any other planning condition(s) considered necessary by the Director of Planning and Strategic Transport

Informatives

- 1) Community Infrastructure Levy
- 2) Code of practise for Construction Sites
- 3) Ecology consideration
- 4) Highway works
- 5) Accessible units
- 6) Any other informative(s) considered necessary by the Director of Planning and Strategic Transport

2.4 That the Committee confirms that adequate provision has been made by the imposition of conditions, for the preservation or planting of trees as required by Section 197 of the Town and Country Planning Act 1990.

3.0 PROPOSAL AND LOCATION DETAILS

3.1 The proposal includes the following:

- Demolition of existing house
- Erection of a three storey building with accommodation in roof to create 6 residential units (3 x 1 bedroom and 3 x 2 bedroom units) with provision of communal external amenity space and children's play space
- Erection of 2 x two storey semi-detached houses with accommodation in roof space (3 bedrooms) with private gardens
- Provision of 4 off-street parking spaces
- Provision of associated refuse and cycle stores

3.2 During the course of the application amended plans have been received to: Alter the internal layout of the flatted development to ensure that all units have private amenity space and adequate outlook and access to light; repositioning of side dormer window to front elevation; Loss of lower ground floor of the dwellings fronting Petersfield Crescent and amended design; Relocation of bin stores; Additional information regarding land levels.

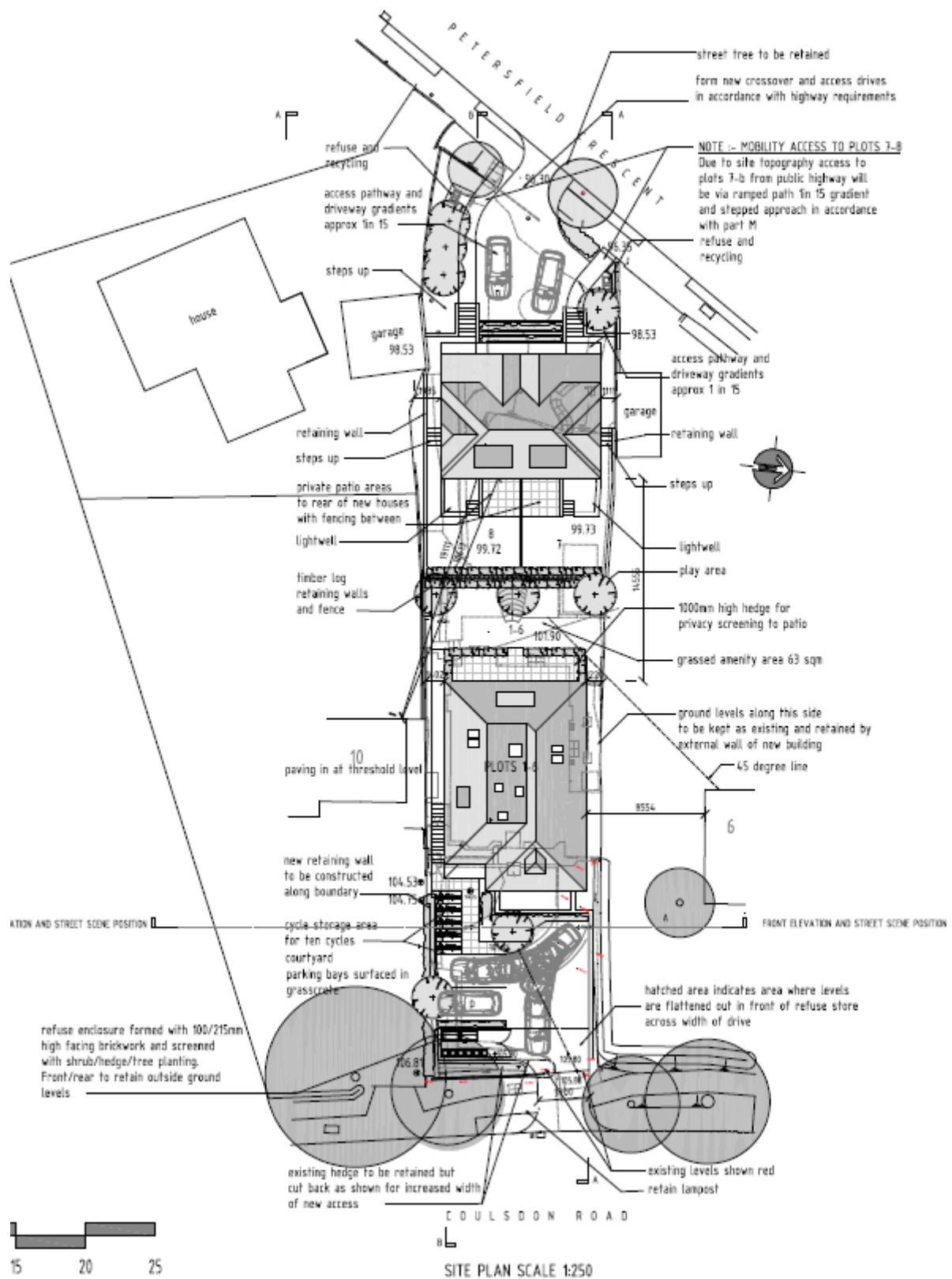


Figure 1 Proposed site plan

Site and Surroundings

- 3.3 The site comprises a single storey detached dwelling located to the west side of Coulsdon Road, opposite the junction with Hartley Down. The house sits at a lower level than the highway and the levels across the site fall significantly from east to west.

The dwelling has a large rear garden of approximately 400sqm in size. The rear garden of the property backs onto Petersfield Crescent and there is an existing dropped kerb from this road however it is noted that it is not in use.

- 3.4 This is a predominantly residential area with an array of dwelling types present. Dwellings on Coulsdon Road are detached and semi-detached and vary in appearance, being mostly two storey in height. Plot sizes are also very varied. Properties on the north west side of Petersfield Crescent are two-storey semi-detached all of a similar appearance and era. Detached infill dwellings have been erected to the south east side of the road, to the south of the proposed application site.
- 3.5 There are no specific policies relating directly to this site however it is noted that it is an area at low risk of surface water flooding and potential for groundwater flooding. The site has a PTAL of 2 indicating relatively poor access to public transport.



Figure 2 Aerial street view highlighting the proposed site within the surrounding streetscene

Planning History

- 3.5 82/01463/P Erection of garage in rear garden with access onto Petersfield Crescent. Granted 12.10.1982
- 3.6 97/00333/P Erection of single storey front extension and alterations to roof including provision of front and rear dormers. Granted 13.03.1997
- 3.7 18/05180/PRE New residential development to create 9 units. It was summarised that residential development of the site is acceptable in principle. Concerns were raised to the quality of amenity for the proposed flatted block, the appearance of the dwellings fronting Peterfield Crescent, inadequate highway information being provided.

3.8 Application at 2 Coulsdon Road 19/03003/FUL Demolition of existing dwelling and erection of new building to create 9 flats with associated car and cycle parking provision, refuse storage and landscaping. Granted permission by Planning Committee on 6th February 2020.

4.0 SUMMARY OF KEY REASONS FOR RECOMMENDATION

- The principle of the development is acceptable given the residential character of the surrounding area.
- The proposal creates three family sized units
- Amended plans have been received to ensure that the buildings respect the character of the surrounding area.
- The living conditions of adjoining occupiers would be protected from undue harm.
- The living standards of future occupiers are satisfactory and Nationally Described Space Standard (NDSS) compliant.
- The level of parking and impact upon highway safety and efficiency is considered acceptable and can be controlled through conditions.
- Sustainability aspects can be controlled by conditions.

5.0 CONSULTATION RESPONSE

5.1 The views of the Planning Service are expressed in the MATERIAL PLANNING CONSIDERATIONS section below.

Thames Water

5.2 With the information provided Thames Water has been unable to determine the waste water infrastructure needs of this application. Should the Local Planning Authority look to approve the application a condition should be attached requiring a drainage strategy detailing any on and/or off site drainage works to be submitted and approved with the local planning authority in consultation with the sewerage undertaker. [OFFICER COMMENT: A condition is recommended]

6.0 LOCAL REPRESENTATION

6.1 The application has been publicised by 13 letters of notification to neighbouring properties in the vicinity of the application site. The number of representations received from neighbours in response to notification and publicity of the application are as follows:

No of individual responses: Objecting: 60 Supporting: Comment: 0

6.2 The neighbours were renotified with regard to the amended plans and 9 objections (included in the total above) were received.

6.3 The following issues were raised in representations. Those that are material to the determination of the application, are addressed in substance in the MATERIAL PLANNING CONSIDERATIONS section of this report:

Objection	Officer comment

<i>Design and appearance</i>	
Overdevelopment of the site/high density	Addressed in Section 8.22 of this report.
Out of keeping with existing development in the area in terms of height and bulk. Three/four storey out of keeping on two storey area. Intrusive design.	Addressed in Section 8.8 – 8.23 of this report.
Petersfield Crescent is a small road with 16 houses and the new units will change the character entirely	Addressed in Sections 8.16 - 8.20 of this report.
Four storeys give the narrow semi-detached properties a townhouse look, which will appear cramped and incongruous with the prevailing streetscape.	The design of the proposed houses fronting Petersfield Crescent have been amended during the course of the application. Addressed in Sections 8.16 - 8.20 of this report.
Blocks of flats are out of keeping in the area, contrary to Policy DM37.	Planning policies and the Suburban Design Guide advocate infill development for new residential units in the suburbs. There is no objection to the principle of flatted development in this area.
<i>Impact on amenities of neighbouring properties</i>	
Overbearing impact on and loss of light and privacy to neighbouring properties	Addressed in Sections 8.34 - 8.50 of this report.
Extra pollution and noise disturbance	This is a residential development and there is no evidence or reason to suggest that the proposal would result in extra pollution or noise that is not associated with a residential area.
Loss of views	This is not a material planning consideration.
Inaccurate plans fail to present the position of adjacent dwellings	The plans have been amended to show the full extent of the adjacent dwellings.
May effect light to solar panels at 12 Petersfield Crescent	The closest part of the dwelling at No.12 is 32m from the front edge of the application site and located to the south west.
<i>Trees/Ecology/Environment</i>	
Loss of protected trees on Coulsdon Road to form the new access.	The trees on Coulsdon Road will not be affected by the development. Addressed in Sections 8.21 and 8.65 of this report.

Decimates the existing mature garden	Addressed in Section 8.21 of this report.
Loss of wildlife habitat	Addressed in Section 8.66 of this report.
Dramatically increase the carbon footprint of the area	Conditions will be imposed to ensure carbon emissions are compliant with policy and Building Regulations. Addressed in Section 8.62 of this report.
Overdevelopment of this nature contravenes the objective of maintaining air quality (DM23).	Conditions will be imposed to ensure carbon emissions are compliant with policy and Building Regulations. This is a residential development and there is no evidence or reason to suggest that the proposal would result in extra pollution or noise that is not associated with a residential area.
<i>Transport and parking</i>	
Inadequate parking provision will exacerbate parking problems on Petersfield Crescent. Increase parking problems due to new dropped kerb.	Addressed in Sections 8.51 – 8.55 of this report.
Lack of parking will cause cars to park on Coulsdon Road which would be dangerous. Adverse impact on highway safety and congestion. Exacerbate existing traffic problems at a busy and dangerous junction	Addressed in Section 8.53 of this report.
Dangerous access/egress. Cars reversing onto Petersfield Crescent is dangerous	Addressed in Section 8.59 of this report.
Increased traffic will cause more risk of accidents	Addressed in Section 8.58 – 8.59 of this report.
Traffic chaos during construction	A Constriction Logistics Plan will be required by condition.
Loss of parking will affect the shops on Coulsdon Road	The Transport Statement indicates that there is ample available on street parking in the surrounding streets and as such the proposal will not affect trade to the local shops. Additional local occupiers may be beneficial for trade.

Inadequate cycle and refuse storage. The cycle parking is not secure.	Addressed in Sections 8.57, 8.60 and 8.61 of this report. Full details of cycle storage will be secured by condition.
Given the hilly topography the site will not be attractive to cyclists. SDG 2.6.8 proposes the use of e-bikes in hilly areas. There is no indication that the proposed cycle racks contain charging points for e-bikes.	Cycle parking is shown to be provided in accordance with London Plan requirements.
No electric vehicle charging points proposed	This would be secured by condition
<i>Amenities of future occupiers</i>	
Too small, disproportionate sized gardens	Addressed in Section 8.27 of this report.
Not all flats have private amenity as required by policy and some are too small	The plans have been amended so that all units have private amenity areas, all of which accord with required standards. Addressed in Section 8.27 of this report.
The sunken courtyard to Flat 1 will provide unacceptable daylight	The courtyard to the front of Flat 1 has been removed from the scheme.
Inadequate playspace. Communal garden not accessible by wheelchair users	Addressed in Sections 8.28 and 8.30 of this report.
No affordable housing provision	This is a minor development and there is no policy requirement for affordable housing.
70% of houses should have 3 bedrooms. Doesn't accord with policy	The policy referred to is for major developments (10+ units). The proposal is compliant with Policy SP2.7 for family units. Addressed in Section 8.7 of this report.
Other matters	
Increase flood risk. Surface water flooding is already a problem the area	Policy DM25 requires all development to incorporate sustainable drainage techniques. A condition will be imposed requiring site specific SuDS to be provided. Addressed in Section 8.63 of this report
Pressure on existing sewerage system. A mains waterpipe runs under the site.	Addressed in Section 8.64 of this report

<p>It is not clear how the developer can excavate without disturbing the sewer (especially given the proposed changes to existing ground levels). The house at 16 Petersfield Crescent had to be re-sited on its plot due to the sewer location. There is no evidence of consultation with Thames Water regarding the location of sewers.</p>	
<p>Set precedence for other such developments in the area</p>	<p>There is no objection to the principle of infill residential development in this area. The proposal re-provides family housing in a residential area in accordance with Local Plan policy.</p>
<p>Multiple dwellings not allowed based on original 1932 deeds of the existing house</p>	<p>This is a private matter for the developer and is not a material planning consideration.</p>
<p>Extra strain on local services e.g. GPs and schools which are already unable to cope.</p>	<p>The application is CIL liable. Addressed in Section 8.67 of this report.</p>
<p>Devalue existing house prices</p>	<p>This is not a material planning consideration.</p>
<p>Cumulative impact with proposal at 2 Coulsdon Road 19/03003/FUL</p>	<p>Parking concern addressed in Section 8.55 of this report. Each development will provide suitable on-site sustainable drainage and each will provide CIL contributions.</p>
<p>There are sufficient apartments in Coulsdon already</p>	<p>There is no objection to the principle of infill residential development in this area. The proposal re-provides family housing in a residential area in accordance with Local Plan policy.</p>
<p>The plans do not appear to reflect accurately the irregular shape of the site at 8 Coulsdon Road. There is a note on the design regarding the need to take accurate measurements before work commences. A cursory look at this long and narrow plot of land (in person or using Google Maps satellite data) reveals boundaries that are not straight and a width that varies along the length of the plot. Accurate measurements should be taken before any decision is taken on the planning application since there is a significant doubt as to the feasibility of the current proposal.</p>	<p>Officers have visited the site and assessed the plans and are satisfied with the information provided.</p>

6.4 Cllr Margaret Bird (Old Coulsdon Ward Councillor) make representations, objecting to the proposal and referring it to Planning Committee:

- This site fronts both Coulsdon Rd and Petersfield Crescent.
- This application has a huge impact on residents in Petersfield Crescent as it is close to another application for 9 flats at No 2 Coulsdon Rd which also designed to front Petersfield Crescent so the impact must be taken into context with that as yet undecided application.
- The south side of the road on Petersfield Crescent is physically the higher side and the 3 storey houses will tower over the current homes unless the height is restricted, on the north side of the road impacting on their quality of life enjoyed for over 60 years by way of intrusion.
- The house next door is elevated but set well back from the road so reducing intrusion.
- 70% of new homes in this area should have 3 or more bedrooms given the PTAL rating according to the local plan but this development of 8 dwellings only has 3 less than half of 70%.
- Petersfield Crescent is a small road with just 16 homes and the increase in dwellings with the already proposed No 2 Coulsdon Rd will change the character completely.
- This is an area of family homes not flat dwellings and needs to be reflected in all of and proposals to maintain the character of Old Coulsdon ward.

6.5 Hartley and District Resident's Association have objected to the proposal and requested determination by Planning Committee:

- No affordable housing
- Only 1 x 3 bed unit proposed
- Blocks of four storey flats out of character
- Contemporary design, bulking and massing is too large, overbearing, out of scale and out of character
- Loss of bungalow
- Adverse impact on side windows of 6 Coulsdon Road
- Over development of the site
- Significant loss of wildlife habitat and green garden with most of the existing garden being built on or paved.
- A full survey of the site with level details should be provided to demonstrate the proposal will fit into the site as it would appear there are errors on the drawings.
- Four car parking spaces are insufficient. This will result in overspill parking onto Petersfield Crescent. Need to consider the development of 2 Coulsdon Road.
- No electric charge points are being provided for the four onsite car parking spaces. AI
- No disabled car parking is provided.
- Fails to provide a turning area to allow cars to access and egress the parking bays on Petersfield Road. Not demonstrated that adequate sightlines can be achieved for vehicles exiting the driveways taking into consideration the existing trees in the roadside verges.
- Additional noise

- A Health Impact Statement nor daylight assessment analysis have been provided for the new development.
- Too many housing units are being crammed into this small site which will result in substandard accommodation for the future occupiers
- A Demolition/Construction Logistics Plan has not been provided.
- The flood risk assessment concludes that a Sustainable Drainage Strategy (SUDS) should be developed for the Site. This has not been done.

7.0 RELEVANT PLANNING POLICIES AND GUIDANCE

7.1 In determining any planning application, the Council is required to have regard to the provisions of its Development Plan so far as is material to the application and to any other material considerations and the determination shall be made in accordance with the plan unless material considerations indicate otherwise. The Council's adopted Development Plan consists of the Consolidated London Plan 2015, the Croydon Local Plan 2018 and the South London Waste Plan 2012.

7.2 Government Guidance is contained in the National Planning Policy Framework (NPPF), issued in February 2019. The NPPF sets out a presumption in favour of sustainable development, requiring that development which accords with an up-to-date local plan should be approved without delay. The NPPF identifies a number of key issues for the delivery of sustainable development, those most relevant to this case are:

- Promoting sustainable transport;
- Delivering a wide choice of high quality homes;
- Requiring good design.

7.3 The main policy considerations raised by the application that the Committee are required to consider are:

7.4 Consolidated London Plan 2015

- 3.3 Increasing housing supply
- 3.4 Optimising housing potential
- 3.5 Quality and design of housing developments
- 3.8 Housing choice
- 5.1 Climate change mitigation
- 5.2 Minimising carbon dioxide emissions
- 5.3 Sustainable design and construction
- 5.12 Flood risk management
- 5.13 Sustainable drainage
- 5.16 Waste net self sufficiency
- 6.3 Assessing effects of development on transport capacity
- 6.9 Cycling
- 6.13 Parking
- 7.2 An inclusive environment
- 7.3 Designing out crime
- 7.4 Local character
- 7.6 Architecture

- 7.21 Woodlands and trees

Policy 3.3 of the London Plan 2016 recognises the pressing need for more homes in London and Policy 3.8 states that Londoners should have a genuine choice of homes which meet their requirements for different sizes and types of dwellings in the highest quality environments. The impact of the draft London Plan is set out in paragraph 7.7 below.

7.5 Croydon Local Plan 2018

- SP2 - Homes
- SP6.3 - Sustainable Design and Construction
- DM1 - Housing choice for sustainable communities
- SP4 – Urban Design and Local Character
- DM10 - Design and character
- DM13 - Refuse and recycling
- DM16 – Promoting healthy communities
- SP6 – Environment and Climate Change
- DM23 - Development and construction
- DM25 – Sustainable drainage systems and reducing floor risk
- SP7 – Green Grid
- DM27 – Biodiversity
- DM28 – Trees
- SP8 – Transport and communications
- DM29 - Promoting sustainable travel and reducing congestion
- DM30 - Car and cycle parking in new development

7.6 There is relevant Supplementary Planning Guidance as follows:

- London Housing SPG March 2016
- Croydon Suburban Design Guide Supplementary Planning Document April 2019

7.7 Emerging New London Plan

Whilst the emerging New London Plan is a material consideration, the weight afforded is down to the decision maker linked to the stage a plan has reached in its development. The Plan appears to be close to adoption. The Mayor's Intend to Publish version of the New London Plan is currently with the Secretary of State and no response had been submitted to the Mayor from the Secretary of State. Therefore, the New London Plan's weight has increased following on from the publication of the Panel Report and the London Mayor's publication of the Intend to Publish New London Plan. The Planning Inspectors' Panel Report accepted the need for London to deliver 66,000 new homes per annum (significantly higher than existing adopted targets), but questioned the London Plan's ability to deliver the level of housing predicted on "small sites" with insufficient evidence having been presented to the Examination to give confidence that the targets were realistic and/or achievable. This conclusion resulted in the Panel Report recommending a reduction in London's and Croydon's "small sites" target.

The Mayor in his Intend to Publish New London Plan has accepted the reduced Croydon's overall 10 year net housing figures from 29,490 to 20,790 homes, with the "small sites" reduced from 15,110 to 6,470 homes. Crucially, the lower windfall housing

target for Croydon (641 homes a year) is not dissimilar to but slightly larger the current adopted 2018 Croydon Local Plan target of 592 homes on windfall sites each year.

It is important to note, should the Secretary of State support the Intend to Publish New London Plan, that the overall housing target in the New London Plan would be 2,079 new homes per annum (2019 – 2029) compared with 1,645 in the Croydon Local Plan 2018. Therefore, even with the possible reduction in the overall New London Plan housing targets, assuming it is adopted, Croydon will be required to deliver more new homes than our current Croydon Local Plan 2018 and current London Plan (incorporating alterations 2016) targets.

For clarity, the Croydon Local Plan 2018, current London Plan (incorporating alterations 2016) and South London Waste Plan 2012 remain the primary consideration when determining planning applications.

8.0 MATERIAL PLANNING CONSIDERATIONS

8.1 The main planning issues raised by the application that the Planning Committee are required to consider are as follows:

1. Principle of development
2. Townscape and visual impact
3. Housing quality for future occupiers
4. Residential amenity for neighbours
5. Access and parking
6. Sustainability and environment
7. Other matters

Principle of Development

8.2 This application must be considered against a backdrop of significant housing need, not only across Croydon, but also across London and the south-east. All London Boroughs are required by the London Plan to deliver a number of residential units within a specified plan period. In the case of the London Borough of Croydon, there is a requirement to deliver a minimum of 32,890 new homes between 2016 and 2036 (Croydon's actual need identified by the Croydon Strategic Housing Market Assessment would be an additional 44,149 new homes by 2036, but as there is limited developable land available for residential development in the built up area, it is only possible to plan for 32,890 homes). This requirement is set out in policy SP2.2 of the Croydon Local Plan (CLP) (2018), which separates this target into three relatively equal sub targets with 10,760 new homes to be delivered within the Croydon Opportunity Area, 6,970 new homes as identified by specific site allocations for areas located beyond the Croydon Opportunity Area boundary and 10,060 homes delivered across the Borough on windfall sites. The draft London Plan, which is moving towards adoption (although in the process of being amended) proposes significantly increased targets which need to be planned for across the Borough. In order to provide a choice of housing for people in socially-balanced and inclusive communities in Croydon, the Council will apply a presumption in favour of sustainable development of new homes.

8.3 The site sites on the border between the Coulsdon/Old Coulsdon. The "Places of Croydon" section of the CLP (2018) identifies Coulsdon as an area of moderate residential growth based on available land whereby residential development will

respect the existing character and local distinctiveness. Kenley and Old Coulsdon is defined as an area of sustainable growth of the suburbs, with some opportunity for windfall sites will see growth mainly by infilling with dispersed integration of new homes respecting existing residential character and local distinctiveness.

- 8.4 The Croydon Suburban Design Guide (2019) sets out how suburban intensification can be achieved to high quality outcomes and thinking creatively about how housing can be provided on windfall sites. As is demonstrated above, the challenging targets will not be met without important windfall sites coming forward, in addition to the large developments within Central Croydon and on allocated sites.
- 8.5 The application is for a flatted development providing additional homes within the borough, which the Council is seeking to promote. The site is located within an existing residential area and as such providing that the proposal respects existing residential character and local distinctiveness, and accords with all other relevant material planning considerations, the principle of development is supported.
- 8.6 CLP Policy DM1.2 seeks to prevent the net loss of 3-bedroom homes (as originally built) and homes less than 130m². The existing building on site is a 3 bedroom house with a floor area of approximately 107sqm. All of the proposed units have floor spaces of less than 130sqm and 2 of the new units would comprise three bedrooms. There would therefore be no net loss of homes under 130sqm or three-bedroom homes as required by Policy DM1.2.
- 8.7 Policy SP2.7 seeks to ensure that a choice of homes is available to address the borough's need for homes of different sizes and that this will be achieved by setting a strategic target for 30% of all new homes up to 2036 to have three or more bedrooms. CLP policy goes on to say that within three years of the adoption of the plan, an element may be substituted by two-bedroom (four person) homes. The application proposes 2 x 3 bedroom units and 1 x 2 bedroom 4 person unit. Overall, the proposal provides a net gain in family accommodation (37.5%) and contributes towards the Councils goal of achieving a strategic target of 30% three bedroom plus homes.

Townscape and Visual Impact

- 8.8 This is a predominantly residential area with an array of building types present. Dwellings on Coulsdon Road are detached and semi-detached and vary in appearance, being mostly two storey in height with pitched roofs. Properties on the north west side of Petersfield Crescent are two-storey semi-detached all of a similar appearance and era. Detached dwellings to the south east side of the road vary in building line, mass and height. The buildings mostly have red/brown tiled roofs and there are an array of materials to the elevations including render, mock-tudor, brick and hanging tile.
- 8.9 The existing building on site was erected in the 1930's and benefitted from extensions to the front elevation in the 1990s. The building does not hold any special significant architectural merit and therefore there is no objection to its demolition.
- 8.10 CLP Policy DM10.1 states that proposals should achieve a minimum height of 3 storeys whilst respecting a) the development pattern, layout and siting; b) the scale, height, massing and density; and c) the appearance, existing materials and built and natural features of the surrounding area.

8.11 The Suburban Design Guide suggests appropriate ways of accommodating intensified development on sites and suggests that where surrounding buildings are predominantly detached dwellings of two (2) or more storeys, new developments may be three (3) storeys with an additional floor contained within the roof space or set back from the building envelope below, see diagram below.



Figure 2.10c: Where surrounding buildings are predominantly detached dwellings of two (2) or more storeys, new developments may be three (3) storeys with an additional floor contained within the roof space or set back from the building envelope below.

8.12 The proposal is for two buildings, one facing Coulsdon Road and one facing Petersfield Crescent.

Coulsdon Road building

8.13 The building facing Coulsdon Road is a three storey building with a fourth floor of accommodation contained in the roofspace. The building utilises existing ground levels to provide a lower ground floor level. From the front elevation, the lower ground floor would not be visible, the building appearing as a two storey building with a pitched roof which is comparable with other dwellings to this side of Coulsdon Road. The building would have a higher overall ridge height than the dwellings either side, however given the changes in ground level across the sites and the variation in building type, it is considered that the height respects that of buildings in the area.



Figure 3. Street elevation on Coulsdon Road



Figure 4. Proposed CGI from Coulsdon Road

- 8.13 The footprint of the proposed building is comparable with the existing bungalow on site, the existing building footprint being 120sqm and the proposed 150sqm. The proposed building sits forward of the existing house by approximately 3m but is still set behind the front building line of No.6 to the north and therefore would be appropriately sited within the plot. The building is also set in from the side boundaries of the plot, retaining a gap between the buildings of a similar size to the dwellings in the row to the south on Coulsdon Road. From the front elevation the building appears as a two storey detached building with a pitched roof and therefore is respectful of the appearance and massing of the adjacent properties. Whilst the depth of the building is noted, the side elevations would be masked by the buildings that flank either side and therefore this mass would not be harmfully obtrusive from within the public realm.
- 8.14 The rear elevation as seen from Petersfield Crescent, it is noted that the building does have a more imposing visual impact as a result of the significant change in levels across the site. The building would be located approximately 28m back from the boundary with Petersfield Crescent and would be set behind the proposed development that would front this road. As per the existing houses on Coulsdon Road that can be seen from Petersfield Crescent, the buildings are viewed as a backdrop to existing gardens with greenery and boundary treatment in front. Given the set back from the road and the existing view of dwellings, it is considered that the building would cause no undue harm to the existing character of the area.
- 8.15 The material palette is appropriate in this locality, maintaining a traditional appearance. The elevations would be finished in brick and render and the roof finished in plain clay tiles. These materials would sit comfortably with the surrounding area. Submission of specific material details will be secured by condition.

Houses to Petersfield Crescent

- 8.16 The proposed houses fronting Petersfield Crescent have been significantly altered in mass and appearance since the application was originally submitted.
- 8.17 There are three existing houses on the south east side of Petersfield Crescent. No's 14 and 15 face the road and are set back approximately 7-8m from the back edge of

the pavement. No.16 is a more recent infill property and is angled so that it does not sit front on to the street. As such there is no clearly established building line on this side of Petersfield Crescent. As per No.8, the proposed units would not directly face the highway either, and given that there is no strong front building line, this is considered to be appropriate in this specific setting. At its closest point, the proposed building would be located approximately 7m from the back edge of the pavement which is comparable with No's. 14 and 15 to the south. As such, the proposal would not have an overwhelming presence in the street scape in terms of its position.

8.18 The proposed building presents as a pair of semi-detached houses which is characteristic of Petersfield Crescent. As originally submitted, the application proposed significant excavation to form a pair of three-storey townhouses with additional accommodation in the roof space. Officers were of the opinion that the initial proposal was out of keeping with the appearance of adjacent properties on Petersfield Crescent and would have a harmful impact on the appearance of the street scene, by reason of the overall height of the building and roof form comprising half hips and narrow gable features that accentuated the verticality of the building.

8.19 The scheme was subsequently amended so that the lower storey was removed, the existing land levels better respected. The building now appears as two-storey with accommodation in the roof space. The frontage has been designed to comprise decorative gable features that are comparable with the dwellings to the north west side of Petersfield Crescent, with brickwork to the elevations and mock-Tudor timber and rendered elements to the gable features.



Figure 5. Street elevation on Petersfield Crescent



Figure 6. Existing houses on Petersfield Crescent

- 8.20 The main roof is pitched, however elements are present to either side to enable the roof space to be utilised as additional habitable accommodation. These elements are set back from the frontage by 1.2m and down from the ridge by 0.7m. On balance, it is considered that the appearance of the amended building would be appropriate in the street scene, respecting the existing character and layout of buildings on Petersfield Crescent.
- 8.21 The existing garden area would be developed to provide residential accommodation. There are no significant or protected trees in this garden, the garden mostly laid to lawn with ornamental shrubs and bushes to the boundaries. On the Petersfield Road frontage it is proposed to create one vehicular access for the two dwellings to retain/provide as much of a soft frontage as possible. New hedging and trees are proposed to the front forecourt as well as planters to the front of the houses. Coulsdon Road is lined with mature street trees which would not be affected by the development. The plans have been amended to ensure that the existing hedge to the front boundary on Coulsdon Road is retained. Full details of landscaping will be secured by condition including protection measures for the retained hedging and street trees.
- 8.22 The site has a suburban setting with a PTAL rating of 2 and as such the London Plan indicates that the density levels ranges of 150-250 habitable rooms per hectare (hr/ha) are appropriate. The proposal would be in excess of this range at 295 hr/ha. However, the London Plan further indicates that it is not appropriate to apply these ranges mechanistically, as the density ranges are broad, to enable account to be taken of other factors relevant to optimising potential – such as local context, design and transport capacity. The application site is a large plot within an established residential area and is comparable in size to other flatted and neighbouring back-land developments approved throughout the borough. As outlined above, the proposal as amended would overall result in a development that would respect the pattern and rhythm of neighbouring area and would not harm the appearance of the street scene.
- 8.23 Therefore on balance, having considered all of the above, against the backdrop of housing need, officers are of the opinion that the proposed development that would comply with the objectives of the above policies in terms of respecting local character.

Housing Quality for Future Occupiers

- 8.24 All of the proposed new units would comply with internal dimensions required by the Nationally Described Space Standards (NDSS).
- 8.25 Whilst the proposed units in the flatted block face either east or west, they all have secondary windows in the south and/or north elevations which would allow extra light and ventilation into the units. No units are north facing only. The lower ground floor unit has been amended during the course of the application so that the east facing bedroom has been removed following concerns that this would receive inadequate light (as a result of the ground level changes). The unit in the roof space initially had only a side facing bedroom window that was shown to be obscurely glazed. The dormer window has been relocated to the front elevation to enable this room to have some outlook.

- 8.26 The proposed houses are dual aspect with windows facing east and west. These units would receive adequate light and would be well ventilated. Overall, the quality of proposed internal amenity space is considered acceptable.
- 8.27 With regard to external amenity space, the London Housing SPG states that a minimum of 5sqm of private outdoor space should be provided for 1-2 person dwellings and an extra 1sqm for each additional unit. The flats all have private amenity in the form of a courtyard or balconies. The houses have provide gardens of approximately 30sqm. All private amenity spaces meet or exceed the required standards.
- 8.28 An area of communal garden (approximately 60sqm) is provided within the site. Children's play space would be provided within this space and full details of this area will be secured by condition.
- 8.29 In terms of accessibility, the level changes across the site make it difficult to provide step free access for the majority of the development. Step free access is provided to the two ground floor units of the flatted block. The applicant has confirmed that the ground floor units will be designed to be wheelchair accessible/adaptable dwellings (to both building regulation Part M4(2) and Part M4(3)). This would be secured by condition insofar as it relates to the approach to the building, private amenity space and internal arrangements of the accommodation.
- 8.30 Given the constraints of the site the other units cannot be accessible for wheelchair users. Given the significant level change the communal garden is only accessible by steps. In order to ensure the development fronting Petersfield Crescent is acceptable in terms of impact on visual amenity, the dwellings are set up from the highway and therefore are not step free. It has been noted through neighbour representation that this area is not very accessible for wheelchair users given the topography and fall and quality of the pavement on Petersfield Crescent. In this instance, it is considered acceptable for the units to not provide step free access for future occupiers.
- 8.31 There would be a gap of 16.5m between the two proposed buildings. The SDG suggests that back to back distances for new to new dwelling should be a minimum of 12m to provide sufficient privacy to the new residents. The proposal accords with this guidance. The level change between the buildings is noted however given the orientation of the buildings it is considered that the new dwellings would receive adequate light.
- 8.32 Overall, given the constraints of the site, the development is considered to provide an acceptable standard of accommodation for future occupiers.

Residential Amenity for Neighbours

- 8.33 The main properties that would be affected by the proposed development are 6 and 10 Coulsdon Road, 16 Petersfield Crescent and the properties on the north west side of Petersfield Crescent.

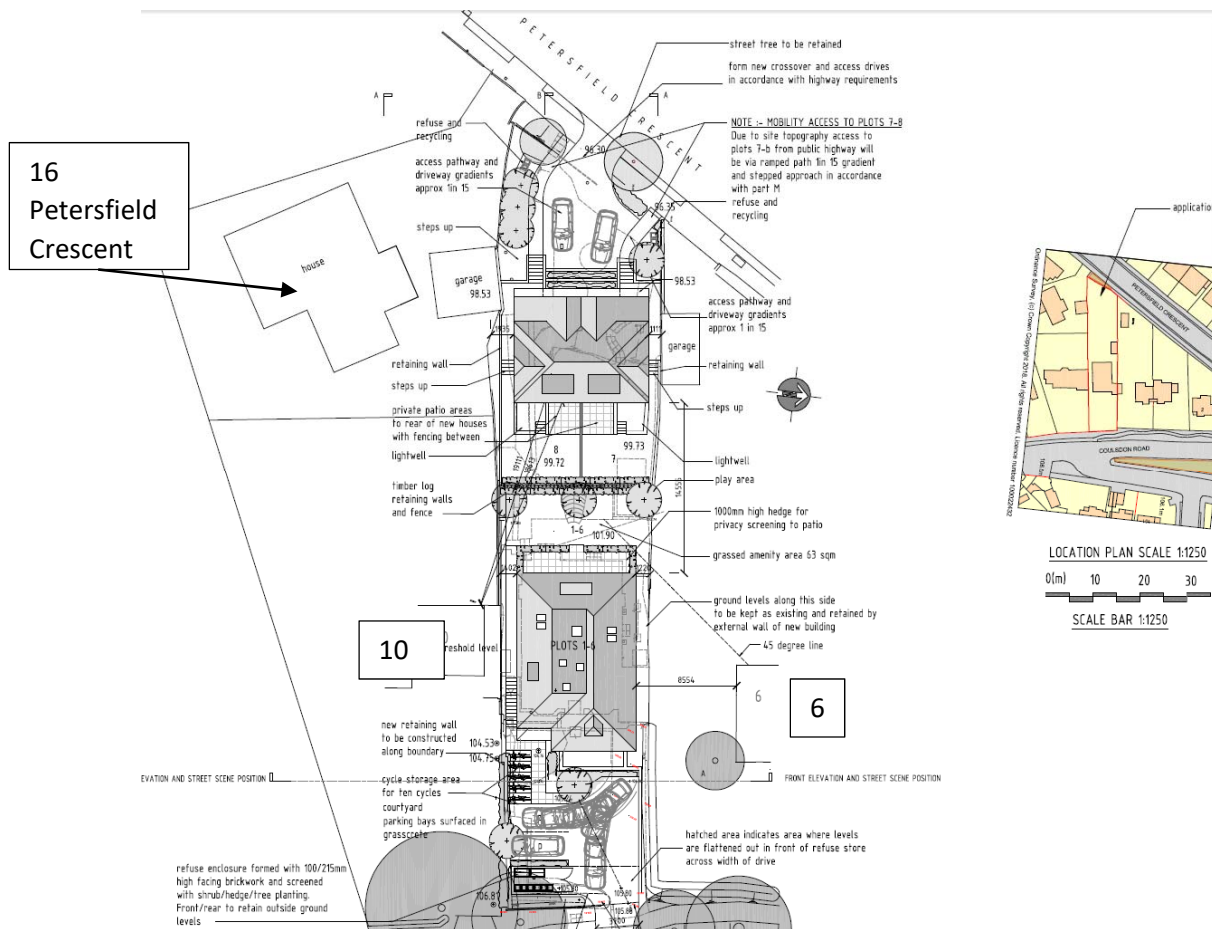


Fig 7: Proposed Block Plan highlighting the relationship with the adjoining occupiers.

6 Coulsdon Road

- 8.34 This semi-detached dwelling is located to the north of the site. It is single storey with accommodation in the roof space. Its side elevation is located approximately 7m from the boundary with No.8 and contains a kitchen window and door and obscurely glazed bathroom windows. There is a detached shed to the side of the dwelling.
- 8.35 The proposed development does not encroach over a 45 degree angle either in plan or vertically from the closest rear window of No.6. As such it is not considered that the development would be unduly overbearing on this property. It is noted that there are mature trees along the side boundary within the garden of No.6 which would screen the development from the house and rear patio of this dwelling. The development site is located to the south of No.6 and therefore the proposal may result in some loss of light to the side facing windows in No.6. Paragraph 2.9.3 of the SDG outlines that daylight and sunlight analysis study will not normally be required where a neighbour's window directly faces onto or over an application site in a manner that is considered to be un-neighbourly. These un-neighbourly windows place undue restraints on the development, and as such the light and outlook they receive will not receive significant protection. Two of the windows serve a bathroom and wc (non-habitable rooms) and the other openings the kitchen which are situated 7m from the boundary with a shed and vegetation in front. Given the location of these openings, in this instance any loss

of light to the kitchen on No.6 is not considered to be so significant that permission should be refused for this reason.

8.36 The windows in the northern side of the proposed building are either secondary or serve bathrooms. As such all windows can be glazed with obscure glass and can be non-opening below 1.7m above floor level, and therefore the proposal will not cause any loss of privacy to No.6.

8.37 The proposed houses to the west of the site are located approximately 23m from the rear elevation of No.6 and are situated on a lower level. Given this separation distance and level change, these houses would not cause any harmful loss of light, outlook or privacy to No.6.

8.38 Overall, on balance, the impact on No.6 is considered to be acceptable.

10 Coulsdon Road

8.39 This detached two-storey dwelling is situated to the south of the application site. It extends across the width of the plot and has an integral double garage to the north side of the ground floor with non-main habitable space to the rear of the garage. There are no side facing windows. The flatted block does not encroach over a 45 degree angle from the rear or front habitable windows of No.10 and therefore would not have an overbearing impact on this property. No.10 is located to the south side of the application site and therefore the proposals would not cause any harmful loss of light.

8.40 The windows in the southern side of the proposed building are either secondary or serve hallways. As such all windows can be glazed with obscure glass and can be non-opening below 1.7m above floor level, and therefore the proposal will not cause any loss of privacy to No.10.

8.41 The proposed houses to the west of the site are located approximately 18m from the rear elevation of No.10 and are situated on a lower level. Given this separation distance and level change, these houses would not cause any harmful loss of light, outlook or privacy to No.10.

8.42 Overall, the impact on No.10 is considered to be acceptable.

16 Petersfield Crescent

8.43 This detached two-storey dwelling is located to the south of the site and is situated at an angle to the highway facing onto the western part of the application site. It has a detached garage to the northern boundary and its garden wraps around the north, east and southern side of the building.

8.44 The proposed houses facing Petersfield Crescent are located to the northern side of the garden and garage of No.16. The houses would be approximately 9.5m from the closest point of the dwellinghouse at No.16 and would not be positioned in view of any main habitable room windows. As such, given the layout and relationship between buildings, the proposal would not be significantly overbearing or cause any harmful loss of outlook.

- 8.45 The proposed building would be located to the north side of No.16 and would cause no harmful loss of daylight or sunlight to any habitable rooms of the adjacent dwelling. Whilst it is noted that the northern part of the garden of No.16 accommodates a hot tub (adjacent to the northern boundary), given the orientation of the sites the proposal would cause no loss of sunlight to this part of the garden.
- 8.46 The windows in the southern side of the proposed building serve the staircase and as such can be glazed with obscure glass and can be non-opening below 1.7m above floor level. Therefore the proposal will not cause any loss of privacy to No.16.
- 8.47 The proposed flatted block fronting Coulsdon Road would be located approximately 21m from the closest part of the dwelling at No.16 and at an oblique angle to any main habitable room windows. Given the orientation of the buildings and gap between them, the flatted block would not have any significantly harmful impact on the amenities of No.16 by way of cause any harmful loss of light, outlook or privacy.
- 8.48 Overall, the impact on No.16 is considered to be acceptable.

Dwellings on the opposite side of Petersfield Crescent

- 8.49 Dwellings on the opposite side of Petersfield Crescent are located to the north west of the site. Their front boundaries are located approximately 10m from the boundary of the application site and approximately 18m from the closest point of the proposed building. Given this gap, and separation by the road, the proposal would not cause any harmful loss of light, outlook or privacy to these dwellings.
- 8.50 Overall, the impact on the neighbouring residential property is not so significant that permission should be refused for this reason and conditions would be imposed to prevent the proposals from causing any loss of privacy.

Parking and Access

Parking

- 8.51 The site has a PTAL rating of 2 which means that it has relatively poor access to public transport links. The London Plan sets out maximum car parking standards for residential developments based on public transport accessibility levels and local character. 1-2 bedroom units should provide less than 1 space per unit and 3 bedroom units up to 1.5 spaces per unit. Therefore the maximum requirement for this development would be 9 spaces. 2011 Census data estimates that car parking demand from the proposed development will generate a demand of 7 spaces.
- 8.52 It is proposed to create a total of 4 vehicular parking spaces off road, two for the development fronting Coulsdon Road (6 x 1 and 2 bedroom units) and two for the two houses (3 bedroom) fronting Petersfield Crescent. The development is therefore potentially likely to result in a parking overspill of 3-5 vehicles onto the surrounding roads.
- 8.53 The applicant has undertaken an on-street parking survey to recognised Lambeth methodology. This survey shows that roads surveyed in the immediate area have a parking stress of between 15-20% (317 available spaces). It is important to note that Coulsdon Road to the south of Petersfield Crescent has been discounted from the survey due to safety concerns of parking in this area.

- 8.54 It is considered that residents of the proposed development are likely to park on the roads closest to the site, which are Petersfield Crescent, Hartley Down and Hartley Way. The parking stress survey shows that Petersfield Crescent has 29% parking stress (36 available spaces), Hartley Down has an average of 19% parking stress (60 available spaces) and Hartley Way has an average of 14% parking stress (52 available spaces). Given the low parking stress in the area, it is not considered that the additional of 3-5 extra vehicles parking on street would have a significantly harmful impact on highway safety in this instance.
- 8.55 It is noted that there is a current planning application/planning permission has recently been granted at 2 Coulsdon Road (reference 19/03003/FUL) which proposes 6 off-road parking bays for 9 units. Given the low parking stress recorded in the area, it is considered that there is ample space on street to accommodate any overspill parking demand from both of the proposed developments.
- 8.56 Local Plan Policy DM30 states that 20% of parking bays should have EVCP with future provision available for the other bays. Details and provision of the EVCP will be conditioned.
- 8.57 Cycle storage areas would be provided within the site for the proposed development. Each house would have space for two cycles inside the buildings. The proposed flats would generate a demand for 9 cycle bays (as required by the London Plan). A storage area for 10 cycles would be provided to the front of the building. Full details of this storage area will be secured by condition.

Access

- 8.58 TRICS data has been used to assess trip generation from the proposed development and finds that the development would generate an additional two vehicle movements in each peak hour which is unlikely to be perceptible and the effects on the highway network are therefore concluded to be negligible.
- 8.59 The Transport Statement provides manoeuvring plans that demonstrate that vehicles can manoeuvre into the proposed parking spaces. On Coulsdon Road vehicles can access and egress in forward gear. Vehicles would need to manoeuvre on Petersfield Road to access the new dwellings. It is acknowledged that the access is opposite Hartley Down. This is the existing situation for the majority of existing properties on this road and the amount of vehicular movements associated with this access would be low, therefore it is considered appropriate in this instance. The submitted plans show that the required pedestrian and vehicle sightlines can be achieved from both vehicular accesses to the site.

Refuse storage/collection

- 8.60 Individual refuse storage areas are proposed for the two houses fronting Petersfield Crescent. The plans show that the scale of the refuse areas is adequate for the needs of the development.
- 8.61 A refuse storage area is shown to the front of the flats fronting Coulsdon Road. Given the fall in level across the site, the plans have been amended to show the refuse store located behind the front boundary hedge whereby the bins will be at a similar level to the existing vehicular access.

Environment and sustainability

- 8.62 Conditions will be attached to ensure that a 19% reduction in CO2 emissions over 2013 Building Regulations is achieved and mains water consumption would meet a target of 110 litres or less per head per day.
- 8.63 The site is located within an area low risk of surface water and groundwater flooding. A Flood Risk Assessment (FRA) has been submitted as part of the application which outlines the risks of flooding at the site. Policy DM25 requires all development to incorporate sustainable drainage measures (SuDS). The report outlines SuDS measures that could be feasible at the site including rainwater harvesting, green roofs, permeable paving, swales and soakaways. Onsite investigation is required and therefore a condition requiring site specific SuDS measures would be imposed on any planning permission.
- 8.64 Thames Water has commented that from the information provided they are unable to determine the waste water infrastructure needs of this application. They have required that should planning permission be granted, a condition be applied requiring a drainage strategy detailing any on and/or off site drainage works to be submitted to ensure that the proposal does not lead to sewerage flooding.

Other matters

- 8.65 Trees and landscape - There are no significant or protected trees in the garden of No.8. The garden is mostly laid to lawn with ornamental shrubs and bushes to the boundaries. 10 Coulsdon Road has large trees to its southern boundary and these are at a distance from the proposed development and would therefore not be affected. On the Petersfield Road frontage it is proposed to create one vehicular access and retain/provide as much of a soft frontage as possible. The existing street tree would be retained. New hedging and trees are proposed to the front forecourt as well as planters to the front of the houses. Coulsdon Road is lined with mature street trees which would not be affected by the development. The plans have been amended to ensure that the existing hedge to the front boundary on Coulsdon Road is retained. Full details of landscaping will be secured by condition including protection measures for the retained hedging and street trees.
- 8.66 Ecology – Respondents have commented that the proposal would lead to a loss of wildlife habitat. The application site is not near an area of special scientific interest or a site of nature conservation value. There are concerns raised by public about biodiversity. The site is a residential property in an adequate state of repair. As such, it is not considered likely to support protected species' habitats. Whilst there would be an overall loss of landscaped space, it is not considered to be high in biodiversity value. An informative would be included on any decision making the applicant aware that it is an offence to harm protected species or their habitat and in the event that protected species are found on site the applicant should refer to Natural England standing advice.
- 8.67 The development will be liable for a charge under the Community Infrastructure Levy (CIL). This payment will contribute to delivering infrastructure to support the development of the area.

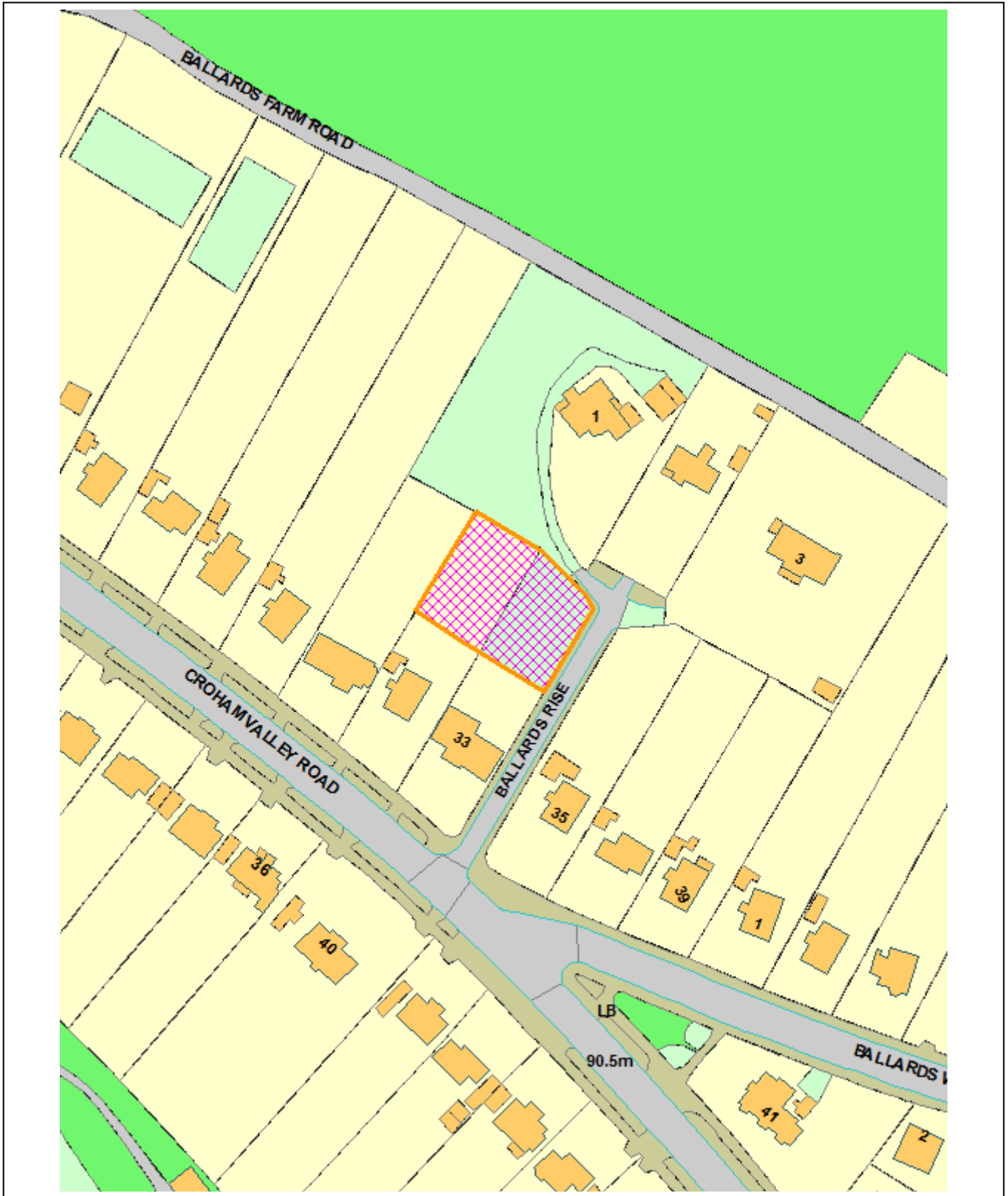
Conclusion and planning balance

- 8.68 The principle of residential development is considered acceptable in this area. The development accords with policy requirements and the Suburban Design Guide in terms of its massing and overall impact on the visual amenities of the area. The

proposal has been designed to ensure there would be no unacceptably harmful impact on the amenities of the adjacent properties and provides adequate amenity for future residents. The impact on the highway network is acceptable. The proposal's design and appearance is satisfactory and does not weigh against it in the balance. The proposal would lead to a reduction in garden area, but not of high biodiversity value and reversing on to Petersfield Crescent, but not so frequently as to raise concerns about highway safety. The proposal would provide acceptable quality of accommodation and a good number and mix of units. Therefore, with the conditions recommended the proposal is considered to be in accordance with the relevant policies.

8.69 All other relevant policies and considerations, including equalities, have been taken into account.

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PART 6: Planning Applications for Decision

Item 6.5

1 SUMMARY OF APPLICATION DETAILS

Ref: 19/04615/FUL
 Location: Land rear of 31-33 Croham Valley Road, (Facing onto Ballards Rise), South Croydon
 Ward: South Croydon
 Description: Erection of 2 two storey detached buildings with accommodation within the roofspace comprising 8 flats, bin store, formation of vehicular access and provision of 7 parking spaces
 Drawing Nos: 250-D-00, 250-D-01-REVC, 250-D-02-REVC, 250-D-03-REVB, 250-D-04-REVB, 250-D-05, 250-D-06-REV-B, 250-D-07-REV-A, 250-D-08-REV-A, 250-D-09-REV-B, 250-D-10-REV-A, 250-D-11, 250-D-12-REV-C, 250-D-13-REV-A, 250-D-14, 250-D-15, 250-D-16, 250-D-17-REV-B, 250-D-18-REV-A, 250-D-19-REV-A, 250-D-20, 250-D-21, 250-D-22, 250-D-23, 250-D-24-REV-A, 250-D-25, 1710-GUADR-L-002 P02, Design and Access Statement (subject to amendments), Parking Stress, Flood Risk Assessment.
 Applicant: Mr Owens, Silverleaf Investments 2 LLP
 Case Officer: Hayley Crabb

	studio	1 bed	2 bed	3 bed	4 bed
Existing houses					
Proposed Flats		2	4	2	

All units are proposed for private sale

Number of car parking spaces	Number of cycle parking spaces
7	14

1.1 This application is being reported to Planning Committee as objections above the threshold have been received, the application has been referred by the Croham Valley Residents' Association and by the Ward Councillor (Cllr Michael Neal) requested Committee consideration in accordance with the Council's Constitution.

2 RECOMMENDATION

2.1 That the Committee resolve to GRANT planning permission subject to the completion of a S.106 Agreement to secure the following heads of terms:

- A financial contribution of £6,000 towards highway management measures and the delivery of sustainable transport initiatives in and around Croham Valley Road and neighbouring streets.

2.2 That the Director of Planning and Strategic Transport has delegated authority to negotiate the legal agreement indicated above.

2.3 That the Director of Planning and Strategic Transport has delegated authority to issue the planning permission and impose conditions and informatives to secure the following matters:

Conditions

- 1) Development to be carried out in accordance with the approved drawings and reports except where specified by conditions
- 2) Materials to be submitted with samples
- 3) Depth of window and door reveals to be agreed along with privacy screens to terraces)
- 4) No use of flat roofs (apart from those shown as terraces)
- 5) Car parking to be provided as specified in the application prior to occupation
- 6) Bin store/vehicular access/visibility splays as specified in the application
- 7) No additional windows in the flank elevations/obscure glazed first/second floors
- 8) Landscaping scheme to be submitted including hard/soft landscaping, retaining walls, boundary treatments and planting as boundary screening and SUDs techniques
- 9) Prior to first occupation, electric charge points to be submitted and approved by the LPA
- 10) Flood Risk Assessment
- 11) Construction Logistics Plan
- 12) One unit to comply with M4(3)
- 13) 19% reduction in carbon emissions
- 14) Water usage restricted to 110 litres per person per day
- 15) Highways condition
- 16) Commencement of development within three years of consent being granted
- 17) Any other planning condition(s) considered necessary by the Director of Planning and Strategic Transport

Informatives

- 1) CIL liability
- 2) Code of Practice for Construction Sites
- 3) Highway requirements

- 4) Protected species
- 5) Any other informative(s) considered necessary by the Director of Planning and Strategic Transport

2.4 That the Planning Committee confirms that adequate provision has been made, by the imposition of conditions, for the preservation or planting of trees as required by Section 197 of the Town and Country Planning Act 1990.

3 PROPOSAL AND LOCATION DETAILS

Proposal

3.1 The proposal comprises the following:

- Erection of 2 buildings (blocks) comprising 2 x three bedroom (one 6 person and one 4 persons), 2 x 2 bedroom (4 persons), 2 x 2 bedroom (3 persons) and 2 x 1 bedroom flats (2 persons)
- Private amenity space for each flat
- Communal/play space at rear
- Integrated cycle stores
- Integral bin store
- Formation of vehicular access and provision of 7 parking spaces

Site and Surroundings

- 3.2 The application site is located on the north eastern side of Croham Valley Road (a local distributor road) on the western side of the junction with Ballards Rise. The proposed back-land development would front onto Ballards Rise.
- 3.3 Croham Valley Road is characterised by detached properties set back from and fronting the road, set within relatively generous plots. There are three properties at the end of Ballards Rise which also have spacious grounds. There is a significant rise in land levels from Croham Valley Road up Ballards Rise of at least a storey.
- 3.4 To the rear of the properties on Ballards Rise is designated as Metropolitan Green Belt/Site of Nature Conservation Importance and Archaeological Priority Area as identified in the Croydon Local Plan. It has a Public Transport Accessibility Level (PTAL) of 1b, considered to be 'very poor'.



Planning History

- 3.5 The application site has had quite and extensive planning history.
- 3.6 (LBC Ref 07/00115/P) Erection of detached three bedroom house at rear with integral garage, formation of vehicular access onto Ballards Rise. Planning Permission **REFUSED** on grounds of character of the locality, unsatisfactory cramped backland development, detrimental to the amenities of adjoining occupiers from loss of privacy, poor outlook and visual intrusion and inadequate private amenity space
- 3.7 (LBC Ref 10/00910/P) Erection of a detached two bedroom bungalow at rear; formation of vehicular access onto Ballards Rise. Planning Permission **REFUSED** on grounds of character of the locality, unsatisfactory cramped backland development and detrimental to the amenities of adjoining occupiers from loss of privacy and visual intrusion. The application was the subject of a planning appeal which was dismissed on character ground only.
- 3.8 (LBC Ref 14/02840/P) Erection of a detached four bedroom house and detached garage; provision of vehicular access and associated parking. Planning Permission **REFUSED** as it was deemed it would be out of keeping with the character of the locality, resulting in an unsatisfactory cramped backland development that would be detrimental to the visual amenity of the street scene by reason of its design, scale, form and prominent siting and would be detrimental to the amenities of the occupiers of adjoining residential property resulting in visual intrusion. The application was the subject of a planning appeal which was dismissed on character ground only.
- 3.9 (LBC Ref 15/01512/P) Erection of a detached four bedroom house; provision of vehicular access and associated parking. Planning Permission **REFUSED** as it was deemed it would be out of keeping with the character of the locality by reason of its massing, siting, scale and depth of rear garden, resulting in an unsatisfactory

cramped backland development that would be detrimental to the visual amenity of the street scene.

3.10 (LBC Ref 15/04683/P) Erection of four bedroom single storey detached house with basement; provision of associated parking – Planning Permission **GRANTED**

3.11 (LBC Ref 16/03888/P) Erection of four bedroom single storey detached house with basement; provision of associated parking (without compliance with condition 7 - development to be carried out in accordance with approved plans - attached to planning permission 15/04683/P) – Planning Permission **GRANTED**

3.12 (LBC Ref 18/05411/FUL) Erection of 2 two storey detached buildings with accommodation within the roofspace comprising 1 three bedroom, 5 two bedroom and 3 one bedroom flats, bin stores, formation of vehicular accesses and provision of associated parking – Planning Permission Refused as it was deemed the proposed development would represent an over-development of the site as a consequence of an excessive number of units linked to the failure of the scheme to adequately accommodate off street car parking to support the number of units proposed, detrimental to the amenities and convenience of existing and future residential occupiers and highway conditions and safety within Ballards Rise and neighbouring streets.

4 SUMMARY OF KEY REASONS FOR RECOMMENDATION

- The principle of residential development on the site is acceptable;
- The design and appearance of the development is appropriate for the site;
- There would be no undue harm to the residential amenities of adjoining occupiers;
- The living standards of future occupiers are acceptable and compliant with the Nationally Described Space Standards and the London Plan;
- The level of parking and impact upon highway safety and efficiency is acceptable;
- Sustainability aspects of the development can be controlled by condition.

5 CONSULTATION RESPONSE

- The views of the Planning Service are expressed in the MATERIAL PLANNING CONSIDERATIONS section below.

6 LOCAL REPRESENTATION

6.1 The application has been publicised by 20 letters of notification to neighbouring properties in the vicinity of the application site. The number of representations received from neighbours and local groups in response to notification and publicity of the application were as follows:

No of individual responses: 26 Objecting: 26 Supporting: 0 Comment: 0

6.2 The following issues were raised in representations. Those that are material to the determination of the application, are addressed in substance in the MATERIAL PLANNING CONSIDERATIONS section of this report:

Summary of objections	Response
Fails to meet 30% of flats as 3 bedroom or more/residential mix/no other flats in area/	Addressed in Section 8.3 of this report
Overdevelopment by virtue of its bulk and massing/exceeding the density matrix/ Cramped accommodation/use of back garden land	Addressed in Section 8.4-8.5 of this report
Out of keeping/character with the area/Obtrusive by design/Overbearing	Addressed in Section 8.6-8.13 of this report
Visible from Metropolitan Green Belt/Metropolitan Open Land/Ballards Hillside/impact on hills and valleys	Addressed in Section 8.10 of this report
Adverse impact on neighbouring properties - Loss of privacy/Visual intrusion/Loss of light/Overlooking	Addressed in Section 8.14-8.15 of this report
Noise disturbance	Addressed in Section 8.15 of this report
Lack of private/communal amenity space	Addressed in Section 8.17 of this report
Access road too narrow – cul-de-sac/small to accommodate vehicular access and parking/blocking access to existing houses and blocking emergency vehicles	Addressed in Section 8.21-8.24 of this report
Impact on highway safety/insufficient parking/impact on Ballards Rise/impact on emergency vehicles/parking report/impact on pets and children	Addressed in Section 8.21-8.24 of this report
Drainage/Flooding due to excavation	Addressed in Section 8.29 of this report

Impact on wildlife/vegetation	Addressed in Section 8.30 of this report
Adverse impact on local services	The development would be liable for CIL which could ameliorate any impact on local services and infrastructure
Financial gain	This is not a planning consideration
Construction Logistics Plan	This would be secured through a pre-commencement condition

6.3 Cllr Michael Neal has objected to the scheme, making the following representations:

- DM10.1 The application does not respect:
 - a. The development pattern, layout and siting does not enhance the local character of the area
 - b. The scale, height, massing, and density is too large for the site and therefore an over development.
This is a back garden development and should not be permitted.
- DM10.6
The amenity of the occupiers of adjoining buildings are not protected through loss of light.
- DM10.9
It does not respect or enhance or strengthen the local character of the area.

6.4 Croham Valley Residents Association has objected to the scheme, making the following representations:

- Lack of parking/parking bays/impact on parking/emergency vehicles/impact on Ballards Rise
- Overdevelopment of the site by virtue of its bulk and mass
- The proposed building with 8 flats would be out of character
- Cramped site/lack of communal amenity space
- Loss of privacy/visual intrusion
- Fails to provide sufficient mix of family accommodation
- Special character of hills and valleys
- Does not respect building heights
- Buildings would be visible from the Metropolitan Green Belt and/or Metropolitan Open Land
- Does not respect the existing character of the area
- Affect the water table/Increase risk of flooding

- Disturbance to wildlife/affecting nearby natural vegetation

7 RELEVANT PLANNING POLICIES AND GUIDANCE

7.1 In determining any planning application, the Council is required to have regard to the provisions of its Development Plan so far as is material to the application and to any other material considerations. Such determination shall be made in accordance with the Plan unless material considerations indicate otherwise. The Council's adopted Development Plan consists of the Consolidated London Plan 2015, the Croydon Local Plan (February 2018), and the South London Waste Plan 2012.

7.2 Government Guidance is contained in the National Planning Policy Framework (NPPF) issued in February 2019. The NPPF sets out a presumption in favour of sustainable development, requiring that development which accords with an up-to-date local plan should be approved without delay. The NPPF identifies a number of key issues for the delivery of sustainable development, those most relevant to this case are:

- Promoting sustainable transport;
- Delivery of housing
- Promoting social, recreational and cultural facilities and services the community needs
- Requiring good design.

7.3 The main policy considerations raised by the application that the Committee are required to consider are:

7.4 Consolidated London Plan 2015

- 3.3 Increasing housing supply
- 3.4 Optimising housing potential
- 3.5 Quality and design of housing developments
- 3.8 Housing choice
- 3.9 Mixed and balanced communities
- 5.1 Climate change mitigation
- 5.2 Minimising carbon dioxide emissions
- 5.3 Sustainable design and construction
- 5.7 Renewable energy
- 5.10 Urban greening
- 5.12 Flood risk management
- 5.13 Sustainable drainage
- 5.14 Water quality and wastewater infrastructure
- 5.15 Water use and supplies
- 5.16 Waste net self sufficiency
- 5.18 Construction, Demolition and excavation waste

- 6.3 Effects of development on transport capacity
- 6.9 Cycling
- 6.10 Walking
- 6.11 Smoothing traffic flow and tackling congestion
- 6.12 Road Network Capacity
- 6.13 Parking
- 7.6 Architecture
- 8.3 Community infrastructure levy

Emerging New London Plan

- 7.5 Whilst the emerging New London Plan is a material consideration, the weight to be afforded is down to the decision maker, linked to the stage the Plan has reached in its development. The Plan appears to be close to adoption. The Mayor's Intend to Publish version of the New London Plan is currently with the Secretary of State and at the time of drafting this report, no response had been submitted to the Mayor. Therefore, the New London Plan's weight has increased following on from the publication of the Panel Report and the London Mayor's publication of the Intend to Publish New London Plan. The Planning Inspectors' Panel Report accepted the need for London to deliver 66,000 new homes per annum (significantly higher than existing adopted targets) but questioned the London Plan's ability to deliver the level of housing predicted on "small sites" with insufficient evidence having been presented to the Examination to give confidence that the targets were realistic and/or achievable. This conclusion resulted in the Panel Report recommending a reduction in London's and Croydon's "small sites" target.
- 7.6 The Intend to Publish version of the New London Plan has accepted the reduction of Croydon's overall 10 year net housing figures - from 29,490 to 20,790 homes, with the "small sites" target reduced from 15,110 to 6,470 homes. Crucially, the lower windfall housing target for Croydon (641 homes a year) is not dissimilar to but slightly higher than the current adopted 2018 Croydon Local Plan target of 592 homes set for windfall sites each year.
- 7.7 Should the Secretary of State support the Intend to Publish New London Plan, the overall housing target in the New London Plan would be 2,079 new homes per annum (2019 – 2029) compared with 1,645 in the Croydon Local Plan 2018. Therefore, even with the possible reduction in the overall New London Plan housing targets (assuming it is adopted) Croydon will be required to deliver more new homes than our current Croydon Local Plan 2018 and current London Plan (incorporating alterations 2016) targets.
- 7.8 For clarity, the Croydon Local Plan 2018, current London Plan (incorporating alterations 2016) and South London Waste Plan 2012 remain the primary consideration when determining planning applications.
- 7.9 Croydon Local Plan (adopted February 2018)

- SP2 – Homes
- DM1 – Housing choice for sustainable communities
- SP4 – Urban Design and Local Character
- DM10 – Design and character
- DM13 – Refuse and recycling
- SP6 – Environment and Climate Change
- DM23 – Development and construction
- DM24 – Land contamination
- DM25 – Sustainable drainage systems and reducing flood risk
- SP7 – Green Grid
- DM27 – Biodiversity
- DM28 – Trees
- SP8 – Transport and Communications
- DM29 – Promoting sustainable travel and reducing congestion
- DM30 – Car and cycle parking in new development
- DM46 – South Croydon

7.10 Other relevant Supplementary Planning Guidance as follows:

- London Housing SPG, March 2016
- National Technical Housing Standards, 2015
- National Planning Practice Guidance
- Suburban Design Guide SPG (2019)

8 MATERIAL PLANNING CONSIDERATIONS

8.1 The main planning issues raised by the application that the committee must consider are:

1. Principle of development
2. Townscape and visual impact
3. Residential amenity of adjoining occupiers
4. Residential amenity of future occupiers
5. Highways and transport
6. Trees and environment
7. Environment and sustainability
8. Other planning considerations

Principle of development

8.2 The London Plan and Croydon Local Plan identify appropriate use of land as a material consideration to ensure that opportunities for development are recognised and housing supply optimised. It is acknowledged that windfall schemes which provide sensitive renewal and intensification of existing residential areas play an important role in meeting demand for larger properties in the Capital, helping to address overcrowding and affordability issues.

- 8.3 The site is located within an existing residential area. The application proposes 2 buildings for 8 flats which would provide additional homes within the borough, which the Council is seeking to promote. The scheme includes 2 x 3 bedroom (one 6 person and one 4 person) family units and 2 x 2 bedroom (4 person) family units and so contributes to meeting the Council's strategic objective of providing 30% of new homes as family homes. Whilst the surround area is characterised by houses, the provision of a flatted scheme is not contrary to policy.
- 8.4 Policy DM10 supports back land development subject to the impact on the character of an area and the amenities of adjoining properties. Given the site has been subdivided with a frontage onto Ballards Rise (with a previous planning permissions having been granted in 2015/16) officers are satisfied that the proposal does not constitute backland development (in its purist sense) and therefore, DM10.4 (e) is not relevant in this instance. The gardens of 31 and 33 Croham Valley Road would however have garden lengths in excess of 10m and each would be in excess of 200 square metres.
- 8.5 In respect to the density of the scheme, representations have raised concern over the intensification of the site and overdevelopment. The site is a suburban setting with a PTAL rating of 1b and as such, the London Plan indicates that the density levels ranges of 150–200 hr/ha habitable rooms per hectare (hr/ha); the proposal would be in excess of this range (321 hr/ha). However, the London Plan density matrix is a guide and cannot be used as a prescriptive measure. Furthermore, the London Plan provides sufficient flexibility for higher density schemes to be supported – subject to detailed consideration of the various effects of a higher density of development. It should be noted the London Plan is currently being revised and the density figures are intended to be removed from the plan. As such, there would be insufficient grounds for refusal based on this particular matter.

Townscape and Visual Impact

- 8.6 Policy DM10.1 and DM46 sets out that developments should seek to achieve three storeys whilst respecting the character of the area, particularly development pattern, layout and siting; scale, height, mass and density; and appearance, materials, features and the Place of Croydon.
- 8.7 The proposed buildings would be located to the rear of 31 and 33 Croham Valley Road. Ballards Rise is a cul-de-sac which have 3 large detached properties at rear. The proposed buildings would front onto Ballards Rise (albeit with side entrances). The proposal is for two buildings (blocks of flats) which would be set within the topography of the site and would be stepped in height. Whilst the proposed buildings would be larger than the neighbouring houses (and larger than the scheme the subject of previous planning permissions) it is considered that the scale and massing of the proposed development would respond well to local character and topography. It is considered the proposed development would be acceptable in terms of its

impact on the character of the area and would not lead to a cramped form of development.

8.8 The proposed buildings would be set back from the pavement with bin storage incorporated into the building. 7 parking spaces would be provided for the proposed development which would be located in front of the building. Cycle storage would also be located within the building which would reduce visual clutter. Full details of the materials would be secured by condition. These would add interest to the development and soften the appearance of the development in the street scene. Soft landscaping would also be provided which would help the development integrate with the existing street and provide a buffer for future occupiers. These can be secured by condition. Below is a plans and illustrative image of how the development would integrate into its surroundings.





- 8.9 It is acknowledged that planning permission has been granted for lower buildings on the plot (ground/lower ground floor level) and schemes previously refused on the site for larger development forms (with refusals of planning permission between 2007 and 2014). These however were for a smaller site to the one proposed and were prior to the adoption of the Croydon Local Plan 2018 and London Housing Supplementary Planning Guidance and the Council's own Suburban Design Guide SPD. The Croydon Local Plan 2018 did not re-introduce the "protection of wooded hillsides" policy in the previous plan – which was a significant element of previous refusals and introduced specific reference to three-storey buildings being acceptable, subject to the issues set out below. As such there has been a significant change in policy since previous schemes were refused – and even since the most recent planning permission for the 4 bedroom detached house in 2015/16.
- 8.10 The site has partially been cleared with some periphery planting to the boundaries. To the rear of the properties on Ballards Rise is Metropolitan Green Belt/Metropolitan Open Land. Whilst the proposal would be visible from Ballards Rise and potentially from Ballards Farm Road, it is considered the proposed site would be adequately screened and sufficient distance to not have an impact on the sense of openness of the Metropolitan Green Belt. A detailed landscaping strategy has been provided, showing screening/trees along the side and rear boundaries, which can be secured by condition.



- 8.11 Whilst the materials would not match those in the area, it is considered the proposed development would be acceptable given the buildings would be set back from the road frontage and built into the topography of the land and given the buildings/roof form, have been designed in order to minimise their appearance from the street scene with soft landscaping. Officers are satisfied with the design quality of the scheme which would relate well to the Ballard way street-scene – making the most of topographical features.
- 8.12 Representations raised with regard to loss of garden land/amenity space. The land has been sub-divided and therefore does not form the rear gardens of properties in Croham Valley Road. Amenity space would be provided in line with policy.
- 8.13 Having considered all of the above, against the backdrop of housing need, officers are satisfied that the proposed development would comply with the objectives of the above adopted policies in terms of respecting local character.

Residential amenity of adjoining occupiers

policy. Moreover all flats would have an adequate level of light and outlook. Unit 1 and Unit 2 show a study room. The rooms are under 7.5m² and therefore not classified as a bedroom.



8.17 The ground floor units would have private external amenity space, the uppers floor flats would each have private amenity space and a small external communal area at rear. The Croydon play space calculator states an area of 12.2m² should be provided for play space. It is considered there is sufficient space at rear to provide a play space which would accord Policy DM10.

8.18 Internal cycle storage is shown close to the main entrance to each blocks, convenient for future occupiers. The 14 spaces would be provided which accord with policy requirements.

8.19 The bin store has been designed to be integral with the building. A waste calculator has been provided to show the bin store would provide adequate space for the

refuse requirements of the development. It is also considered the siting of the bin storage is considered acceptable. There are 3 houses at far end of Ballards Rise and it is likely the refuse collection arrangements would align with existing protocols.

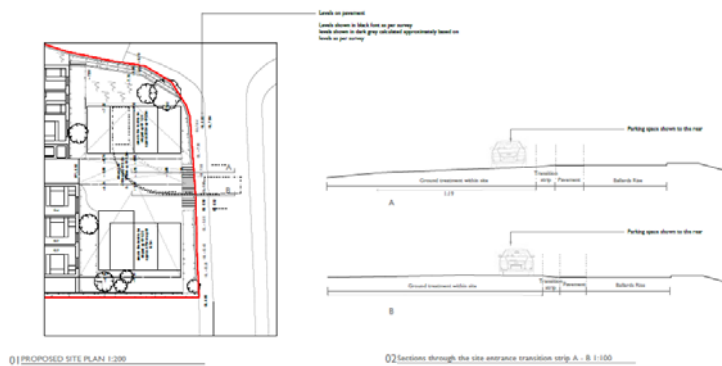
- 8.20 Level access would be provided to the ground floor flat of block B only due to the topography of the site. The ground floor unit would be M4(3) compliant providing a wheelchair accessible unit although these are matters controlled and managed through the Building Regulation process. A ramp could be provided by condition to the other ground floor unit if required but given the topography of the site and the constrained nature of the site it is considered acceptable.

Highways and Parking

- 8.21 The site has a PTAL rating of 1b which means very poor accessibility to public transport links. That said, there is a bus stop in close proximity to the site with regular services to Selsdon, South Croydon and East Croydon districts. Under the provision of the London Plan requirements 8 off-street spaces would be required for the proposed development and the current proposal is providing 7 parking spaces. The Suburban Design Guide SPD indicates that the borough will encourage lower parking provision than the maximum car parking standards set in both the current and draft new London Plan. However it further advises that in areas of very low transport accessibility (such as in areas of PTAL 0-1) it will be harder to access sustainable transport and therefore it may be more difficult to reduce reliance on private cars. It advises that in these area the Council will seek to accommodate all parking within the site (off street) and any anticipated need for on street parking will be judged on a case by case basis. Whilst this approach recognises the reality that the private car might continue (for the time being) to be the preferred transport choice of future residents in this area, it recognises that the issue should be considered on a case by case basis.
- 8.22 In this case, 7 parking spaces are proposed for 8 flats of which 2 flats are 1 bedroom flats. The applicants have undertaken a parking stress test that indicates that there is an adequate level of spaces to park on neighbouring roads to accommodate any overspill of parking from the proposed development (mostly limited to Ballards Way). The survey data indicates that the surrounding area has sufficient capacity to accommodate off-street parking, with maximum stress levels of 30% experienced during the week (11 of 43 spaces parked) indicating that the potential overspill can be accommodated on street without affecting existing residential amenity. Having considered the number of spaces proposed and the proximity of the site to public transport services adjacent to the above site (albeit limited) and parking availability in the vicinity, it is considered (on balance) that the proposed development would provide an adequate level of parking on site and the area would be able to accommodate any potential overspill of parking.
- 8.23 Policy SP8 and DM29 seeks to manage use of the private car and promote sustainable travel. DM30 requires a car club space to be provided on nine unit schemes, where there is likely to be interest from an operator. Whilst there is

sufficient parking on site and on street to ensure that the impact on the network is satisfactory, sustainable travel should still be promoted in accordance with these policies. Therefore, a contribution is recommended, and has been agreed with the applicant, towards the provision of sustainable travel measures, most likely to be traffic management measures on Ballards Rise (to ensure that overspill parking does not obstruct the road) and the provision of a car club space in the local area, which will help mitigate overspill parking as well as encouraging sustainable travel

8.23 The vehicular access would be located on a slope but this is not considered to lead to a significant risk to highway safety. Visibility splays can be provided. It should be noted that the previously refused applications all made similar provisions for access and parking and were not refused on those grounds.



8.24 Objections relating Ballards Rise relating to Access road too narrow – cul-de-sac/small to accommodate vehicular access and parking/blocking access to existing houses and blocking emergency vehicles, Impact on highway safety/insufficient parking/impact on Ballards Rise/impact on emergency vehicles/parking report/impact on pets and children. It is of relevance that a vehicle could currently halt on Ballards Rise and cause the same issue. The parking stress survey acknowledges that Ballards Rise is too narrow to allow vehicles to park on street and it is anticipated (not unreasonably) that future users of the street will continue to observe common sense. The Police and highways enforcement officers have powers relating to obstructing the highway which could be used if necessary and it is considered unreasonable to refuse planning permission for this reason as a sufficient level of parking would be available (on and off street – away from Ballards Rise). In other words, the issue already exists and this scheme should not render the situation any worse (as long as users of the road apply common sense). It should also be noted that the Council propose to place double yellow lines in Ballards Rise, in order to prevent people from parking there.

8.25 A construction logistics plan during the construction works would need to be conditioned. Various highway works would need to be carried out in Ballards Rise to facilitate access arrangements which would require a separate highways agreement under S.278 of the Town and Country Planning Act.

Trees and Environment

- 8.27 At the time of the officer's site visit, the site had partially been cleared with some periphery planting to the boundaries. No arboricultural objection has been raised. Therefore hard/soft landscaping including trees could be secured by condition.

Environment and Sustainability

- 8.28 Conditions can be attached to ensure that a 19% reduction in CO2 emissions over 2013 Building Regulations is achieved and mains water consumption would meet a target of 110 litres or less per head per day.
- 8.29 A Flood Risk Assessment (FRA) has been submitted. The site is within Flood Zone 1 and an area at very low risk of surface water flooding. The Flood Risk Assessment could be conditioned and there are opportunities to incorporate SUDs as part of a landscaping scheme for the site, which could be dealt with by condition. This would prevent a significant impact on flood risk.

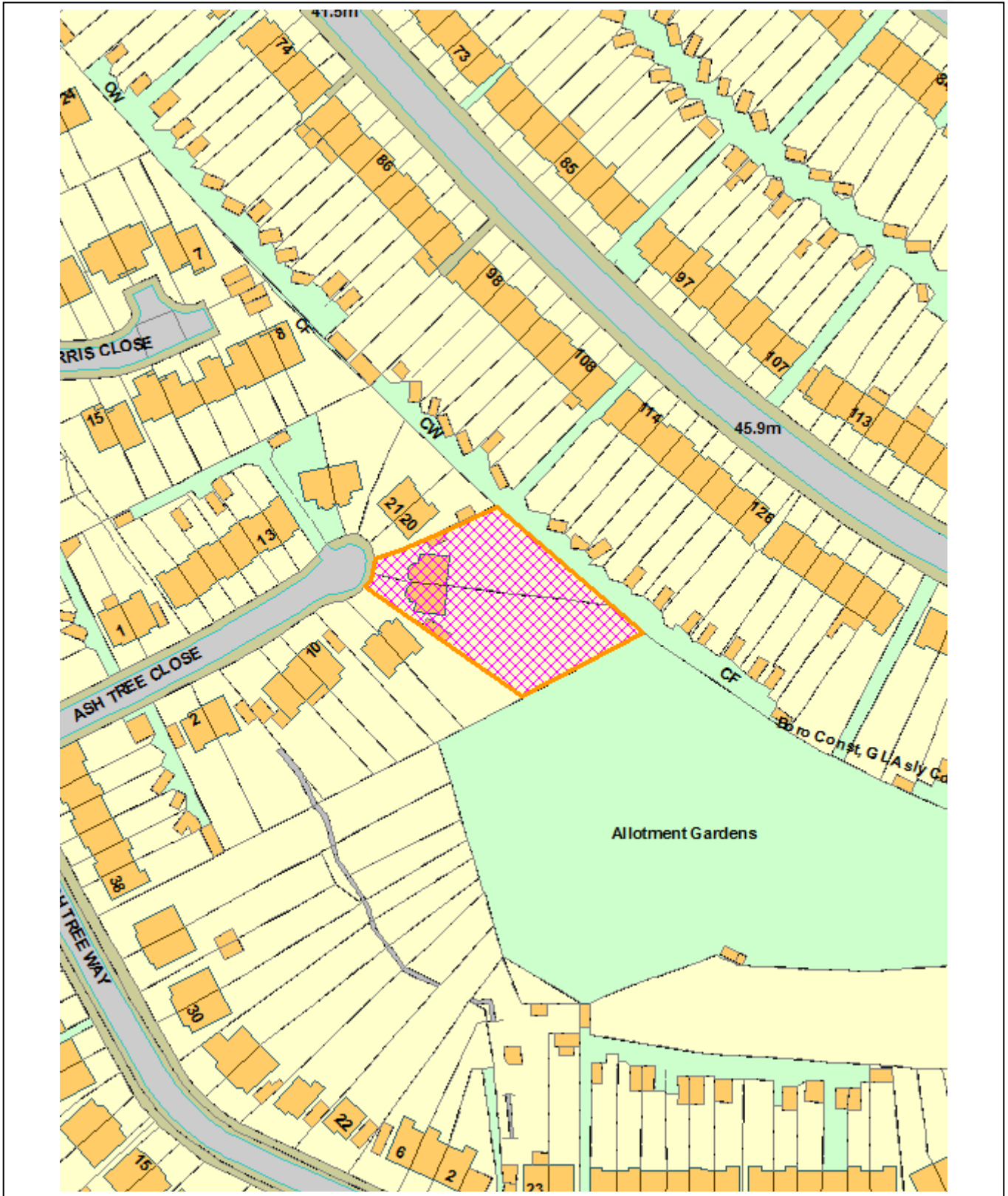
Other Planning Considerations

- 8.30 Objections relating to wildlife/natural vegetation are noted however there is no evidence of protected species on site. Given the site has partially been cleared, a landscaping condition is recommended and wildflower planting is proposed.
- 8.31 A construction Logistics Plan would be secured as a pre-commencement condition.
- 8.32 The development would be CIL liable. The levy amount has been calculated to ensure that the development contributes to meeting the need for physical and social infrastructure, including educational and healthcare facilities.

Conclusions

- 8.33 It is recommended that planning permission should be granted for the proposal, as it would be acceptable in all respects, subject to conditions.
- 8.34 All other relevant policies and considerations, including equalities, have been taken into account.

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PART 6: Planning Applications for Decision

Item 6.6

1.0 SUMMARY OF APPLICATION DETAILS

Ref: 19/04705/FUL
 Location: 16-18 Ash Tree Close, Croydon, CR0 7SR
 Ward: Shirley North
 Description: Demolition of the existing dwellings. Erection of 8 dwellings with associated access, parking, refuse and cycle stores.
 Drawing Nos: 919:1130/PL101 Rev C, 919:1130/PL102 Rev D, 919:1130/PL103 Rev C, 919:1130/PL104 Rev C, 919:1130/PL105 Rev A, 919:1130/PL106 Rev C and the site location plan scaled at 1:1250.
 Applicant: Mr Robert Turner (Turnbull Land)
 Agent: N/A
 Case Officer: Wayne Spencer

	1 bed, 2 person	2 bed, 3 person	2 bed, 4 person	3 bed, 5 person
Houses	0	0	0	8

Number of car parking spaces	Number of cycle parking spaces
8	16

1.1 This application is being reported to committee because the Ward Councillor Sue Bennett and Councillor Richard Chatterjee have made representations in accordance with the Committee Consideration Criteria and requested committee consideration and the number of representations which have been received are above the threshold of the Committee Consideration Criteria.

2.0 RECOMMENDATION

2.1 That the Planning Committee resolve to GRANT planning permission subject to the conditions below:

1. Commence within 3 years
2. In accordance with the approved plans
3. Demolition and Construction Logistics Plan
4. External facing materials (including samples) to be approved
5. Hard and soft landscaping to be approved (to incorporate SuDS)
6. Details of lighting
7. Refuse/cycle stores to be installed/retained in perpetuity
8. Obscure glazing to upper floor north west and south east facing windows
9. Tree Protection in accordance with Arboricultural Report
10. Access road and car parking spaces to be provided as shown
11. EVCPs to be provided for parking spaces
12. 19% carbon dioxide reduction
13. Water usage off 110L per head per day
14. All units to be Part M(4)2 compliant
15. Removal of Permitted Development rights

16. Any other planning condition(s) considered necessary by the Director of Planning and Strategic Transport

Informatives

- 1) Community Infrastructure Levy
- 2) Code of practice for construction sites
- 3) Any other informative(s) considered necessary by the Director of Planning and Strategic Transport.

3.0 PROPOSAL AND LOCATION DETAILS

Proposal

3.1 The applicant seeks full planning permission for the following:

- Demolition of the existing dwellings on site
- Erection of 8 x three bed dwellings
- Refuse and cycle stores to all new properties
- Associated private amenity spaces
- Associated hard and soft landscaping
- 8 parking spaces on site (1 per dwelling)

Site and Surroundings

3.2 The application site lies at the eastern end of Ash Tree Close at the end of the cul-de-sac. The site currently has a pair of semi-detached dwellings with east facing garden spaces which are to be demolished as a result of this proposal.

3.3 The surrounding area is residential in character with properties in Ash Tree Close, Ash Tree Way and Aylesford Avenue being predominantly 2-storeys in height with some properties which have accommodation included in the roof spaces over. The majority of the dwellings are terraced or semi-detached and are of similar character, form and design. The land to the south east is an allotment accessed from Aylesford Avenue.

3.4 The site is not within a Conservation Area and the building in question is neither nationally nor locally listed. The application site is within a PTAL 1a which is considered to have poor public transport options and is also at 'very low' risk of surface water flooding.

Planning History

3.5 08/01150/P – Demolition of existing buildings; erection of 6 two storey four bedroom terraced houses with accommodation in roof space; formation of access road and provision of associated parking – Permission refused

3.6 19/03263/PRE – Erection of 8 x 3-bed dwellings – Amendments suggested to improve the scheme

4.0 SUMMARY OF KEY REASONS FOR RECOMMENDATION

- The principle of the development is acceptable given the residential character of the immediate locality and the extant planning permission.

- The design, form and appearance of the development is appropriate given the context of surrounding area.
- The living conditions of adjoining occupiers would be protected from undue harm.
- The living standards of future occupiers are satisfactory and meet the National Housing Space Standards.
- The highway impact on the surrounding area would be acceptable.
- Sustainability aspects are controllable through the use of planning conditions.
- Flood risk mitigation measures are controllable through the use of planning conditions

5.0 CONSULTATION RESPONSE

5.1 The views of the Planning Service are expressed in the MATERIAL PLANNING CONSIDERATIONS section below.

6.0 LOCAL REPRESENTATION

6.1 The application has been publicised by way of consultation letters sent to the properties which are adjacent to the application site. The number of representations received from neighbours, local groups etc in response to notification and publicity of the application were as follows:

No of individual responses: 59 Objecting: 59 Supporting: 0

6.2 The following issues were raised in representations. Those that are material to the determination of the application are addressed in substance in the MATERIAL PLANNING CONSIDERATIONS section of this report:

Summary of objections	Response
<i>Townscape</i>	
<ul style="list-style-type: none"> • Overdevelopment of the site leading to overcrowding • Change to established Ash Tree Close building line • Gable ended semi-detached buildings out of keeping with Ash Tree Close • Detrimental to the character of the area • Application to redevelop this site with 6 new dwellings was refused in 2008 – why is a larger quantum of development now acceptable? • Impact upon heritage of the area • Lack of street lighting to serve the development 	See paragraphs 8.3 – 8.6
<i>Neighbouring amenity</i>	
<ul style="list-style-type: none"> • Loss of light and overshadowing impact • Overlooking and loss of privacy • Increase noise from additional residents • Noise and air pollution impact and disturbance during construction works • Lack of communal amenity/play spaces for children 	See paragraphs 8.12 – 8.15

<i>Environment</i>	
<ul style="list-style-type: none"> • Loss of garden (green) space • Loss of trees and the impact upon wildlife/CO2 reductions/health • Lack of soft landscaping proposed • Lack of green space for future occupiers 	See paragraphs 8.6, 8.10, 8.21 and 8.22
<i>Highways and Refuse</i>	
<ul style="list-style-type: none"> • Lack of parking provision (and visitor parking) potentially resulting in detrimental highways impact • Concerns over highway (including pedestrian) safety and manoeuvrability within the site • Swept paths encroach on neighbouring plots • Poor access for emergency or large delivery vehicles (during and post construction) • Poor refuse collection access • Lack of refuse storage provision • Refuse management of communal bin store and potential of vermin being attracted if not properly managed 	See paragraphs 8.16 – 8.18
<i>Flooding</i>	
<ul style="list-style-type: none"> • Increased flooding impact due to proximity of Chaffinch Brook 	See paragraph 8.20
Other comments	Response
<ul style="list-style-type: none"> • Pressure on local infrastructure with doctors and schools will be oversubscribed 	See paragraph 8.15
<ul style="list-style-type: none"> • Density of the development for a PTAL 1a does not conform to London Plan 	See paragraph 8.17
<ul style="list-style-type: none"> • No CIL payments made for Shirley North 	See paragraph 8.15
<ul style="list-style-type: none"> • No affordable housing 	Not required as the scheme is for less than 10 units
<ul style="list-style-type: none"> • Children will no longer be able to play in the street – detrimental to their mental health and wellbeing 	There is no requirement for play space to be provided – all dwellings have their own private amenity space which is policy compliant – See paragraph 8.10

6.3 The following comments have been received but are not material to the determination of this application and will require no further assessment:

Summary of comments	Response
Loss of a view	Not a material planning consideration
Damage to neighbouring property/boundary treatment	These matters are not material planning considerations and are covered by alternative legislation (Party Wall Act)

Neighbour does not want any planting to overhang the boundary	This would be a civil matter and not a material planning consideration
Concern over security of neighbouring property during construction works	This would be a civil matter between the neighbour and the developer
Compliance with fire regulations	Considered under Building Regulations
Impact on sewers, drainage and gas supply as a result of additional properties	Not a material planning consideration
Impact upon property prices	Not a material planning consideration
Impact on the community ruining the relationships built up between neighbours	Not a material planning consideration
Site not currently underused as suggested in the submission	Not a material planning consideration

6.4 Councillor Sue Bennett has objected and referred the application to Planning Committee on the following grounds:

- Inappropriate, exceptionally high housing and residential densities
- inadequate parking due to low PTAL rating
- extreme overlooking and invasion of privacy

6.5 Councillor Richard Chaterjee has objected and referred the application to planning Committee on the following grounds:

- Proposed Housing Density totally inappropriate given the PTAL 1a, rating
- Cumulative impact of the proposed, and other recently approved developments, is changing the character of the area without compensatory growth in local infrastructure, GP surgeries or public transport
- Full GIA dimensions not provided for each dwelling
- Minimum in-built storage space not stated on drawings
- Insufficient parking allocation
- Fails to respect the scale, height, massing and density of the surrounding properties
- Inadequate vehicle space within the site
- Access/egress swept paths encroach on neighbouring curtilages
- No allocation figures for amenity space allocation for each dwelling
- Loss privacy and overlooking
- No new street lighting shown for the new access road
- Inadequate refuse storage
- Refuse/cycle storage not integrated with the design concept and appear to be bolted-on afterthoughts
- Building workers' cars would cause significant obstruction to the local road network
- Demolition debris and delivery of construction materials will cause significant disruption and inconvenience to local residents
- The development is extremely close to flood risk from the Chaffinch Brook and therefore would exacerbate the existing potential for flood risk in this locality
- Additional development and loss of trees in the locality would create a further flood risk to surrounding properties in Ash Tree Way and Ash Tree Close
- Overdevelopment for the locality and does not respect the existing residential and housing densities

- The siting and layout would not respect or improve the existing pattern of buildings and the spaces between them
- Would appear a dominant and extremely poorly designed, out of character element in the street scene

6.6 Monks Orchard Residents Association (MORA) have objected to the application and their comments have been included in paragraph 6.2 above. Their main objections are on the following grounds:

- Similar scheme previously refused in 2008
- Overdevelopment – scale and massing inappropriate
- Inappropriate residential densities
- Inadequate parking due to low PTAL rating
- Lack of public transport options
- No CIL allocation to Shirley North
- Not convinced over space standard compliance
- Parking not screened from the road
- Inadequate manoeuvrability to allow access/egress to/from parking spaces
- Lack of amenity space per dwelling
- Overlooking/loss of privacy
- Out of character, harmful to street scene
- Lack of street lighting
- Inappropriate refuse/cycle storage
- Inadequate parking for construction vehicles
- Lack of flood risk mitigation

7.0 RELEVANT PLANNING POLICIES AND GUIDANCE

7.1 In determining any planning application, the Council is required to have regard to the provisions of its Development Plan so far as is material to the application and to any other material considerations and the determination shall be made in accordance with the plan unless material considerations indicate otherwise. The Council's adopted Development Plan consists of the Consolidated London Plan 2015, the Croydon Local Plan 2018 and the South London Waste Plan 2012.

7.2 Government Guidance is contained in the National Planning Policy Framework (NPPF), issued in February 2019. The NPPF sets out a presumption in favour of sustainable development, requiring that development which accords with an up-to-date local plan should be approved without delay. The NPPF identifies a number of key issues for the delivery of sustainable development, those most relevant to this case are:

- Promoting sustainable transport;
- Delivering a wide choice of high quality homes;
- Requiring good design.

7.3 The main policy considerations raised by the application that the Planning Committee is required to consider are:

- 3.3 Increasing housing supply
- 3.4 Optimising housing potential
- 3.5 Quality and design of housing developments

- 3.8 Housing choice
- 5.1 Climate change mitigation
- 5.2 Minimising carbon dioxide emissions
- 5.3 Sustainable design and construction
- 5.12 Flood risk management
- 5.13 Sustainable drainage
- 5.16 Waste net self sufficiency
- 6.3 Assessing effects of development on transport capacity
- 6.9 Cycling
- 6.13 Parking
- 7.2 An inclusive environment
- 7.3 Designing out crime
- 7.4 Local character
- 7.6 Architecture
- 7.21 Woodlands and trees

7.4 Croydon Local Plan 2018:

- SP1.1 Sustainable development
- SP1.2 Place making
- SP2.1 Homes
- SP2.2 Quantities and location
- SP2.6 Quality and standards
- SP4.1 and SP4.2 Urban design and local character
- SP4.11 regarding character
- SP6.1 Environment and climate change
- SP6.2 Energy and carbon dioxide reduction
- SP6.3 Sustainable design and construction
- SP6.4 Flooding, urban blue corridors and water management
- SP8.6 and SP8.7 Sustainable travel choice
- SP8.12 Motor vehicle transportation
- SP8.17 Parking
- DM1: Housing choice for sustainable communities
- DM10: Design and character
- DM13: Refuse and recycling
- DM16: Promoting Healthy Communities
- DM23: Development and construction
- DM25: Sustainable Drainage Systems and Reducing Flood Risk
- DM29: Promoting sustainable travel and reducing congestion
- DM30: Car and cycle parking in new development

7.5 There is relevant Supplementary Planning Guidance as follows:

- London Housing SPG March 2016
- Suburban Design Guide Supplementary Planning Document 2019

Emerging New London Plan

7.6 Whilst the emerging New London Plan is a material consideration, the weight afforded is down to the decision maker linked to the stage a plan has reached in its

development. The Plan appears to be close to adoption. The Mayor's Intend to Publish version of the New London Plan is currently with the Secretary of State and no response had been submitted to the Mayor from the Secretary of State. Therefore, the New London Plan's weight has increased following on from the publication of the Panel Report and the London Mayor's publication of the Intend to Publish New London Plan. The Planning Inspectors' Panel Report accepted the need for London to deliver 66,000 new homes per annum (significantly higher than existing adopted targets), but questioned the London Plan's ability to deliver the level of housing predicted on "small sites" with insufficient evidence having been presented to the Examination to give confidence that the targets were realistic and/or achievable. This conclusion resulted in the Panel Report recommending a reduction in London's and Croydon's "small sites" target.

- 7.7 The Mayor in his Intend to Publish New London Plan has accepted the reduced Croydon's overall 10 year net housing figures from 29,490 to 20,790 homes, with the "small sites" reduced from 15,110 to 6,470 homes. Crucially, the lower windfall housing target for Croydon (641 homes a year) is not dissimilar to but slightly larger the current adopted 2018 Croydon Local Plan target of 592 homes on windfall sites each year.
- 7.8 It is important to note, should the Secretary of State support the Intend to Publish New London Plan, that the overall housing target in the New London Plan would be 2,079 new homes per annum (2019 – 2029) compared with 1,645 in the Croydon Local Plan 2018. Therefore, even with the possible reduction in the overall New London Plan housing targets, assuming it is adopted, Croydon will be required to deliver more new homes than our current Croydon Local Plan 2018 and current London Plan (incorporating alterations 2016) targets.
- 7.9 For clarity, the Croydon Local Plan 2018, current London Plan (incorporating alterations 2016) and South London Waste Plan 2012 remain the primary consideration when determining planning applications.

8.0 MATERIAL PLANNING CONSIDERATIONS

- 8.1 The main planning issues raised by the application that the committee must consider are:
- Principle of development
 - Townscape and visual impact
 - Housing Quality for future occupiers
 - Residential amenity for neighbours
 - Transport
 - Sustainability

Principle of development

- 8.2 The appropriate use of land is a material consideration to ensure that opportunities for development are recognised and housing supply optimised. The site is currently in residential use and has not been designated in the local plan, to be used for any other purpose. The dwellings to be demolished are family dwellings and it is proposed to erect 8 x 3-bed dwellings to replace these. As such, the proposal would be in accordance with the requirements of Croydon Local Plan 2018 Policy DM1.2. The site is outside of the Shirley Intensification Area and would therefore be considered a

'windfall' site. As such, the development would be considered acceptable in principle provided that respects the character and appearance of the surrounding area and there are no other impact issues.

Townscape and visual impact

- 8.3 The site currently contains two dwelling houses which are of two storeys in height with roof space over. The overall height of the proposed buildings would be three storeys in height and would introduce two blocks of four 3-bed dwellings with every 2 properties stepped back to create the appearance of pairs of semis when viewed from the Ash Tree Close.
- 8.4 Ash Tree Close is typically characterised by 2-storey semi-detached and terraced properties, some with roof space accommodation. All surrounding properties have similar scale and massing with traditionally styled appearance. The proposal would involve removing 2 semi-detached dwellings which naturally address the curve of the road however there is no objection to the removal of these dwellings and intensifying the residential capacity of the site in question, subject to appropriate parking, amenity spaces and refuse/cycle provision being made available. The dwellings would follow the slope of the land and would therefore appear slightly higher than the dwellings in Ash Tree Close when viewed from the road however the proposed dwellings are laid out in a staggered form which expresses the dwellings as pairs. Given the massing and height of the proposed buildings in relation to the scale and massing of the surrounding properties, it is considered that the scale and massing of the buildings would be acceptable in this location as the ridge heights are broadly in line with the existing contextual buildings. As this approach references the predominant contextual typologies of the area, it would be acceptable in this context. The height differences would not dominate the existing dwellings in Ash Tree Close due to the staggered nature and the massing is further broken up by the staggering. The building line to no.20 Ash Tree Close would be maintained given the generous offset from the front and rear boundaries. Although fully gabled dwellings are not typical of Ash Tree Close, the proposed dwellings being set back into the site would not address the street in the same way as the existing properties. As a result, the design approach with gable ends and bay features would be acceptable. As the dwellings are of a traditional styling, it is not considered that they would be significantly out of keeping with, or detrimental to, the existing street scene or the heritage of the area. Third party comments refer to a previous refusal for 6 dwellings in 2008 (reference 08/01150/P) however this decision pre-dates the adoption of the current Local Plan, the London Plan and the current Supplementary Planning Guidance. The current scheme conforms to the current development plan and associated documents and therefore the previous refusal can only be afforded limited weight when determining this case.
- 8.5 The dwellings would introduce front dormer windows to all new dwellings which are set in at both sides to prevent the front elevation being dominated by this massing. Plots 5-8 would introduce first floor level chamfered windows to the front elevation however this approach would not be unduly incongruous. The use of contrasting material treatment reinforces the pairing of the dwellings with a good use of brick detailing. The porch detailing gives good legibility to the entrances and the flank windows and features to the flank walls assist with breaking up the brick massing and add architectural interest. The juxtaposition of the built form combined with the design of the buildings would prevent the development from appearing incongruous with the surrounding built form. Suitable materials would be secured by planning condition.

8.6 The boundary treatment adjoining Ash Tree Close is shown as being 2 metre high hedging and a number of trees are to be planted to soften the appearance of the development from the public realm. The same boundary treatment would be included to both sides and the rear boundaries. This approach would be considered acceptable and all soft landscaping could be secured by planning condition. In addition, a suitable lighting scheme could also be secured by planning condition to justify acceptability.



Housing quality for future occupiers

8.7 The proposal results in an increased density on the site by eight additional residential units, all of which would be 3-bed, 5 person units. The scheme exceeds the density matrix (150-200) as set out within the London Plan at approximately 300 habitable rooms per hectare. However, given suburban setting combined with the similar footprint, form and spacing of the proposed dwellings in comparison to the surrounding properties, the acute need for new homes and the fact that the site is very close to the intensification area of Shirley, it is considered an appropriate density for this site.

8.8 The dwellings would need to be compliant with M4(2) of the Building Regulations providing step free access to these units for any future disabled residents and this would appear to be the case. It is considered that compliance with M4(2) rather than M4(3) would be acceptable in this case given the overall scale of the proposed development.

8.9 The National Space Standards and the London Plan states that 3-bed 5 person dwellings split over three floors should provide a minimum internal floor space of 99m². The floor plans show that all eight dwellings measure approximately 106sqm. Having assessed the room sizes and the associated fenestration detailing on the proposal, the habitable rooms of all proposed dwellings would have a good outlook and would have adequate sized windows to allow a significant amount of natural light to enter all of the habitable rooms within these dwellings.

combined with the juxtaposition, design and massing of the development would prevent the dwellings from having a significantly overbearing impact upon either nos.14 or 20 and both properties will achieve a good level of daylight and sunlight to the rear facing windows and their associated rear garden spaces. .

- 8.13 Croydon's Suburban Design Guide SPD requires a separation distance of at least 18 metres to be maintained between the existing habitable room windows of the adjacent building and the habitable room windows of any new build elements. In addition, the first 10 metres of rear garden space of the existing dwellings would need to be protected from any overlooking resulting from this development. The fenestration arrangement, in particular plots 5 and 6, and the positioning of the openings were not acceptable initially as the upper floor front facing windows of plots 5 and 6 overlooked the first 10 metres of the rear garden of no.14. However, the scheme has now been re-designed to provide a chamfered element which now ensures that there would be no direct overlooking to the rear garden of no.14. The side boundary of the rear garden of no.14 Ash Tree Close required strengthening and additional planting was added. Since the chamfered element has been introduced, the density of this planting has been reduced and this would be acceptable given that the main reason for such planting was for overlooking mitigation purposes. The additional trees which are now proposed to the boundary of no.14 Ash Tree Close will assist in screening the development from no.14 as well as softening the appearance of the development from this property. The upper floor side windows being proposed to each of the dwellings break up the brick façades where the staggering of the built form occurs and allows for additional natural ventilation to the rooms they serve. These windows serve either non-habitable rooms/spaces or serve as secondary windows to the habitable rooms and, as such, they could be conditioned to be obscure glazed and fixed shut below 1.7m to protect the privacy of the future occupiers of the adjoining properties.
- 8.14 With regards to potential noise impact from future occupiers, although the residential density on the site would increase the building would need to meet current Building Regulations standards which include relevant sound proofing measures. Therefore, it is not considered that eight residential units in this location would result in a significant increase in noise disturbance. Noise and disturbance during construction works would be controlled by Environmental Health legislation relating to hours of construction and the need for site hoardings and are therefore not material planning considerations.
- 8.15 With regards to third party comments not addressed above, concern was raised regarding the impact that the development would have upon the local doctor's surgeries and school place provision. Given the overall scale of the proposed development and the fact that only eight family units are to be provided, it is not considered that the development would have a significant impact upon doctor's surgery and school provision to warrant the refusal of permission on these grounds. In addition, the development would be subject to the Community Infrastructure Levy (CIL), which would contribute financially to both health and education infrastructure.

Transport

- 8.16 The application site is in an area with a Public Transport Accessibility Level (PTAL) accessibility rating of 1a indicating poor access to public transport links and an enhanced reliance on private motor vehicles. The proposal does include one parking space per dwelling and the Transportation Team confirmed that a parking ratio of 1:1 would be acceptable provided that the vehicles are able to enter and leave the site

safely without the need for excessive reversing. A swept path analysis has been submitted with this application showing the manoeuvres required to get in and out of the proposed spaces and it is considered that there would be adequate space within the site to avoid excessive reversing. The parking spaces do include manoeuvres which encroach upon the delineated footpath in the site and the open frontage of the adjoining plots. However, the Transportation Team consider this arrangement to be acceptable as it relates to a relatively small number of spaces. It is recognised that large vehicles, including emergency vehicles, would find access into the site and manoeuvrability difficult however it is possible for vehicles to temporarily park at the end of Ash Tree Close in emergencies. It has been demonstrated that a fire appliance could park close to the site and the agent has confirmed that the fire hose would reach the required distance to the rearmost part of the furthest house from the fire appliance. The proposal would appear to conform to the required fire standards under Building Regulations however these matters are not material considerations under this planning application and will need to satisfy Building Regulations and the requirements of the London Fire Commissioner should planning permission be granted. The development would be subject to an acceptable Demolition/Construction Logistics Plan in order to prevent undue noise and air pollution during the construction works and to ensure that construction vehicles use appropriate delivery routes and park their vehicles in a suitable location. The submission of this could be controlled by planning condition.

- 8.17 The proposed development would require covered secure cycle storage to be provided in accordance with the standards set out in the London Plan and the London Cycling Design Standards with 2 cycle spaces per dwelling (a total of 16 spaces). The cycle spaces are shown to be located within the rear gardens of each of the dwellings apart from plots 6 and 7 who will have a cycle store at the end of the access road. It is considered that these would be easily accessible for future occupiers however details of the appearance of the cycle stores would need to be secured by planning condition to ensure that it meets London Plan requirements and has no significant impact upon the character or appearance of the area.

Refuse storage

- 8.18 The refuse vehicle would not be able to enter the site and turn within it. The site would rely upon the existing refuse collection service which operates in Ash Tree Close and therefore the development would need to conform to the Council's Refuse and Recycling Guidance. The refuse collection area is within 30 metres of each dwelling and the collection area is within 20 metres of the collection vehicle (i.e. the closest point a refuse vehicle can access). Having a refuse store within each of the dwellings themselves would have compromised the internal spaces and the external stores are to be screened, secure and would be integral to the landscaped area as a whole. The Council would require these stores to be retained for as long as the development exists and the capacity and appearance of these stores would be secured by planning condition.

Sustainability

- 8.19 Conditions would be imposed requiring a 19% carbon dioxide emission reduction target and a water use target of 110L per head per day, in line with policy requirements. No renewable energy provisions have been shown on the submitted documentation however such provision will be secured by planning condition.

Flood Risk

8.20 The site itself is within an area which is at 'very low' risk of surface water flooding. Surface Water Drainage is proposed to be addressed via connection to Thames Water's surface water drainage system and SuDS in the form of permeable paving in order to disperse surface water to the soft landscaped areas and reduce surface water run-off. The Chaffinch Brook is approximately 35 metres from the site at its closest point and the site does not fall within an area at risk of flooding from this source. As such, the SuDS approach to this scheme is considered to be acceptable and the provision of such mitigation measures can be controlled via a suitably worded planning condition.

Trees and Ecology

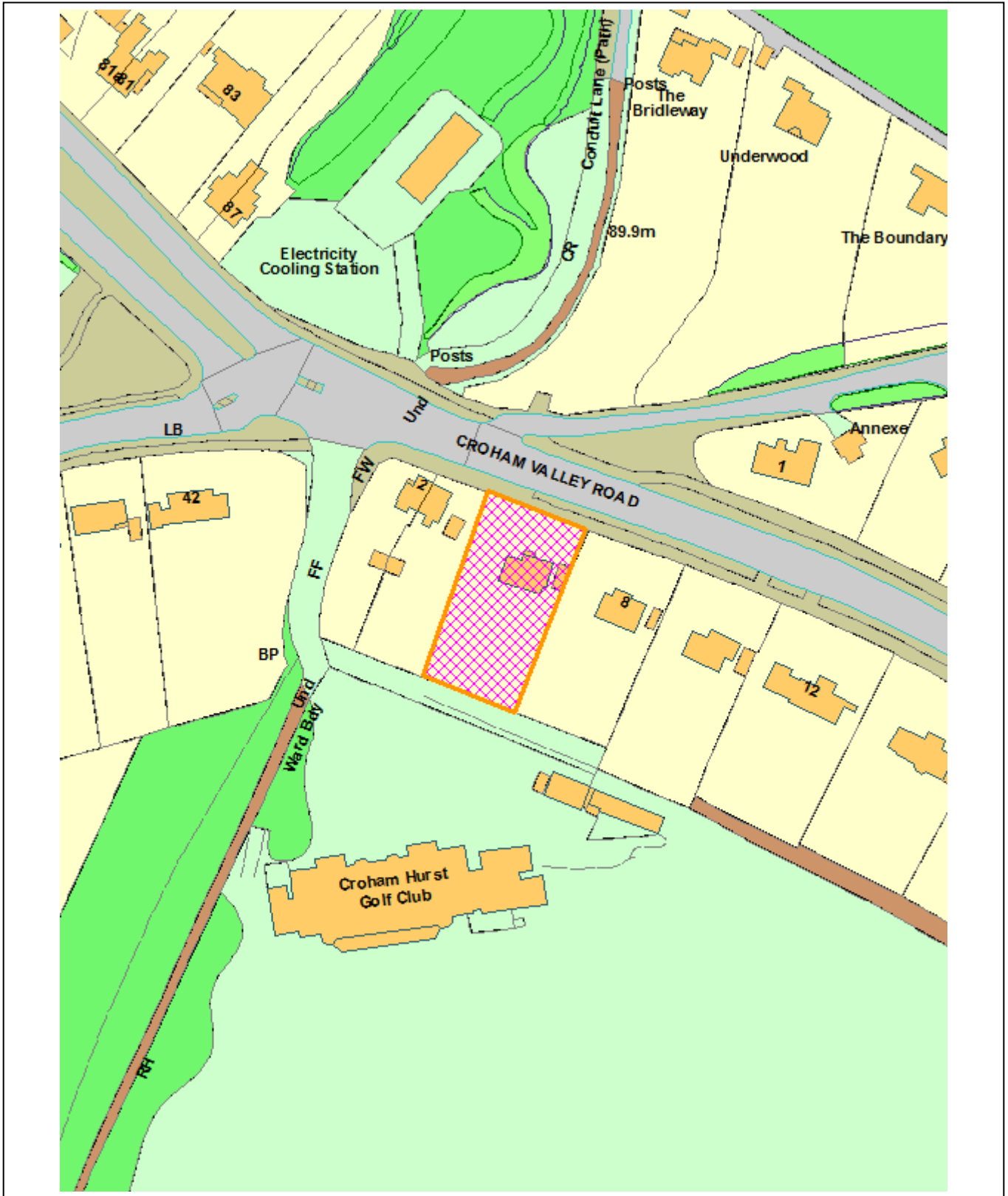
8.21 The proposed development would not involve the loss of any on-site trees and the existing trees surrounding the site are proposed to be retained. Arboricultural information submitted with the application has been assessed and the Council considered that all trees can be retained and can be adequately protected from damage during the construction phase. Additional planting of trees is proposed which will help to soften the appearance of the development and their strategic planting will allow safe access and egress from the site. In conclusion, the development would be acceptable from a tree perspective and the implementation of further on-site trees, including an appropriately chosen species, would be controlled by planning condition. It is not considered that the positioning of the proposed building would have a detrimental impact on the health (or future risk of intensive pruning) of the existing or proposed trees.

8.22 The site does not have any known biodiversity or ecology designations. As such, it is considered that the development would not have any significant impact upon ecology or biodiversity. The landscaping for the development would be subject to a planning condition.

Conclusion

8.23 The proposal would result in the optimal redevelopment of the site which would contribute to local housing need by providing a total of eight new homes within the Borough. The development would not be significantly harmful to the character of the area and would not have a significant impact on the amenities of adjoining occupiers. Landscaping, parking and energy systems are all acceptable in principle and can be secured by condition. It is therefore recommended that planning permission is granted.

8.24 All other relevant policies and considerations, including equalities, have been taken into account.



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1.0 SUMMARY OF APPLICATION DETAILS

Ref: 19/05034/FUL
 Location: 6 Croham Valley Road, South Croydon, CR2 7NA
 Ward: South Croydon
 Description: Demolition of existing house; erection of a two storey building plus roof space to provide 9 apartments; provision of 8 car parking spaces, refuse store and new landscaping.
 Drawing Nos: 100 Rev 1, 101 Rev 6, 102 Rev 8, 103 Rev 8, 104 Rev 8, 105 Rev 4, 200 Rev 7, 201 Rev 7, 202 Rev 4, 301 Rev 2, 400 Rev 4, 401 Rev 5, 402 Rev 5, 403 Rev 4, 2019/4836/001 Rev A.
 Applicant: Trinity Square Developments
 Agent: N/A
 Case Officer: Samantha Dixon

	1 bed	2 bed	3 bed	4 bed	5 bed
Existing			1		
Proposed flats	1 (1 x 2 person)	5 (5 x 4 person)	3 (3 x 5 person)	0	

All units are proposed for private sale

Number of car parking spaces	Number of cycle parking spaces
8	21

1.1 This application is being reported to committee because the ward councillor (Councillor Michael Neal) made a representation in accordance with the Committee Consideration Criteria and requested committee consideration and objections above the threshold in the Committee Consideration Criteria have been received.

2.0 RECOMMENDATION

2.1 That the Committee resolve to GRANT planning permission subject to the completion of a S.106 Agreement to secure the following heads of terms:

- A financial contribution of £6,000 towards highway management measures and the delivery of sustainable transport initiatives in and around Croham Valley Road and neighbouring streets.
- The costs associated with the removal and replanting of street trees (including on-going maintenance) to facilitate the formation of the crossover

2.2 That the Director of Planning and Strategic Transport has delegated authority to negotiate the legal agreement indicated above.

2.2 That the Director of Planning and Strategic Transport has delegated authority to issue the planning permission and impose conditions and informatives to secure the following matters:

Conditions

1. Time limit of 3 years
2. Development to be carried out in accordance with the approved drawings and reports except where specified by conditions
3. Details of site specific SuDS to be submitted prior to any above ground works
4. Flood resistance and resilience measures to be submitted prior to any above ground works
5. Biodiversity enhancement strategy to be submitted prior to any above ground works.
6. Details of materials to be submitted prior to any above ground works
7. Hard and soft landscaping including boundary treatment, retaining walls and maintenance to be submitted prior to occupation
8. Full details of cycle and bin stores to be submitted prior to occupation
9. Wildlife sensitive light design scheme to be submitted prior to occupation
10. Details of children's playspace to be submitted prior to occupation
11. Details of electric vehicle charging point to be submitted prior to occupation
12. Details of ramp to the rear to be agreed prior to occupation
13. 19% Carbon reduction
14. Construction Logistics Plan as submitted
15. No additional windows in the flank elevations
16. Obscure glazing to windows in flank elevations at first and second floor if below 1.7m
17. Inclusive access M4(2) and M4(3)
18. Car parking to be provided as specified
19. reinstatement of existing vehicle access
20. No obstruction over 0.6m in height in pedestrian visibility splays
21. Accordance with mitigation and enhancement measures outlined in the submitted Ecological Survey
22. Accordance with Construction environmental management plan (biodiversity)
23. 110litre Water usage
24. Any other planning condition(s) considered necessary by the Director of Planning and Strategic Transport

Informatives

- 1) Community Infrastructure Levy
- 2) Code of practise for Construction Sites
- 3) Highways works
- 4) Any other informative(s) considered necessary by the Director of Planning and Strategic Transport

2.3 That the Committee confirms that adequate provision has been made by the imposition of conditions, for the preservation or planting of trees as required by Section 197 of the Town and Country Planning Act 1990.

3.0 PROPOSAL AND LOCATION DETAILS

3.1 The proposal includes the following:

- Demolition of existing house
- Erection of a three storey building to create 9 residential units including 3 x 3 bed flats, 5 x 2 bed flats and 1 x 1 bed flats.

- Provision of communal external amenity space and children’s play space
- Provision of 8 off-street parking spaces
- Provision of associated refuse and cycle stores

3.2 During the course of the application amended plans have been received which alter the location of the bin and cycle stores and to provide information on visibility splays and vehicular manoeuvring within the site.

Site and Surroundings

3.3 The site is located to the south side of Croham Valley Road and comprises a detached dwelling with vehicular access and a large rear garden which slopes upwards from front to rear.

3.4 There are no specific local plan policy designations related to the site itself. The land to the rear is designated as metropolitan open land and land on the northern side of Croham Valley Road is a Site of Nature Conservation Importance and Croham Hurst Golf Club is immediately behind the site. The site has a PTAL of 2 which indicates relatively poor access to public transport.



Figure 1: Aerial street view highlighting the proposed site within the surrounding streetscene

Planning History

3.5 19/03579/PRE Proposed demolition of existing dwelling, erection of a building comprising 5 two bedroom flats, 3 three bedroom flats and 1 one bedroom flat.

3.6 The following applications at nearby sites are also of relevance:

3 Croham Valley Road (diagonally opposite but one site)

18/06067/FUL Demolition of existing building and the erection of a five storey building (two storey fronting Ballards Farm Road with basement levels) comprising 7 flats with creation of new access off Croham Valley Road, parking areas (7 spaces) refuse storage and landscaping

Application under consideration

5 Croham Valley Road

19/03628/FUL Demolition of the existing property and erection of 6 houses (3 houses fronting Croham Valley Road and 3 houses fronting Ballards Farm Close), gardens, car parking, new accesses, refuse and recycling.

A report into this proposal appears elsewhere on this agenda.

R/O 31-33 Croham Valley Road

19/04615/FUL) Erection of 2 two storey detached buildings with accommodation within the roof-space comprising 8 flats, bin store, the formation of vehicular access and provision of 7 parking spaces.

A report into this proposal also appears elsewhere on this agenda.

4.0 SUMMARY OF KEY REASONS FOR RECOMMENDATION

- The principle of the development is acceptable given the residential character of the surrounding area.
- The scheme is of a high quality design, utilising the contemporary reinterpretation approach the appearance of the development is appropriate, respecting the character of the surrounding area.
- The living conditions of adjoining occupiers would be protected from undue harm subject to conditions.
- The living standards of future occupiers are satisfactory and Nationally Described Space Standard (NDSS) compliant.
- The level of parking and impact upon highway safety and efficiency is considered acceptable and can be controlled through conditions and S106.
- Sustainability aspects can be controlled by conditions.

5.0 CONSULTATION RESPONSE

5.1 The views of the Planning Service are expressed in the MATERIAL PLANNING CONSIDERATIONS section below.

6.0 LOCAL REPRESENTATION

6.1 The application has been publicised by 10 letters of notification to neighbouring properties in the vicinity of the application site. The number of representations received

from neighbours in response to notification and publicity of the application are as follows:

No of individual responses: 22 Objecting: 22 Supporting: 0 Comment: 0

6.2 The following issues were raised in representations. Those that are material to the determination of the application, are addressed in substance in the MATERIAL PLANNING CONSIDERATIONS section of this report:

Objection	Officer comment
<i>Design and appearance</i>	
Massive overdevelopment of the site with the building occupying most of the site that is evidenced by how much it extends beyond the rear building line of the neighbouring properties. Will add 20 bedrooms and potentially 37 persons	Addressed in Section 8.16 of this report.
By virtue of its bulk, mass and poor design would create an overbearing incompatible building that fails to integrate into the neighbourhood, causing significant harm to the appearance of the site, the surrounding area and street scene. Has a very unattractive, irregular shape and a contemporary style.	Addressed in Section 8.8 – 8.14 of this report.
Roof ridge line of unacceptably high	Addressed in Section 8.10 of this report.
Frontage of site dominated by parking bays	Addressed in Section 8.15 of this report.
Flats out of keeping in the area	Planning policies and the Suburban Design Guide advocate infill development for new residential units in the suburbs. There is no objection to the principle of flatted development in this area. Addressed in Section 8.4 of this report.
<i>Impact on amenities of neighbouring properties</i>	
The rear building line extends a totally unaccepted distance beyond that of the two neighbouring properties 4 and 8 Croham Valley Road.	Addressed in Sections 8.25 and 8.28 of this report.

A number of existing surrounding properties will suffer loss of privacy and visual intrusion due to there being multiple balconies.	Addressed in Sections 8.23 to 8.29 of this report.
Extra noise and disturbance	This is a residential development and there is no evidence or reason to suggest that the proposal would result in extra noise or disturbance that is not associated with a residential area. A condition will be imposed requiring compliance with the Construction Logistics Plan to ensure construction noise is not harmful to local residents.
<i>Transport and parking</i>	
Inadequate parking provision	Addressed in Sections 8.30 – 8.33 of this report.
Parking bays and turning area are too small meaning vehicles will park on the road	Addressed in Sections 8.36 of this report.
Cumulative impact of cars on road with other nearby developments	Addressed in Section 8.32 of this report.
Highway danger of cars parked on busy road and bus route	Addressed in Sections 8.32 and 8.33 of this report. The Transport Statement discounts parking of cars on Croham Valley Road as it is recognised that this is a busy main thoroughfare. There is adequate available parking on adjacent roads to accommodate any potential overspill parking.
Obstruct two bus stops	The proposed development would have a vehicular access in a similar position to the existing. The proposal would not obstruct any bus stop. Comments regarding parking on Croham Valley Road are addressed above.
<i>Amenities of future occupiers</i>	
Lack of both private amenity and communal amenity space for residents	Addressed in Sections 8.19 and 8.20 of this report.

No affordable housing provision	This is a minor development and there is no policy requirement for affordable housing.
Need for more family homes not flats.	The proposal would provide 3 x three bedroom unit and 5 x 2 bedroom 4 person units which is an increase in family units over the existing situation.
Other matters	
Will be visible from areas of Metropolitan Green Belt and/or Metropolitan Open Land	The site is not located in the Metropolitan Green Belt
Extra pressure on local services	The application is CIL liable. Addressed in Section 8.45 of this report.

6.4 The following Councillor has made representations:

- Cllr Michael Neale (South Croydon Ward Councillor) objected to the proposal and referred it to Planning Committee:
 - The development pattern, layout and siting does not enhance the local character of the area
 - It does not respect or enhance or strengthen the local character of the area
 - The scale, height, massing and density is too large for the site and therefore an overdevelopment
 - The amenity of the occupiers of adjoining buildings are not protected through loss of light

6.5 Croham Valley Resident's Association have objected to the proposal:

- Massive overdevelopment of the site with the building occupying most of the site that is evidenced by how much it extends beyond the rear building line of the neighbouring properties.
- Will add 20 bedrooms and potentially 37 persons
- By virtue of its bulk, mass and poor design would create an overbearing incompatible building that fails to integrate into the neighbourhood, causing significant harm to the appearance of the site, the surrounding area and street scene. Has a very unattractive, irregular shape and a contemporary style.
- The rear building line extends a totally unaccepted distance beyond that of the two neighbouring properties 4 and 8 Croham valley Road.
- A number of existing surrounding properties will suffer loss of privacy and visual intrusion due to there being multiple balconies.
- Lack of parking provision
- Parking bays and turning area are too small meaning vehicles will park on the road
- Roof ridge line of unacceptably high
- Building of 9 flats is out of character with existing properties

- Frontage of site dominated by parking bays
- Lack of both private amenity and communal amenity space for residents
- The building will be visible from areas of Metropolitan Green Belt and/or Metropolitan Open Land.

7.0 RELEVANT PLANNING POLICIES AND GUIDANCE

7.1 In determining any planning application, the Council is required to have regard to the provisions of its Development Plan so far as is material to the application and to any other material considerations and the determination shall be made in accordance with the plan unless material considerations indicate otherwise. The Council's adopted Development Plan consists of the Consolidated London Plan 2015, the Croydon Local Plan 2018 and the South London Waste Plan 2012.

7.2 Government Guidance is contained in the National Planning Policy Framework (NPPF), issued in February 2019. The NPPF sets out a presumption in favour of sustainable development, requiring that development which accords with an up-to-date local plan should be approved without delay. The NPPF identifies a number of key issues for the delivery of sustainable development, those most relevant to this case are:

- Promoting sustainable transport;
- Delivering a wide choice of high quality homes;
- Requiring good design.

7.3 The main policy considerations raised by the application that the Committee are required to consider are:

7.4 Consolidated London Plan 2015

- 3.3 Increasing housing supply
- 3.4 Optimising housing potential
- 3.5 Quality and design of housing developments
- 3.8 Housing choice
- 5.1 Climate change mitigation
- 5.2 Minimising carbon dioxide emissions
- 5.3 Sustainable design and construction
- 5.12 Flood risk management
- 5.13 Sustainable drainage
- 5.16 Waste net self sufficiency
- 6.3 Assessing effects of development on transport capacity
- 6.9 Cycling
- 6.13 Parking
- 7.2 An inclusive environment
- 7.3 Designing out crime
- 7.4 Local character
- 7.6 Architecture
- 7.21 Woodlands and trees

7.5 Croydon Local Plan 2018

- SP2 - Homes
- SP6.3 - Sustainable Design and Construction
- DM1 - Housing choice for sustainable communities
- SP4 – Urban Design and Local Character
- DM10 - Design and character
- DM13 - Refuse and recycling
- DM16 – Promoting healthy communities
- SP6 – Environment and Climate Change
- DM23 - Development and construction
- DM25 – Sustainable drainage systems and reducing floor risk
- SP7 – Green Grid
- DM27 – Biodiversity
- DM28 – Trees
- SP8 – Transport and communications
- DM29 - Promoting sustainable travel and reducing congestion
- DM30 - Car and cycle parking in new development

7.6 There is relevant Supplementary Planning Guidance as follows:

- London Housing SPG March 2016
- Croydon Suburban Design Guide Supplementary Planning Document April 2019

7.7 Emerging New London Plan

Whilst the emerging New London Plan is a material consideration, the weight afforded is down to the decision maker linked to the stage a plan has reached in its development. The Plan appears to be close to adoption. The Mayor's Intend to Publish version of the New London Plan is currently with the Secretary of State and no response had been submitted to the Mayor from the Secretary of State. Therefore, the New London Plan's weight has increased following on from the publication of the Panel Report and the London Mayor's publication of the Intend to Publish New London Plan. The Planning Inspectors' Panel Report accepted the need for London to deliver 66,000 new homes per annum (significantly higher than existing adopted targets), but questioned the London Plan's ability to deliver the level of housing predicted on "small sites" with insufficient evidence having been presented to the Examination to give confidence that the targets were realistic and/or achievable. This conclusion resulted in the Panel Report recommending a reduction in London's and Croydon's "small sites" target.

The Mayor in his Intend to Publish New London Plan has accepted the reduced Croydon's overall 10 year net housing figures from 29,490 to 20,790 homes, with the "small sites" reduced from 15,110 to 6,470 homes. Crucially, the lower windfall housing target for Croydon (641 homes a year) is not dissimilar to but slightly larger the current adopted 2018 Croydon Local Plan target of 592 homes on windfall sites each year.

It is important to note, should the Secretary of State support the Intend to Publish New London Plan, that the overall housing target in the New London Plan would be 2,079 new homes per annum (2019 – 2029) compared with 1,645 in the Croydon Local Plan 2018. Therefore, even with the possible reduction in the overall New London Plan housing targets, assuming it is adopted, Croydon will be required to deliver more new homes than our current Croydon Local Plan 2018 and current London Plan (incorporating alterations 2016) targets.

For clarity, the Croydon Local Plan 2018, current London Plan (incorporating alterations 2016) and South London Waste Plan 2012 remain the primary consideration when determining planning applications.

8.0 MATERIAL PLANNING CONSIDERATIONS

8.1 The main planning issues raised by the application that the Planning Committee are required are as follows:

1. Principle of development
2. Townscape and visual impact
3. Housing quality for future occupiers
4. Residential amenity for neighbours
5. Access and parking
6. Trees, landscaping and ecology
7. Sustainability and environment
8. Other matters

Principle of Development

8.2 This application must be considered against a backdrop of significant housing need, not only across Croydon, but also across London and the south-east. All London Boroughs are required by the London Plan to deliver a number of residential units within a specified plan period. In the case of the London Borough of Croydon, there is a requirement to deliver a minimum of 32,890 new homes between 2016 and 2036 (Croydon's actual need identified by the Croydon Strategic Housing Market Assessment would be an additional 44,149 new homes by 2036, but as there is limited developable land available for residential development in the built up area, it is only possible to plan for 32,890 homes). This requirement is set out in policy SP2.2 of the Croydon Local Plan (CLP) (2018), which separates this target into three relatively equal sub targets with 10,760 new homes to be delivered within the Croydon Opportunity Area, 6,970 new homes as identified by specific site allocations for areas located beyond the Croydon Opportunity Area boundary and 10,060 homes delivered across the Borough on windfall sites. The draft London Plan, which is moving towards adoption (although in the process of being amended) proposes significantly increased targets which need to be planned for across the Borough. In order to provide a choice of housing for people in socially-balanced and inclusive communities in Croydon, the Council will apply a presumption in favour of sustainable development of new homes.

8.3 This presumption includes South Croydon, which is identified in the "Places of Croydon" section of the CLP (2018) as being an area for sustainable growth of the suburbs with a mix of windfall and infill development that respects the existing residential character and local distinctiveness. The Croydon Suburban Design Guide (2019) has recently been adopted, which sets out how suburban intensification can be achieved to high quality outcomes and thinking creatively about how housing can be provided on windfall sites. As is demonstrated above, the challenging targets will not be met without important windfall sites coming forward, in addition to the large developments within Central Croydon and on allocated sites.

8.4 The application is for a flatted development providing additional homes within the borough, which the Council is seeking to promote. The site is located within an existing

residential area and as such providing that the proposal accords with all other relevant material planning considerations, the principle of development is supported.

- 8.5 CLP Policy DM1.2 seeks to prevent the net loss of 3-bedroom homes (as originally built) and homes less than 130m². The existing building on site is a 3 bedroom house with a floor area of approximately 137sqm. All of the proposed units have floor spaces of less than 130sqm and 3 of the new units would comprise three bedrooms. There would therefore be no net loss of homes under 130sqm or three-bedroom homes as required by Policy DM1.2.
- 8.6 Policy SP2.7 seeks to ensure that a choice of homes is available to address the borough's need for homes of different sizes and that this will be achieved by setting a strategic target for 30% of all new homes up to 2036 to have three or more bedrooms. CLP policy goes on to say that within three years of the adoption of the plan, an element may be substituted by two-bedroom (four person) homes. The application proposes 3 x 3 bedroom units and 5 x 2 bedroom 4 person units. Overall, the proposal provides a net gain in family accommodation and contributes towards the Council's goal of achieving a strategic target of 30% three bedroom plus homes.

Townscape and Visual Impact

- 8.7 The existing dwelling on site is a two storey detached property dating back to the 1930s/40s. It is clad in white render with a pitched roof and double garage to the side. The building does not hold any significant architectural merit and therefore there is no objection to its demolition.
- 8.8 CLP Policy DM10.1 states that proposals should achieve a minimum height of 3 storeys whilst respecting a) the development pattern, layout and siting; b) the scale, height, massing and density; and c) the appearance, existing materials and built and natural features of the surrounding area.
- 8.9 The dwellings to the east of the site along Croham Valley Road are large detached properties of a full two storeys. No.2-4 Croham Valley Road (the first building when approaching from the west) is a semi-detached pair with the first floor rooms contained partly within the roof space. This area generally has a traditional character, comprising mostly individual dwellings with generous gaps between them. The buildings have mostly rendered, brick or tiled elevations with pitched tiled roofs.
- 8.10 The Suburban Design Guide (SDG) suggests appropriate ways of accommodating intensified development on sites depending on the character, height and type of surrounding buildings in the area. The character of the immediate surrounding area does not fall distinctly into any of the ideologies set out in the SDG. In this case, it is considered that the proposed two storeys with accommodation in the roofspace is an appropriate design solution for this site. It conforms to the three storeys required by local plan policy whilst ensuring that the building is not overly dominant from the western approach and in comparison to No.2-4. The overall ridge height of the proposal sits above that of the neighbouring properties either side, however the sloping roofs and the distance between the properties allows a distinct gap to be retained between the buildings.



Figure 2: Plan of proposed frontage within the street scene

8.11 The front of the building sits in line with the neighbouring property at No.8 and well behind the building at No.4 and thereby retains the established building line on this side of Croham Valley Road.

8.12 The new building is conceived as a contemporary interpretation of the traditional suburban house form that dominates 20th century architecture such as the dwellings found along Croham Valley Road. The building has the appearance of a single dwelling within the street scene. It has a gabled front elevation with hipped roofs. The building consist of a plinth at ground floor level with 1.5 storeys of accommodation above. The plinth and offset gable with chimney feature successfully breaks up the massing of the building and create a built form that appears consistent with the prevailing scale along Croham Valley Road. Front dormers are proposed but they are lightweight and do not dominate the roof form.



Figure 3. Visual interpretation of front elevation

8.13 The front of the proposed building has a strong visual presence and a clear design approach which is enhanced by the use of brickwork and simple palette of materials. It is proposed to utilise red brick to the elevations with red tiles to the roof and part of the façade. Red brick, red tiles and hanging tiles can be found on properties in the surrounding area. This would be offset by the use of dark frames to the fenestration and balustrades. Full details of external materials will be secured by condition.

- 8.14 The building has a greater footprint than the current house however given the layout of the buildings in this row the impact on the appearance of the wider area is not harmfully affected. The building is set in from the side boundaries and the roof slope maintains a visual gap between the plots. Whilst the proposed built form is significantly deeper than the adjacent properties, this width steps in considerably at the rear and the overall mass will not be readily apparent from any public vantage points.
- 8.15 The existing dwelling has a large area of hardstanding to the frontage with two accesses onto the highway. It is noted that numerous dwellings on Croham Valley Road also have large expanses of hardstanding to the front of the buildings. The extent of hard surfacing at the front of the site would be increased to provide a parking forecourt. One of the accesses would be stopped up and a more centralising access provided. A beech hedge border is proposed along the rest of the front boundary to provide a soft buffer between the hardstanding and highway. An area of soft landscaping is also proposed to the western part of the frontage. Overall, given the existing situation and introduction of additional soft landscaping, the proposed hardstanding would not have an overly dominant or incongruous impact on the visual amenities of the area. The refuse and cycle stores would be discretely located to the side of the building which is welcomed.
- 8.16 The site has a suburban setting with a PTAL rating of 2 and as such the London Plan indicates that the density levels ranges of 150-250 habitable rooms per hectare (hr/ha) are appropriate. The proposal would provide 294 hr/ha. However, the London Plan further indicates that it is not appropriate to apply these ranges mechanistically, as the density ranges are broad, to enable account to be taken of other factors relevant to optimising potential – such as local context, design and transport capacity. The application site is a large plot within an established residential area and is comparable in size to other flatted and neighbouring back-land developments approved throughout the borough. As outlined above, the proposal would overall result in a development that would respect the pattern and rhythm of neighbouring area and would not harm the appearance of the street scene.
- 8.17 Therefore, having considered all of the above, against the backdrop of housing need, officers are of the opinion that the proposed development would create a high quality contemporary reinterpretation that would comply with the objectives of the above policies in terms of respecting local character.

Housing Quality for Future Occupiers

- 8.18 All of the proposed new units would comply with internal dimensions required by the Nationally Described Space Standards (NDSS). The majority of units are dual aspect, and the single aspect units are all south facing. Overall, the quality of proposed internal amenity space is considered acceptable.
- 8.19 With regard to external amenity space, the London Housing SPG states that a minimum of 5sqm of private outdoor space should be provided for 1-2 person dwellings and an extra 1sqm for each additional unit. All units have private amenity spaces that meet or exceed the required standards.
- 8.20 A communal garden (approximately 250sqm) is provided at the rear of the site, accessed to the side of the building. A childrens play space is shown to be provided

within the communal garden space and full details of this area will be secured by condition.

8.21 In terms of accessibility, step-free access is provided into the building and to the communal garden. Unit 1 is shown to be a wheelchair user dwelling (building regulations M4(3) compliant) and the other two ground floor units would be accessible and adaptable dwellings (building regulations M4(2) compliant). This would be secured by condition. A ramped access to the rear communal space is proposed. A condition is recommended regarding the detailed design of this so that it can provide facilities for those in wheelchairs. A disabled parking bay is also proposed.

8.22 Overall, the development is considered to result in a high quality development including 3 x three bedroom units and good amount of family accommodation, all with adequate amenities and provides an acceptable standard of accommodation for future occupiers.

Residential Amenity for Neighbours

8.23 The main properties that would be affected by the proposed development are 4 and 8 Croham Valley Road.

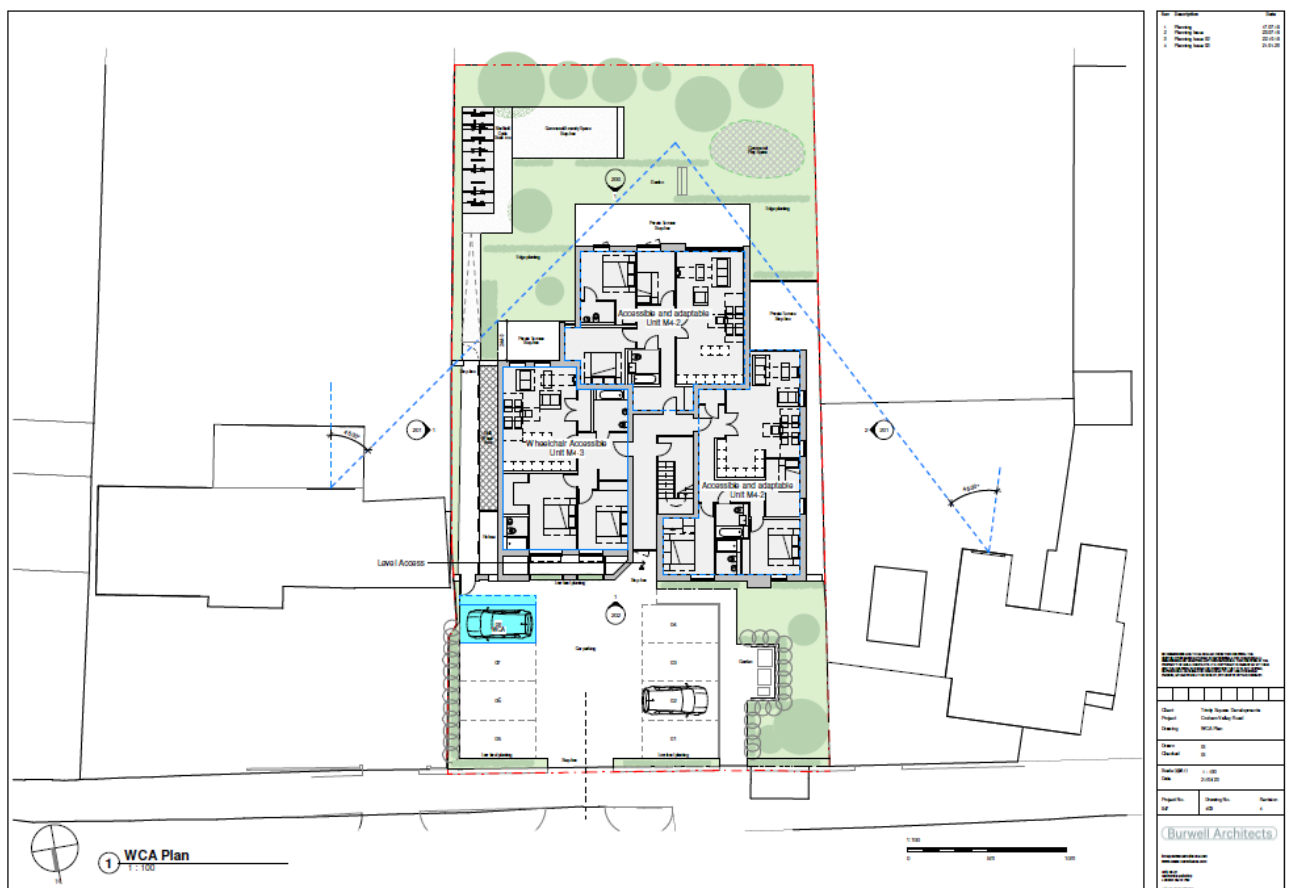


Fig 4: Proposed Block Plan highlighting the relationship with the adjoining occupiers.

4 Croham Valley Road

8.24 This semi-detached dwelling is located to the north west of the site. This building is located approximately 8m from its side boundary and has detached garage to its eastern side. It has a first floor east facing window in the roof gable facing the

application site as well as two ground floor side windows, one to the front entrance porch.

- 8.25 As per the existing building on site, the proposed building sits further back on the site than No.4. Whilst the proposal building extends significantly deeper into the plot than No.4, the rear element is stepped and set off the boundary. The development does not encroach over a 45 degree angle from the rear windows of No.4 either horizontally or vertically. Therefore the proposal would not be unduly overbearing or cause an unacceptable loss of outlook from the rear elevation.
- 8.26 No.4 has windows in its side elevation, approximately 8m from the shared boundary. The house is divided from the application site by its detached garage as well as mature landscaping. The proposed building sits on a building line to the rear (south east) of these windows and as such it is not considered that the proposed building would have any harmful impact on outlook from them. One ground floor window serves the entrance porch to No.4 and the second window in this side is located next to the garage and therefore outlook and light to this window is already impeded. The first floor window is located in the roof gable and the proposed building does not impede a 25 degree angle from this window, meaning that it is unlikely that the development would cause any harmful impact in terms of light to it. Further, Paragraph 2.9.3 of the SDG outlines that daylight and sunlight analysis study will not normally be required where a neighbour's window directly faces onto or over an application site in a manner that is considered to be un-neighbourly. These un-neighbourly windows place undue restraints on the development, and as such the light and outlook they receive will not receive significant protection.
- 8.27 There are no windows proposed that would cause any loss of privacy to No.4. The proposed ground floor windows in the side elevation would face the boundary fence. The first floor level side facing windows and secondary and would be conditioned to be obscurely glazed and non-opening below 1.7m above floor level. Overall, the impact on No.4 is acceptable.

8 Croham Valley Road

- 8.28 This building is located to the south east of the application site. It is a two storey detached dwelling with an attached double garage adjacent to the application site. The proposed building does not encroach over a 45 degree angle from the rear windows of No.11 either horizontally or vertically. Therefore the proposal would not be unduly overbearing or cause an unacceptable loss of outlook from the rear elevation and given the orientation of the buildings would cause no harmful loss of light.
- 8.29 There are no windows proposed that would cause any loss of privacy to No.8. The only side facing first floor window is secondary and would be conditioned to be obscurely glazed. Overall, the impact on No.8 is acceptable.

Parking and access

- 8.30 The site has a PTAL rating of 2 which means that it has relatively poor access to public transport links. It is however noted that there is a bus stop directly outside the site which provides 2 bus routes (64 and 433). The London Plan sets out maximum car parking standards for residential developments based on public transport accessibility levels and local character. 1-2 bedroom units should provide less than 1 space per unit

and 3 bedroom units up to 1.5 spaces per unit. The maximum required for the proposed scheme would be 10.5 spaces.

- 8.31 It is proposed to create 8 vehicular parking spaces off road all from a single access from Croham Valley Road. Based on the above maximum parking standards, there could be a maximum shortfall of 2.5 spaces.
- 8.32 A parking stress survey has been undertaken to Lambeth Methodology which has also considered the cumulative impact of this development with other consented schemes or submitted planning applications in the surrounding area. The survey shows that a minimum of 24 vacant parking spaces out of a capacity of 34 spaces are available overnight (20 if the other developments are taken into account equating to 41% existing parking stress). It is important to note that the survey discounted any parking on Croham Valley Road given that it is a main busy thoroughfare and noted that parking is only possible to one side of Manor Way. Given the low parking stress in the area, it is considered that the additional of a potential 2.5 extra vehicles parking on street would not have a significantly harmful impact on highway safety in this instance, the proposal potentially resulting in a maximum parking stress of 48.5%.
- 8.33 Policy SP8 and DM29 seeks to manage use of the private car and promote sustainable travel. DM30 requires a car club space to be provided on nine unit schemes, where there is likely to be interest from an operator. Whilst there is sufficient parking on site and on street to ensure that the impact on the network is satisfactory, sustainable travel should still be promoted in accordance with these policies. Therefore, a contribution is recommended, and has been agreed with the applicant, towards the provision of sustainable travel measures, most likely to be traffic management measures on Croham Valley Road (to ensure that the bus route is not affected by any overspill parking) and the provision of a car club space in the local area, which will help mitigate overspill parking as well as encouraging sustainable travel. Similar arrangements have been agreed with the developers of 5 Croham Valley Road and the R/O 31-33 Croham Valley Road. Taking into account the site's accessibility to public transport, relevant car ownership data and capacity for on-street are parking to accommodate any overspill, the proposal would provide for an appropriate number of parking spaces which would not detrimentally impact highway safety within the surrounding area.
- 8.34 Local Plan Policy DM30 states that 20% of parking bays should have EVCP with future provision available for the other bays. Details and provision of the EVCP will be conditioned. One disabled bay is shown to be provided for the wheelchair accessible unit.
- 8.35 Access to the parking area would be in a similar position to one of the existing vehicular accesses at the site. The existing second access would be stopped up. Croham Valley Road is a relatively straight road and has good visibility in both directions. The access arrangement is acceptable and a condition to ensure there is no obstruction within pedestrian visibility splays will be imposed on any permission.
- 8.36 The parking bays shown on the plans and gap between them are adequate to ensure that vehicles can park on site and can enter and leave the site in a forward gear.
- 8.37 A cycle storage area would be provided within the building. 17 cycle parking spaces would need to be provided in line with London Plan requirements (1 space for 1 bed flats and 2 spaces for all other units). The plan shows that 20 cycles could be stored

in line with this requirement. The plans have been amended to provide a cycle store within the rear amenity space to ensure that the proposed stands are of an accessible style for easy use of all residents. Full details of the appearance of this structure would be required by condition.

- 8.38 Refuse storage – as originally proposed the bins were shown to be located to the side of the building. However, the drag distance to the highway for operatives would have been over 20m and therefore the bins have been relocated to the front of the site and would be screened to the front and side by soft landscaping. The plans demonstrate that the size of the structure is adequate to accommodate bins required. Full details of its appearance and landscaping would be required by condition. An area for bulky waste would be provided to the side of the building.
- 8.39 A Construction Logistics Plan (CLP) has been submitted. The Council's Highway's and Environmental Health teams find the CLP acceptable and the details within the CLP would be secured by condition.

Trees, landscaping and ecology

- 8.40 The site is not covered by any Tree Preservation Order. The development would result in a loss of some mature trees and landscaping in the rear garden of the site. These trees have no amenity value and there is no objection to their removal. In accordance with Local Plan Policy DM10.8, a comprehensive landscaping scheme needs to be provided to compensate for the loss of the existing trees. The applicant has subsequently provided an illustrative landscaping plan showing replacement tree planting to the front and rear of the building as well as other hedging and shrubbery. Full details of soft landscaping including a maintenance plan will be secured by condition.
- 8.41 There is a street tree located to the front of the site which would need to be removed to facilitate the development. The Council's Tree & Woodlands Officer has commented that the tree to be removed is a beech sapling and there is no objection to its removal subject to the applicant funding its replacement on a 1 to 4 ratio for new trees to be planted on the highway in the local area along with their maintenance. The applicant has agreed to make a contribution to cover the cost of the replacement trees and this would be secured by legal agreement.
- 8.42 Ecology – The existing house would be demolished, there are trees on the site that would be removed and the garden is currently very overgrown. The land to the rear of the site is designated as metropolitan open land and land on the northern side of Croham Valley Road is a Site of Nature Conservation Importance. The applicant has provided a Preliminary Ecological Appraisal which has been reviewed by the Council's Ecological consultant. The consultant has no objection to the proposal subject to securing biodiversity mitigation and measurement by condition. These conditions are recommended to be attached to any permission granted.

Environment and sustainability

- 8.43 Conditions can be attached to ensure that a 19% reduction in CO2 emissions over 2013 Building Regulations is achieved and mains water consumption would meet a target of 110 litres or less per head per day.

8.44 The site is located within an area at medium/high risk of surface water flooding resulting from heavy rainfall and surface water runoff and medium risk of flooding from rivers. The site also has potential for groundwater flooding to occur at the surface. A Flood Risk Assessment and SUDS/Drainage Report (FRA) has been submitted as part of the application which outlines the risks of flooding at the site. The report considers sustainable drainage methods for the site and suggests that the following methods could be employed; soakaway (although testing is still required); surface water swale (although no details of how this could be accommodate have been provided) and permeable paving. A condition requiring site specific SuDS measures would be imposed on any planning permission granted. The FRA also outlines potential flood resilience measures for the ground floor units to protect against groundwater flooding. Site specific measures will also be secured by condition.

Other matters

8.45 The development will be liable for a charge under the Community Infrastructure Levy (CIL). This payment will contribute to delivering infrastructure to support the development of the area, such as local schools.

Conclusions

8.46 The principle of residential development is considered acceptable within this area. The development has successfully been designed as a contemporary reinterpretation of the traditional dwellings found in the area. The development accords with the Suburban Design Guide in terms of its massing and overall impact on the visual amenities of the area. With the imposition of conditions the proposal would have no harmful impact on the adjacent properties and provides adequate amenity for future residents. The applicant has demonstrated that the proposal would have an acceptable impact on the highway network and a contribution towards sustainable transport measures will be secured by legal agreement. The loss of existing trees on site would be mitigated by replacement tree planting and the removal of the existing street tree would be mitigated by replacement tree planting secured by legal agreement. The proposal is considered to be accordance with the relevant polices.

8.47 All other relevant policies and considerations, including equalities, have been taken into account.

PLANNING COMMITTEE AGENDA

PART 8: Other Planning Matters

1 INTRODUCTION

- 1.1 In this part of the agenda are reports on planning matters, other than planning applications for determination by the Committee and development presentations.
- 1.2 Although the reports are set out in a particular order on the agenda, the Chair may reorder the agenda on the night. Therefore, if you wish to be present for a particular application, you need to be at the meeting from the beginning.
- 1.3 The following information and advice applies to all those reports.

2 FURTHER INFORMATION

- 2.1 Members are informed that any relevant material received since the publication of this part of the agenda, concerning items on it, will be reported to the Committee in an Addendum Update Report.

3 PUBLIC SPEAKING

- 3.1 The Council's constitution only provides for public speaking rights for those applications being reported to Committee in the "Planning Applications for Decision" part of the agenda. Therefore reports on this part of the agenda do not attract public speaking rights.

4 BACKGROUND DOCUMENTS

- 4.1 For further information about the background papers used in the drafting of the reports in part 7 contact Mr P Mills (020 8760 5419).

5 RECOMMENDATION

- 5.1 The Committee to take any decisions recommended in the attached reports.

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PLANNING COMMITTEE AGENDA

27th February 2020

Part 8 Other Planning Matters

Item 8.1

Report of: Director of Planning and Strategic Transport Author: Pete Smith	Title: Weekly Planning Decisions and Performance
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1. Purpose

- 1.1 This report provides a list of cases determined (since the last Planning Committee) providing details of the site and description of development (by Ward), whether the case was determined by officers under delegated powers or by Planning Committee/Sub Committee and the outcome (refusal/approval).

Planning Decisions

- 1.2 Attached as Appendix 1 is the list of delegated and Planning Committee/Sub Committee decisions taken between 27th January and 14th February 2020.
- 1.4 During this period the service issued 286 decisions (ranging from applications for full planning permission, applications to discharge or vary planning conditions, applications for tree works, applications for prior approval, applications for non-material amendments and applications for Certificates of Lawful Development). 9 applications were withdrawn by the applicants (which also appear on the list).
- 1.5 Out of the 286 decisions issued, 52 were refused (18%). Therefore the approval rate for last week was 82%.
- 1.6 Notable decisions are listed below
- Planning permission has recently been issued in respect of the redevelopment of 29-35 Russell Hill Road – involving the erection of two buildings comprising 106 new apartments with associated car parking and hard and soft landscaping (LBC Ref 19/03604/FUL). This case was considered by the Planning Committee at its meeting of the 19th December 2020, which also resolved to grant planning permission for the redevelopment of the neighbouring site (37 Russell Hill Road) to provide a further 47 apartments (LBC Ref 19/00467/FUL). Planning permission for this neighbouring site was issued on the 23rd December 2019.
 - On 13th February 2020, planning permission was refused for the redevelopment of the bungalow and garage at 3B Godstone Road involving the erection of a three/four storey building comprising 5 flats with associated bin store, formation of vehicular access and provision

of 6 parking spaces at rear (LBC Ref 19/01384/FUL). The reasons for refusal focussed on the schemes failure to respect the character and appearance of the immediate area, the quality of residential accommodation, the effect of the scheme on immediate neighbours, the failure to provide adequate refuse storage and inadequate details provided to satisfy tree protection and the ability of vehicles to access and enter the safely.

- On 13th February 2020, planning permission was refused for the conversion of existing dwelling-house to a 6-rooms HMO (C4) following the construction of a two-storey side extension, a loft conversion and amendments to existing fenestration; in addition to the construction of a detached two-storey dwelling-house (C3) with associated car parking spaces, hard and soft landscaping - following the division of the plot for 141 Brancaster Lane (19/05357/FUL). The reasons for refusal related to the siting and mass of the proposed detached building - failing to respect the character and appearance of the area/street-scene and adequacy of cycle and refuse storage/collection arrangements. Members may recall that planning permission had previously been granted in respect of this property in May 2019 for the erection of two storey side/rear and roof extensions and conversion into 6 flats with associated parking, balconies and landscaping (LBC Ref 19/00885/FUL).
- On 4th February 2020, planning permission was refused for the redevelopment of the existing bungalow at 53 Homefield Road involving the erection of a two and a half storey detached residential building with accommodation in the roof space comprising 5 flats with associated bin and cycle stores, alterations to provide 4 car parking spaces at front (LBC Ref 19/05303/FUL). The reasons for refusal focussed on the failure to provide a satisfactory standard of accommodation (including external amenity space and internal space standards) the impact on neighbouring amenity and the failure to provide adequate refuse storage arrangements.
- On 14th February 2020, planning permission was refused for the construction of 6 additional car parking spaces (associated with a previously consented residential redevelopment of the former Normanton Park Hotel) to provide a total of 31 car parking spaces (LBC Ref 19/06030/FUL). The reasons for refusal focussed on over-reliance on use of the private car over more sustainable transport modes and the increased area of hard standing within the rear garden, detrimental to the character and appearance of the area. Members may recall that a planning permission for the redevelopment of this former hotel to provide 29 apartments with 25 off street car parking spaces was granted planning permission back in December 2017 (LBC Ref 16/02577/P) which is now nearing completion on site.

DEVELOPMENT MANAGEMENT - PLACE DEPARTMENT

DELEGATED PLANNING DECISIONS (Ward Order)

The following is a list of planning applications determined by the Head of Development Management under delegated powers since the last meeting of the Planning Committee.

Note: This list also includes those decisions made by Planning Committee and released in this time frame as shown within the level part of each case.

NOTE: The cases listed in this report can be viewed on the Council's Website.

Please note that you can also view the information supplied within this list and see more details relating to each application (including the ability to view the drawings submitted and the decision notice) by visiting our Online Planning Service at the Croydon Council web site (www.croydon.gov.uk/onlineplans).

Once on the Council web page please note the further information provided before selecting the Public Access Planning Register link. Once selected there will be various options to select the Registers of recently received or decided applications. Also; by entering a reference number if known you are able to ascertain details relating to a particular application. (Please remember to input the reference number in full by inserting any necessary /'s or 0's)

Ref. No. :	19/05877/FUL	Ward :	Addiscombe East
Location :	9 Colworth Road Croydon CR0 7AD	Type:	Full planning permission
Proposal :	Installation of replacement Upvc windows		
Date Decision:	31.01.20		

Permission Granted

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Proposal : Erection of single-storey side/rear extension, installation of French doors in rear elevation and new brickwork to rear outrigger.

Date Decision: 04.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05894/DISC **Ward : Addiscombe West**
Location : Land Adjacent To East Croydon Station And Type: Discharge of Conditions
Land At Cherry Orchard Road, Cherry
Orchard Gardens, Billington Hill, Croydon
Proposal : Discharge of condition 9 (Travel Plan) attached to planning permission 17/05046/FUL for the Erection of two 25 storey towers (plus plant) and a single building ranging from 5 to 9 storeys (plus plant) to provide a total of 445 residential units, with flexible commercial, retail and community floorspace (A1/A2/A3/A4/B1a/D1/D2) at ground and first floor level of the two towers and associated amenity, play space, hard and soft landscaping, public realm, cycle parking and car parking with associated vehicle accesses.

Date Decision: 29.01.20

Approved

Level: Delegated Business Meeting

Ref. No. : 20/00003/HSE **Ward : Addiscombe West**
Location : 33 Leslie Park Road Type: Householder Application
Croydon
CR0 6TN
Proposal : Erection of first floor rear extension, demolition of existing conservatory, installation of 2 windows in side elevation and internal alterations.

Date Decision: 14.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 20/00342/DISC **Ward : Addiscombe West**
Location : Land Adjacent To East Croydon Station And Type: Discharge of Conditions
Land At Cherry Orchard Road, Cherry
Orchard Gardens, Billington Hill, Croydon.
And Land At Cherry Orchard Road, Cherry
Orchard Gardens, Billington Hill, Croydon.

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Proposal : Discharge of condition 17 (District Energy) attached to planning permission 17/05046/FUL for the Erection of two 25 storey towers (plus plant) and a single building ranging from 5 to 9 storeys (plus plant) to provide a total of 445 residential units, with flexible commercial, retail and community floorspace (A1/A2/A3/A4/B1a/D1/D2) at ground and first floor level of the two towers and associated amenity, play space, hard and soft landscaping, public realm, cycle parking and car parking with associated vehicle accesses

Date Decision: 05.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/03391/LE
Location : 56 Kimberley Road
Croydon
CR0 2PU
Proposal : Use of building as 2 separate two-bed flats.
Ward : **Bensham Manor**
Type: LDC (Existing) Use edged

Date Decision: 04.02.20

Lawful Dev. Cert. Granted (existing)

Level: Delegated Business Meeting

Ref. No. : 19/04207/FUL
Location : 109 Richmond Road
Thornton Heath
CR7 7QF
Proposal : Alterations and extension to existing detached storage building at rear for use a self-contained residential unit
Ward : **Bensham Manor**
Type: Full planning permission

Date Decision: 31.01.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05210/DISC
Location : Garage And Land Adjoining 1 Kimberley Road
Croydon
CR0 2PY
Proposal : Details pursuant to the discharge of conditions 2 (water usage), 5 (landscaping) and 6 (materials) of planning permission 19/01307/FUL 'demolition of existing garage and the construction of a two storey detached one bed house.'
Ward : **Bensham Manor**
Type: Discharge of Conditions

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Level: Delegated Business Meeting

Ref. No. : 19/05841/HSE **Ward : Bensham Manor**
Location : 14 Bensham Close Type: Householder Application
Thornton Heath
CR7 7AH
Proposal : Demolition of existing rear extension and erection of single storey rear and side extension.

Date Decision: 05.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05940/LP **Ward : Bensham Manor**
Location : 2 Bridport Road Type: LDC (Proposed) Operations
Thornton Heath edged
CR7 7QG
Proposal : Alteration to existing outrigger and erection of single-storey rear extension.

Date Decision: 06.02.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/05941/HSE **Ward : Bensham Manor**
Location : 2 Bridport Road Type: Householder Application
Thornton Heath
CR7 7QG
Proposal : Erection of two-storey side extension.

Date Decision: 12.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05951/GPDO **Ward : Bensham Manor**
Location : 2 Bridport Road Type: Prior Appvl - Class A Larger
Thornton Heath House Extns
CR7 7QG
Proposal : Erection of single storey rear extension projecting out 4.5 metres with a maximum height of 3 metres

Date Decision: 29.01.20

(Approval) refused

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Level: Delegated Business Meeting

Ref. No. : 19/06001/PA8 **Ward : Bensham Manor**
Location : Outside **Type: Telecommunications Code**
304-306 Bensham Lane **System operator**
Thornton Heath
CR7 7EQ
Proposal : Proposed installation of a 15.0m monopole, supporting 6 no antenna within a shroud, together with the installation of ground-based equipment cabinets and ancillary development.

Date Decision: 14.02.20

Not approved

Level: Delegated Business Meeting

Ref. No. : 20/00565/DISC **Ward : Bensham Manor**
Location : 218 Melfort Road **Type: Discharge of Conditions**
Thornton Heath
CR7 7RQ
Proposal : Discharge of Condition 3 attached to Planning Permisison Ref 18/02647/FUL for Alterations, Conversion of dwellinghouse to form 1 x 2 bedroom flat, 2 x 1 bedroom flats, and 1 studio flat, provision of associated parking, refuse storage and cycle storage.

Date Decision: 07.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 18/03145/DISC **Ward : Broad Green**
Location : 4-6 Montague Road **Type: Discharge of Conditions**
Croydon
CR0 3SS
Proposal : Discharge of Condition - 2 (Part C - Balcony Balustrades), Condition 4 (Landscaping), and Part-Discharge of Condition 8 - Contaminated Land attached to planning permission 17/03407/FUL for the demolition of the existing houses: erection of a four storey building comprising of 5 one bedroom and 7 two bedroom flats: provision of cycle storage and refuse/recycling store and associated landscaping

Date Decision: 30.01.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/01514/FUL **Ward : Broad Green**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : Land At Junction Of Factory Lane Known As Type: Full planning permission
12
Enterprise Close
Croydon

Proposal : Alterations, Use of land for reception, separation and storage of metal for recycling and provision of associated reception, dust suppression, unloading and storage areas.

Date Decision: 31.01.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/03353/DISC **Ward : Broad Green**
Location : 4-6 Montague Road Type: Discharge of Conditions
Croydon
CR0 3SS

Proposal : Part-Discharge of Conditions 8 and 15 attached to Planning Permission 17/03407/FUL for Demolition of the existing houses, Erection of a four storey building comprising 5 one bedroom and 7 two bedroom flats, provision of cycle storage, and refuse/ recycling store, and associated landscaping.

Date Decision: 30.01.20

Part Approved / Part Not Approved

Level: Delegated Business Meeting

Ref. No. : 19/03661/DISC **Ward : Broad Green**
Location : Lombard House Type: Discharge of Conditions
2 Purley Way
Croydon
CR0 3JP

Proposal : Part Discharge of condition 12 (Carbon Reduction Blocks C, D and E) attached to Planning permission 15/01236/P for the Demolition of existing buildings; redevelopment of site to provide new buildings ranging from three to six storeys in height comprising 32 one bedroom. 48 two bedroom, 13 three bedroom and 3 four bedroom residential units and 2,296 M2 of commercial floorspace (within class B1a & B1c) provision of associated parking, open space and landscaping

Date Decision: 30.01.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/04822/FUL **Ward : Broad Green**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 10 Wellington Road
Croydon
CR0 2SH
Type: Full planning permission

Proposal : Conversion to form 1 x 3B5P Flat, 2 x 1B1P Flats, Erection of Single Storey Rear Extension, Loft Conversion, associated Cycle & Refuse Storage & Internal Alterations to 10 Wellington Road Croydon CR0 2SH.

Date Decision: 12.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05538/CONR
Location : Harris Invictus Academy Croydon
88 London Road
Croydon
CR0 2TB
Ward : **Broad Green**
Type: Removal of Condition

Proposal : Variation of condition 23 (BREEAM) of planning permission 15/05559/P. Removal of requirement for all mandatory credits for BREEAM 'excellent' to be met (8/10 met).

Date Decision: 12.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05630/HSE
Location : 8 Chapman Road
Croydon
CR0 3NU
Ward : **Broad Green**
Type: Householder Application

Proposal : Erection of two storey side extension, two storey rear extension and single storey rear extension.

Date Decision: 07.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05642/LP
Location : 270 London Road
Croydon
CR0 2TH
Ward : **Broad Green**
Type: LDC (Proposed) Use edged

Proposal : Change of Use from A1 (retail) to A2 (Financial and professional Services)

Date Decision: 29.01.20

Lawful Dev. Cert. Granted (proposed)

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Level: Delegated Business Meeting

Ref. No. : 19/05777/FUL **Ward : Broad Green**
Location : 216 Mitcham Road Type: Full planning permission
Croydon
CR0 3JG
Proposal : Erection of part single/part two storey rear extension
Date Decision: 31.01.20

Withdrawn application

Level: Delegated Business Meeting

Ref. No. : 19/05827/ADV **Ward : Broad Green**
Location : 66 Purley Way Type: Consent to display
Croydon advertisements
CR0 3JP
Proposal : Non illuminated fascia sign
Date Decision: 05.02.20

Consent Granted (Advertisement)

Level: Delegated Business Meeting

Ref. No. : 19/05834/FUL **Ward : Broad Green**
Location : 1 Kelling Gardens Type: Full planning permission
Croydon
CR0 2RP
Proposal : Erection of new 1 Bedroom Studio Flat
Date Decision: 07.02.20

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/05850/LP **Ward : Broad Green**
Location : 65 Sutherland Road Type: LDC (Proposed) Operations
Croydon edged
CR0 3QL
Proposal : Erection of loft conversion, including dormer in the rear roof slope and roof lights in the front roof slope.
Date Decision: 31.01.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Proposal : Discharge of Condition 22 attached for permission 18/02663/FUL for 'Demolition of the existing buildings, hardstanding and car parking on the site. The erection of up to 11,398sqm (GEA) of new floorspace, arranged as 3no. 1-2 storey buildings for industrial, warehousing and ancillary use (Use Classes B1b, B1c, B2 and/or B8); a single storey security hut; and a single storey substation. The construction of a new road linking Hesterman Way to Purley Way via Waddon Marsh Way. Reconfiguration of car park spaces (with 13 additional spaces), external landscaping, 1.8-3.0m high boundaries, access gates, freestanding bin stores, plant and equipment, covered cycle stores, service yards and associated works.'

Date Decision: 14.02.20

Part Approved / Part Not Approved

Level: Delegated Business Meeting

Ref. No. : 20/00363/NMA

Ward : **Broad Green**

Location : 78 Purley Way
Croydon
CR0 3JP

Type: Non-material amendment

Proposal : Non material amendment to planning permission reference 19/03360/FUL granted on the 20/09/2019 for the 'Erection of four storey building for storage (Use Class B8) with associated water sprinkler tank, landscaping, cycle parking and car parking/ access.' To increase the size of the sprinkler tank.

Date Decision: 29.01.20

Approved

Level: Delegated Business Meeting

Ref. No. : 18/05086/CAT

Ward : **Crystal Palace And Upper Norwood**

Location : 57 Bedwardine Road
Upper Norwood
London
SE19 3AS

Type: Works to Trees in a Conservation Area

Proposal : Lime Tree - Pollard to previous points.

Date Decision: 28.01.20

No objection (tree works in Con Areas)

Level: Delegated Business Meeting

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Date Decision: 31.01.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. :	19/05667/LP	Ward :	Crystal Palace And Upper Norwood
Location :	6 Hermitage Road Upper Norwood London SE19 3QR	Type:	LDC (Proposed) Operations edged
Proposal :	Loft conversion		

Date Decision: 29.01.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. :	19/05686/DISC	Ward :	Crystal Palace And Upper Norwood
Location :	133 Auckland Rise Upper Norwood London SE19 2DY	Type:	Discharge of Conditions
Proposal :	Discharge of condition 6 attached planning permission (19/01473/HSE) for a two storey side extension to the dwelling.		

Date Decision: 31.01.20

Approved

Level: Delegated Business Meeting

Ref. No. :	19/05753/HSE	Ward :	Crystal Palace And Upper Norwood
Location :	11 Telford Close Upper Norwood London SE19 3AG	Type:	Householder Application
Proposal :	Demolition of timber shed and erection of outbuilding and works to tree.		

Date Decision: 31.01.20

Permission Granted

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Level: Delegated Business Meeting

Ref. No. : 19/05824/HSE **Ward : Crystal Palace And Upper Norwood**
Location : 4 Pytchley Crescent **Type: Householder Application**
Upper Norwood
London
SE19 3QT
Proposal : Alterations to existing garage and erection of single/two storey side/rear extension

Date Decision: 05.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05868/NMA **Ward : Crystal Palace And Upper Norwood**
Location : Land To The West Of 83, 85 And 113 **Type: Non-material amendment**
Hermitage Road
Upper Norwood
London
SE19 3QN
Proposal : Amendment to planning permission 16/05891/FUL for the Erection of a part three, part four storey building comprising no. 6 two bedroom and 1 one bedroom flats and 1 two storey two bedroom and 1 three storey three bedroom house together with car parking, landscaping and associated works (amendment to hard landscape area between Block A and Block B).

Date Decision: 05.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05869/HSE **Ward : Crystal Palace And Upper Norwood**
Location : 46A Maberley Road **Type: Householder Application**
Upper Norwood
London
SE19 2JA
Proposal : Erection of spiral staircase and door to rear elevation.

Date Decision: 05.02.20

Permission Granted

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Ref. No. : 19/05366/DISC **Ward : Coulsdon Town**
Location : 16A The Grove Type: Discharge of Conditions
Coulsdon
CR5 2BH

Proposal : Discharge of conditions 10 (CLP) attached to planning permission 19/01007/FUL for the erection of part two/part three storey detached dwelling house with associated bin and cycle stores, formation of vehicular access, and provision of 2 parking spaces for the host property and 2 parking spaces for the proposed dwelling.

Date Decision: 14.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05441/FUL **Ward : Coulsdon Town**
Location : Unit A2c Type: Full planning permission
Redlands
Coulsdon
CR5 2HT

Proposal : Excavation of base of former quarry embankment wall to enlarge warehouse yard and construction of retaining wall. [Part retrospective].

Date Decision: 10.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05773/FUL **Ward : Coulsdon Town**
Location : 49 Chipstead Valley Road Type: Full planning permission
Coulsdon
CR5 2RB

Proposal : Proposed change of use from A1 (hairdressers) to Sui-Generis (beauty salon).

Date Decision: 14.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05791/HSE **Ward : Coulsdon Town**
Location : 23 Wilhelmina Avenue Type: Householder Application
Coulsdon
CR5 1NL

Proposal : Erection of first floor side extension.

Date Decision: 12.02.20

Permission Granted

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Proposal : Alterations including erection of a single storey rear and side extension, and excavation of the sloping rear garden to form a patio area and installation of retaining walls.

Date Decision: 07.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/06013/FUL
Location : Sperrin House
1 Brighton Road
Coulsdon
CR5 2FB

Ward : Coulsdon Town
Type: Full planning permission

Proposal : Amendment to planning consent reference 18/00841/FUL to provide additional off street car parking spaces

Date Decision: 13.02.20

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/06046/GPDO
Location : 11 Woodcote Grove Road
Coulsdon
CR5 2AG

Ward : Coulsdon Town
Type: Prior Appvl - Class A Larger House Extns

Proposal : Erection of a single storey rear extension which projects out by 4 metres from the rear wall of the original house with an eaves height of 3 metres and a maximum overall height of 3 metres

Date Decision: 04.02.20

Prior Approval No Jurisdiction (GPDO)

Level: Delegated Business Meeting

Ref. No. : 20/00506/NMA
Location : 74 Portnalls Road
Coulsdon
CR5 3DE

Ward : Coulsdon Town
Type: Non-material amendment

Proposal : Non-material amendment to planning application 18/01521/HSE

Date Decision: 07.02.20

Not approved

Level: Delegated Business Meeting

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 12 Cranmer Road
Croydon
CR0 1SR
Type: Works to Trees in a
Conservation Area
Proposal : Yew T1 - Reduce height by up to 2m and reduce sides by up to 1.5m to form oval shape

Date Decision: 28.01.20

No objection (tree works in Con Areas)

Level: Delegated Business Meeting

Ref. No. : 19/03236/PAD
Location : 67 George Street
Croydon
CR0 1LD
Type: **Ward : Fairfield**
Determination prior approval
demolition
Proposal : Demolition of 2-storey former funeral parlour building.

Date Decision: 07.02.20

Approved (prior approvals only)

Level: Delegated Business Meeting

Ref. No. : 19/03611/NMA
Location : 17-21 Dingwall Road
Croydon
CR0 2NA
Type: **Ward : Fairfield**
Non-material amendment
Proposal : Full planning application for a residential-led mixed use development ranging in height from 9 (ground plus 8 levels) to 24 storeys (ground plus 23 levels), containing 181 residential units (86 no. 1 bed units, 81 no. 2 bed units, 14 no. 3 bed units) with flexible commercial space at ground, first and second floor level, 8 no. disabled access car parking spaces, cycle parking, and associated amenity space, hard and soft landscaping (Non material amendment to alter the basement, ground and first floor layouts to accommodate the revised energy strategy resolving operational and technical detailed design requirements, alterations to both stair and lift cores, additional communal space at floor 09 and amended balcony/window detailing at Floors 01, 22 and 23).

Date Decision: 11.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/04775/DISC
Type: **Ward : Fairfield**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 1 Parker Road And Land To The Rear Type: Discharge of Conditions
Including
18A, 20A And 20C South End
Croydon
CR0 1DN

Proposal : Discharge of condition 21 (Foundation and Piling Work) of planning permission
18/04953/FUL.

Date Decision: 29.01.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/04899/DISC Ward : **Fairfield**
Location : 1 Parker Road And Land To The Rear Type: Discharge of Conditions
Including
18A, 20A And 20C South End
Croydon
CR0 1DN

Proposal : Discharge of condition 16 (contaminated land) of planning permission 18/04953/FUL for
the demolition of existing buildings and erection of a 2 storey building containing music
rehearsal and event space at ground floor level (sui generis) and 2 residential units (1 x 1
bed and 1 x studio) above and the erection of a 3 storey terrace containing 6 x 3
bedroom dwellinghouses to the rear together with car and cycle parking, refuse storage
and amenity space.

Date Decision: 29.01.20

Part Approved / Part Not Approved

Level: Delegated Business Meeting

Ref. No. : 19/05408/DISC Ward : **Fairfield**
Location : 1 Parker Road And Land To The Rear Type: Discharge of Conditions
Including
18A, 20A And 20C South End
Croydon
CR0 1DN

Proposal : Discharge of condition 9 (Construction Logistics Plan) of planning permission
18/04953/FUL

Date Decision: 29.01.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05493/LBC Ward : **Fairfield**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : Whitgift Almshouses
North End
Croydon
CR0 1UB

Type: Listed Building Consent

Proposal : Internal alterations to the Boiler Room to allow fire separation between the kitchenette and WC.

Date Decision: 11.02.20

Listed Building Consent Granted

Level: Delegated Business Meeting

Ref. No. : 19/05569/FUL
Location : 22B Chatsworth Road
Croydon
CR0 1HA

Ward : **Fairfield**
Type: Full planning permission

Proposal : Alterations to 4 x windows to change their materials from UPVC to timber (retrospective)

Date Decision: 14.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05614/FUL
Location : 18A Beech House Road
Croydon
CR0 1JP

Ward : **Fairfield**
Type: Full planning permission

Proposal : Conversion of roof space into habitable accommodation, erection of front and rear roof lights and rear terrace (Amended description).

Date Decision: 07.02.20

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/05883/FUL
Location : 49-51 Wellesley Road
Croydon
CR0 2AJ

Ward : **Fairfield**
Type: Full planning permission

Proposal : Erection of single storey rear extension at lower ground floor level to enlarge two existing flats.

Date Decision: 07.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05904/FUL **Ward : Fairfield**
Location : 20 Ainsworth Road **Type: Full planning permission**
Croydon
CR0 3SH
Proposal : Use of annex outbuilding as a one bed self-contained dwelling (retrospective), involving the construction of a single storey side extension and demolition of existing garages.
Date Decision: 06.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05913/FUL **Ward : Fairfield**
Location : Christopher Wren Yard **Type: Full planning permission**
117 High Street
Croydon
CR0 1QG
Proposal : Installation of external platform lift from lower ground to upper ground level
Date Decision: 04.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05919/OUT **Ward : Fairfield**
Location : 49 Church Road **Type: Outline planning permission**
Croydon
CR0 1SJ
Proposal : Outline application for a new two storey building containing 2no flats
Date Decision: 11.02.20

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/06002/NMA **Ward : Fairfield**
Location : The Magistrates Court **Type: Non-material amendment**
Barclay Road
Croydon
CR0 1JN

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 6-44 Station Road And Queens Hall Car Park, Poplar Walk, Croydon (St Michaels Square) Type: Non-material amendment

Proposal : Variation/alteration of trigger points of conditions 28 and 30 of planning permission 15/01419/P.

Date Decision: 13.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/02143/TRE Ward : **Kenley**
Location : Lightwood Court Valley Road Kenley CR8 5DG Type: Consent for works to protected trees

Proposal :
1. Sycamore - section fell 3 x stems and reduce to shape
2. Sycamore- Pollard by 40% (1 - 1.5mtrs approx), shape and deadwood
3. Sycamore - remove 3 leaning stems
4. Sycamore- fell tree to ground level
5. Sycamore - fell large leaning stem and smaller stem in front
6. Sycamore - fell stem leaning towards flats
7. Sycamore - fell smaller stem to left.
8. Oak - remove two dead limbs (TPO no. 149)

Date Decision: 28.01.20

Not Determined application

Level: Delegated Business Meeting

Ref. No. : 19/04071/FUL Ward : **Kenley**
Location : Land At 44 Abbots Lane Kenley CR8 5JH Type: Full planning permission

Proposal : Erection of detached chalet bungalow at rear, formation of vehicular access and provision of associated parking.

Date Decision: 29.01.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04554/FUL Ward : **Kenley**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 35B Little Roke Avenue Type: Full planning permission
Kenley
CR8 5NN
Proposal : Alterations, erection of a proposed ground floor front / side extension
Date Decision: 14.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04798/TRE Ward : **Kenley**
Location : 8 Kenwood Ridge Type: Consent for works to protected
Kenley trees
CR8 5JW
Proposal : T1- Fraxinus Excelsior -Remove 1x split and damaged lateral branch encroaching over
the rear garden of Number 8. - Overall crown reduction of 2.5m. T2 _ T3 Prunus Avium -
fell to ground level due to suppression, poor form and condition.
(TPO no. 35, 1987)
Date Decision: 30.01.20

Consent Granted (Tree App.)

Level: Delegated Business Meeting

Ref. No. : 19/05454/HSE Ward : **Kenley**
Location : 18 Kenmore Road Type: Householder Application
Kenley
CR8 5NU
Proposal : Demolition of existing rear extension and erection of single storey rear extension with
associated external steps.
Date Decision: 30.01.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05733/LP Ward : **Kenley**
Location : 158 Old Lodge Lane Type: LDC (Proposed) Operations
Purley edged
CR8 4AL
Proposal : Loft conversion including the erection of a rear dormer and hip to gable extension
Date Decision: 29.01.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/05734/HSE **Ward : Kenley**
Location : 158 Old Lodge Lane **Type: Householder Application**
Purley
CR8 4AL
Proposal : Garage conversion to habitable room and a single storey side/rear infill extension
Date Decision: 29.01.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05839/LP **Ward : Kenley**
Location : 40 Oaks Road **Type: LDC (Proposed) Operations**
Kenley **edged**
CR8 5NR
Proposal : Loft conversion with replacement rear dormer and hip to gable extension
Date Decision: 05.02.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/06029/HSE **Ward : Kenley**
Location : 111 Mosslea Road **Type: Householder Application**
Whyteleafe
CR3 0DS
Proposal : Single storey side/rear extension; replacement front garage door with window including new roof and alterations (conversion of garage into habitable space).
Date Decision: 14.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 20/00488/DISC **Ward : Kenley**
Location : 193 Hayes Lane **Type: Discharge of Conditions**
Kenley
CR8 5HN
Proposal : Discharge of condition 4 (landscaping) attached to planning permission 17/06370/FUL for Demolition of existing detached dwelling; Erection of a two storey detached dwelling, soft/hard landscaping and other associated works.
Date Decision: 07.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05472/HSE
Location : 75 Homestead Way
Croydon
CR0 0AW
Ward : **New Addington South**
Type: Householder Application
Proposal : Erection of a hip to gable roof extension and rear dormer following demolition of existing chimney, insertion of roof lights.

Date Decision: 30.01.20

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/02388/FUL
Location : 303-305 Norbury Avenue
Norbury
London
SW16 3RW
Ward : **Norbury Park**
Type: Full planning permission
Proposal : Demolition of existing dwellings. Erection of 3-storey building to provide 2 x 3-bed, 3 x 2-bed and 4 x 1-bed flats (9 in total) with associated parking, amenity spaces, refuse and cycle storage (amended drawings).

Date Decision: 13.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04278/HSE
Location : 37 Ryecroft Road
Norbury
London
SW16 3EW
Ward : **Norbury Park**
Type: Householder Application
Proposal : Alterations, demolition and erection of a part single/part two storey rear extension and conversion of garage to habitable room

Date Decision: 28.01.20

Permission Granted

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Level: Delegated Business Meeting

Ref. No. : 19/05200/FUL **Ward : Norbury Park**
Location : 14 Gibson's Hill Type: Full planning permission
Norbury
London
SW16 3JN

Proposal : Erection of a 3 bedroom single storey dwelling with accommodation in the roof space with associated off street parking, refuse and cycle storage

Date Decision: 30.01.20

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/05293/HSE **Ward : Norbury Park**
Location : 50 Covington Way Type: Householder Application
Norbury
London
SW16 3SQ

Proposal : Erection of single storey rear extension, conversion of existing garage, erection of first floor side extension, erection of porch extension, internal alterations, removal of chimney stack and alterations to land levels at the rear of the site to include terrace.

Date Decision: 30.01.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05693/HSE **Ward : Norbury Park**
Location : 10 Biggin Hill Type: Householder Application
Upper Norwood
London
SE19 3HY

Proposal : Conversion of the existing garage to a habitable room and the erection of single-storey rear extension.

Date Decision: 31.01.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05902/HSE **Ward : Norbury Park**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 38 County Road
Thornton Heath
CR7 8HN
Type: Householder Application
Proposal : Erection of single-storey rear extension, erection of part single/two storey side extension and erection of front porch extension.

Date Decision: 05.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05945/HSE
Location : 20 County Road
Thornton Heath
CR7 8HN
Type: Householder Application
Ward : **Norbury Park**
Proposal : Erection of single-storey rear extension.

Date Decision: 05.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05948/HSE
Location : 35 Highbury Avenue
Thornton Heath
CR7 8BP
Type: Householder Application
Ward : **Norbury Park**
Proposal : Erection of single storey front extension

Date Decision: 12.02.20

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/05958/HSE
Location : 56 Christian Fields
Norbury
London
SW16 3JZ
Type: Householder Application
Ward : **Norbury Park**
Proposal : Demolition of existing rear extension and erection of single-storey rear extension.

Date Decision: 06.02.20

Permission Granted

Level: Delegated Business Meeting

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 1391 - 1393 London Road Type: Full planning permission
Norbury
London
SW16 4AN

Proposal : Alterations involving side extension, rebuilding of existing roof, with the addition of dormers and internal alterations to create two 1 bedroom and three 2 bedroom flats with refuse and cycle storage

Date Decision: 31.01.20

P. Granted with 106 legal Ag. (3 months)

Level: Planning Committee

Ref. No. : 19/03851/FUL Ward : **Norbury And Pollards Hill**
Location : 1392 London Road Type: Full planning permission
Norbury
London
SW16 4BZ

Proposal : Restrospective application for single storey rear extension, installation of new extraction system. new shopfront and signage

Date Decision: 28.01.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04786/DISC Ward : **Norbury And Pollards Hill**
Location : 97 Pollards Hill South Type: Discharge of Conditions
Norbury
London
SW16 4LS

Proposal : Discharge of condition 7 (hard and soft landscaping), 10 (Construction Logistics Plan) and 12 (Water usage and CO2) of planning permission of 19/00490/FUL for demolition of the existing dwelling; erection of building comprising of 3 x one bed flats and 1 x two bed flat and 4 x four bed semi-detached dwellings to the rear of the site with associated parking, landscaping, cycle and refuse storage (8 total).

Date Decision: 12.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05732/FUL Ward : **Norbury And Pollards Hill**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 132 Norbury Court Road
Norbury
London
SW16 4HY

Type: Full planning permission

Proposal : Alterations and conversion of the dwelling into 5 self-contained flats with associated refuse/cycle storage and parking.

Date Decision: 07.02.20

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/05815/FUL

Ward : Norbury And Pollards Hill

Location : 6 Craignish Avenue
Norbury
London
SW16 4RN

Type: Full planning permission

Proposal : Conversion to form 1 three bedroom and 2 one bedroom flats

Date Decision: 31.01.20

Withdrawn application

Level: Delegated Business Meeting

Ref. No. : 19/05907/HSE

Ward : Norbury And Pollards Hill

Location : 116 Norbury Crescent
Norbury
London
SW16 4JZ

Type: Householder Application

Proposal : Demolition and erection of single storey side/rear extension

Date Decision: 12.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05936/FUL

Ward : Norbury And Pollards Hill

Location : 6 Pollards Hill East
Norbury
London
SW16 4UT

Type: Full planning permission

Proposal : Change of use from three flats into a single house

Date Decision: 14.02.20

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Ref. No. : 19/04003/FUL **Ward : Old Coulsdon**
Location : 76-80 Waddington Avenue **Type: Full planning permission**
Coulsdon
CR5 1QN

Proposal : Construction of a two-storey four-bedroom dwellinghouse to the front, a row of 8 x two-storey semi-detached dwellinghouses (1x 2-bed and 7x3bed) to the rear with associated vehicular access, 15 car parking spaces, refuse refuge and hard and soft landscaping; following demolition of existing bungalow and garages.

Date Decision: 29.01.20

Permission Granted

Level: Planning Committee

Ref. No. : 19/05303/FUL **Ward : Old Coulsdon**
Location : 53 Homefield Road **Type: Full planning permission**
Coulsdon
CR5 1ET

Proposal : Demolition of an existing bungalow and construction of a two and a half storey detached residential building with accommodation in the roof space comprising 5 flats with associated bin and cycle stores, alterations to provide 4 car parking spaces at front.

Date Decision: 04.02.20

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/05618/DISC **Ward : Old Coulsdon**
Location : Stone Cottage **Type: Discharge of Conditions**
Coulsdon Road
Coulsdon
CR3 5QS

Proposal : Discharge of condition 6 - intrusive site investigation - attached to planning permission 15/05487/P (Demolition of Stone Cottage, erection of three bedroom chalet bungalow with basement area; erection of detached garage)

Date Decision: 14.02.20

Not approved

Level: Delegated Business Meeting

Ref. No. : 19/05689/FUL **Ward : Old Coulsdon**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 64 Coulsdon Rise
Coulsdon
CR5 2SB
Type: Full planning permission

Proposal : Demolition of existing single storey dwellinghouse and erection of a two storey 5 bedroom detached dwellinghouse with car parking, refuse and cycle storage, boundary treatment; raised patio to rear and hard and soft landscaping.

Date Decision: 07.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05767/HSE
Location : 102 Ellis Road
Coulsdon
CR5 1BZ
Ward : Old Coulsdon
Type: Householder Application

Proposal : Demolition of the existing outbuilding and the erection of single/two storey front/side extension.

Date Decision: 03.02.20

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/05778/HSE
Location : 4 Larkin Close
Coulsdon
CR5 2LS
Ward : Old Coulsdon
Type: Householder Application

Proposal : Conversion of garage to habitable room and erection of two-storey side extension including replacement garage.

Date Decision: 03.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05835/HSE
Location : 52 Keston Avenue
Coulsdon
CR5 1HN
Ward : Old Coulsdon
Type: Householder Application

Proposal : Part single; part two storey rear extension.

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Date Decision: 14.02.20

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/05855/DISC **Ward : Old Coulsdon**
Location : 91 Marlpit Lane **Type: Discharge of Conditions**
Coulsdon
CR5 2HH
Proposal : Discharge of conditions 2 (Materials) and 11 (Construction Logistics) attached to planning permission 19/01900/FUL for, Demolition of single-family dwelling and erection of one 3-storey block, containing 2 x 3-bedroom, 6 x 2-bedroom and 1 x 1-bedroom apartments with associated access, 9 parking spaces, cycle storage and refuse store.

Date Decision: 04.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05912/LP **Ward : Old Coulsdon**
Location : 1 Larkin Close **Type: LDC (Proposed) Operations**
Coulsdon **edged**
CR5 2LS
Proposal : Erection of a single storey rear extension

Date Decision: 31.01.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/05607/DISC **Ward : Park Hill And Whitgift**
Location : 28-30 Fairfield Road **Type: Discharge of Conditions**
Croydon
CR0 5LH
Proposal : Discharge of Conditions 5, 6, 11, 12, 13, 14, 15, 17, 20, 25, and 28 of Planning Permission 17/02696/FUL for Demolition of existing buildings and the erection of part four/ part six storey building with basement comprising 11 one bedroom, 10 two bedroom 12 three bedroom and 1 studio flats: formation of vehicular access and provision of basement parking, provision of associated refuse and cycle storage

Date Decision: 07.02.20

Part Approved / Part Not Approved

Level: Delegated Business Meeting

Ref. No. : 19/05889/FUL **Ward : Park Hill And Whitgift**
Location : Park Hill Junior School Type: Full planning permission
Stanhope Road
Croydon
CR0 5NS
Proposal : Erection of two-storey temporary building accommodating 6 classrooms as substitute classrooms during school refurbishments.

Date Decision: 12.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 20/00553/DISC **Ward : Park Hill And Whitgift**
Location : 28-30 Fairfield Road Type: Discharge of Conditions
Croydon
CR0 5LH
Proposal : Discharge of Condition 3 - External Facing Materials - of Planning Permission 17/02696/FUL for Demolition of existing buildings and the erection of part four/ part six storey building with basement comprising 11 one bedroom, 10 two bedroom 12 three bedroom and 1 studio flats, formation of vehicular access and provision of basement parking, provision of associated refuse and cycle storage

Date Decision: 07.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/01166/FUL **Ward : Purley Oaks And Riddlesdown**
Location : 154 Pampisford Road Type: Full planning permission
South Croydon
CR2 6DA
Proposal : Demolition of a single family dwelling and erection of one 3-storey block, containing 9 flats with associated landscaping, car parking and refuse storage.

Date Decision: 14.02.20

Permission Refused

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05800/LP **Ward : Purley Oaks And Riddlesdown**
Location : 71 Grasmere Road **Type: LDC (Proposed) Operations edged**
Purley
CR8 1DZ
Proposal : Loft conversion including a hip to gable extension and erection of a rear dormer

Date Decision: 12.02.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/05831/HSE **Ward : Purley Oaks And Riddlesdown**
Location : 34 Brancaster Lane **Type: Householder Application**
Purley
CR8 1HF
Proposal : Part single; part two storey side extension and single storey rear extension; rear dormer roof extensions including the raising of the ridge height and 6 rooflights to the front roofslope.

Date Decision: 31.01.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05997/DISC **Ward : Purley Oaks And Riddlesdown**
Location : 83 Purley Downs Road **Type: Discharge of Conditions**
South Croydon
CR2 0RJ
Proposal : Discharge of Condition 5 - External Facing Materials - attached to Planning Permission 18/02626/FUL (Demolition of the existing dwellinghouse. Erection of a detached three storey building comprising 4 one bedroom, 2 two bedroom and 3 three bedroom flats. Formation of access road and provision of associated parking, landscaping, cycle and refuse storage)

Date Decision: 11.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/06078/DISC **Ward : Purley Oaks And Riddlesdown**
 Location : 150 Pampisford Road Type: Discharge of Conditions
 South Croydon
 CR2 6DA
 Proposal : Discharge of condition 12 (Construction Logistics) attached to planning permission 19/01354/FUL for, Demolition of existing building: erection of one 3-storey building comprising 9 units and formation of associated vehicular access and provision of 8 off-street parking spaces, cycle storage and refuse store.

Date Decision: 14.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 20/00043/NMA **Ward : Purley Oaks And Riddlesdown**
 Location : Land To The East Of Montpelier Road And Type: Non-material amendment
 Land And Garages South Of 75-135
 Kingsdown Avenue
 South Croydon
 CR2 6QL
 Proposal : Non-material amendment to planning permission ref. 16/06031/FUL for demolition of existing garages and erection of 1 six storey building comprising 9 two bedroom, 1 one bedroom and 1 three bedroom flats, 1 four storey building comprising 4 two bedroom and 2 one bedroom flats and 13 three bedroom and 4 two bedroom houses. Provision of associated car parking, landscaping and associated works.

Date Decision: 04.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 20/00236/GPDO **Ward : Purley Oaks And Riddlesdown**
 Location : 3 Edgehill Road Type: Prior Appvl - Class A Larger
 Purley House Extns
 CR8 2NB
 Proposal : Erection of a single storey rear extension projecting out 6 metres from the rear wall of the original house with a height to the eaves of 3 metres and a maximum overall height of 3 metres.

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Date Decision: 14.02.20

(Approval) refused

Level: Delegated Business Meeting

Ref. No. :	20/00367/LP	Ward :	Purley Oaks And Riddlesdown
Location :	19 Mitchley Avenue Purley CR8 1BZ	Type:	LDC (Proposed) Operations edged
Proposal :	Alterations, erection of two side dormers at roof level		

Date Decision: 07.02.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. :	18/04179/CONR	Ward :	Purley And Woodcote
Location :	Allan House 20-22 Russell Hill Purley CR8 2JA	Type:	Removal of Condition
Proposal :	Variation of condition 1 (parking, cycling and refuse), condition 2 (landscaping), condition 3 (external facing materials), 7 (planting as specified) and condition 15 (approved drawings) attached to planning permission 12/03156/P and 18/02749/NMA for the demolition of existing buildings, erection of two storey building with accommodation in roofspace comprising 14 one bedroom flats to provide short term accommodation for semi independent living for people leaving residential care and formation of vehicular access and provision of associated parking (amended description)		

Date Decision: 07.02.20

P. Granted with 106 legal Ag. (3 months)

Level: Delegated Business Meeting

Ref. No. :	19/00884/FUL	Ward :	Purley And Woodcote
Location :	Land R/O 30 - 34 Hartley Old Road Purley CR8 4HG	Type:	Full planning permission

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Proposal : Erection of 3 x 5bedroom 9 person detached dwelling houses with associated parking and landscaping. Demolition of the garage to 32 Hartley Old Road to enable the construction of a new vehicle access / crossover.

Date Decision: 07.02.20

Permission Granted

Level: Planning Committee - Minor Applications

Ref. No. : 19/01384/FUL
Location : 3B Godstone Road
Kenley
CR8 5AG
Proposal : Demolition of bungalow and garage,; erection of a three/four storey building comprising 5 flats with associated bin store, formation of vehicular access and provision of 6 parking spaces at rear

Ward : **Purley And Woodcote**
Type: Full planning permission

Date Decision: 13.02.20

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 18/02015/CAT
Location : 17 Woodcote Lane
Purley
CR8 3HB
Proposal : 1. 1 x Large Sycamore Tree - Fell to ground level. The tree is dead.
2. 1 x Horse Chestnut Tree - Thin by 20%, crown lift 4 to 5 metres and remove the deadwood.
3. 1 x Lime Tree - Crown lift to 4 to 5 metres and remove the deadwood.

Ward : **Purley And Woodcote**
Type: Works to Trees in a Conservation Area

Date Decision: 28.01.20

No objection (tree works in Con Areas)

Level: Delegated Business Meeting

Ref. No. : 19/03430/DISC
Location : 95 - 95A Foxley Lane
Purley
CR8 3HP
Proposal : Application to discharge Condition 17 (Materials) of planning permission 18/02613/FUL

Ward : **Purley And Woodcote**
Type: Discharge of Conditions

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Date Decision: 06.02.20

Not approved

Level: Delegated Business Meeting

Ref. No. : 19/03431/DISC
Location : 95 - 95A Foxley Lane
Purley
CR8 3HP

Ward : Purley And Woodcote
Type: Discharge of Conditions

Proposal : Application to discharge Condition 3 (Drainage) of planning permission 18/02613/FUL.

Date Decision: 07.02.20

Not approved

Level: Delegated Business Meeting

Ref. No. : 19/03604/FUL
Location : 29-35 Russell Hill Road
Purley
CR8 2LF

Ward : Purley And Woodcote
Type: Full planning permission

Proposal : Demolition of existing residential dwellings and erection of 2 buildings, comprising of 106 new apartments, with associated hard and soft landscaping, access and car parking.

Date Decision: 14.02.20

P. Granted with 106 legal Ag. (3 months)

Level: Planning Committee

Ref. No. : 19/03881/DISC
Location : 28 Russell Hill
Purley
CR8 2JA

Ward : Purley And Woodcote
Type: Discharge of Conditions

Proposal : Discharge of condition 4 (privacy screens) relating to planning permission ref.18/00891/FUL for the demolition of the existing building; Erection of 1 x four storey building and 1 x two storey building comprising 5 x one bedroom, 5 x two bedroom and 3 x three bedroom flats. Provision of vehicular access and provision of parking spaces, refuse storage and landscaping.

Date Decision: 14.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/03977/DISC
Location : 22 Verulam Avenue
Purley
CR8 3NQ
Ward : **Purley And Woodcote**
Type: Discharge of Conditions
Proposal : Discharge of Conditions 4 (Access Arrangement) and 5 (Construction Method Statement) attached to 17/03832/HSE

Date Decision: 05.02.20

Not approved

Level: Delegated Business Meeting

Ref. No. : 19/04111/HSE
Location : Orchard Leigh
2 Briar Hill
Purley
CR8 3LE
Ward : **Purley And Woodcote**
Type: Householder Application
Proposal : Alterations, erection of a new entrance porch, Installation of skylight windows in the eastern, southern and northern roof slopes and the construction of a dormer in the northern roof slope

Date Decision: 14.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04548/HSE
Location : 5 Silver Lane
Purley
CR8 3HJ
Ward : **Purley And Woodcote**
Type: Householder Application
Proposal : Demolition of the existing garage and the erection of a new garage; excavation for a proposed basement and the erection of a two storey side and rear extension with other alterations.

Date Decision: 13.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04607/FUL
Location : 14 Russell Green Close
Purley
CR8 2NR
Ward : **Purley And Woodcote**
Type: Full planning permission

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Proposal : Construction of a four-storey building, including habitable roof-space, to accommodate seven flats with associated car parking spaces, cycle and bin stores, soft and hard landscaping; following the demolition of existing dwellinghouse.

Date Decision: 31.01.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04687/DISC

Ward : **Purley And Woodcote**

Location : 43 Woodcote Valley Road

Type: Discharge of Conditions

Purley
CR8 3AN

Proposal : Application to discharge condition 6 (Construction Logistic Plan) of planning permission 18/05410/FUL.

Date Decision: 29.01.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/04864/HSE

Ward : **Purley And Woodcote**

Location : 82 Hartley Down

Type: Householder Application

Purley
CR8 4EB

Proposal : Construction of a first floor, single/two storey front/side extensions to form a two-storey dwelling. Construction of a rear roof extension with installation of 4 front and 2 rooflights to each side roof slope and associated alterations.

Date Decision: 14.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05531/CONR

Ward : **Purley And Woodcote**

Location : Forbury Heights

Type: Removal of Condition

39 Russell Green Close
Purley

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Proposal : Section 73 application seeking to vary condition 1 (approved drawings), to allow for amendments to the details of the front, rear and side elevations and increasing the size of the staircase landing; as approved under planning permission 18/03701/FUL for: emolition of existing dwelling and proposed erection of 2 storey building with lower ground floor and accommodation in roof to provide 9 flats (4 x 1 bed, 3 x 2 bed and 2 x 3 bed) with associated car parking and new crossover, amenity space, refuse and cycle stores.

Date Decision: 07.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05563/CONR

Ward : **Purley And Woodcote**

Location : 28 Russell Hill
Purley
CR8 2JA

Type: Removal of Condition

Proposal : Variation of conditions 1 and 18 attached to planning permission ref.18/00891/FUL (Demolition of existing building; Erection of 1 x four storey building and 1 x two storey building comprising 5 x one bedroom, 5 x two bedroom and 3 x three bedroom flats. Provision of vehicular access and provision of parking spaces, refuse storage and landscaping).

Date Decision: 04.02.20

Withdrawn application

Level: Delegated Business Meeting

Ref. No. : 19/05682/DISC

Ward : **Purley And Woodcote**

Location : 195 And Land R/o 197 Brighton Road
Purley
CR8 4HF

Type: Discharge of Conditions

Proposal : Discharge of condition 2 (landscaping), 3 (Materials) and 15 (Construction Logistics Plan) attached to planning permission 19/02508/FUL for the Demolition of an existing bungalow and erection of two-storey house, and the erection of a four storey building to the rear to provide 8 flats(with accommodation in the roof space), including associated proposed amenity space, landscaping, parking, access road, cycle and refuse storage.

Date Decision: 28.01.20

Not approved

Level: Delegated Business Meeting

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Ref. No. : 19/05822/HSE **Ward : Purley And Woodcote**
Location : 203 Brighton Road **Type: Householder Application**
Purley
CR8 4HF
Proposal : Retention of a single storey rear extension

Date Decision: 31.01.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05884/FUL **Ward : Purley And Woodcote**
Location : White Lodge Nursing Home **Type: Full planning permission**
126 Foxley Lane
Purley
CR8 3NE
Proposal : Conversion of care home (C2 use) to a 10 person HMO (sui generis use)

Date Decision: 07.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05956/LP **Ward : Purley And Woodcote**
Location : 31 Foxley Gardens **Type: LDC (Proposed) Operations**
Purley **edged**
CR8 2DQ
Proposal : Erection of L-shaped rear roof dormer extension, conversion of loft space and installation of 3 rooflights in the front roofslope

Date Decision: 07.02.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/05983/NMA **Ward : Purley And Woodcote**
Location : 32 Copse Hill **Type: Non-material amendment**
Purley
CR8 4LH
Proposal : Non-material amendment to planning permission ref. 18/01742/HSE for erection of single/ two storey side extension and single storey rear extension.

Date Decision: 06.02.20

Approved (prior approvals only)

Level: Delegated Business Meeting

Ref. No. : 20/00182/DISC **Ward : Purley And Woodcote**
Location : 41-43 Russell Hill Road Type: Discharge of Conditions
Purley
CR8 2LD

Proposal : Discharge of Condition 5 (Written Scheme of Investigation) attached to permission 18/04264/FUL for the demolition of existing buildings; Erection of 2 x three/four storey buildings comprising 8 x one bedroom, 16 x two bedroom and 4 x three bedroom flats. Provision of vehicular accesses and provision of parking spaces, refuse and cycle storage and landscaping.

Date Decision: 13.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 20/00224/CAT **Ward : Purley And Woodcote**
Location : 10 Furze Lane Type: Works to Trees in a
Purley Conservation Area
CR8 3EG

Proposal : 1 x Thuja - Fell to ground level

Date Decision: 14.02.20

No objection (tree works in Con Areas)

Level: Delegated Business Meeting

Ref. No. : 20/00634/NMA **Ward : Purley And Woodcote**
Location : 12 Hillcroft Avenue Type: Non-material amendment
Purley
CR8 3DG

Proposal : Non-Material Amendment to Planning Permission Ref 19/01202/HSE for Alterations, erection of two storey rear extension and first floor side extension and associated alterations/extension to roof form including 3 rear dormer windows, erection of single storey side extension to form utility room, front extension to roof to form gable frontage, erection of ground floor front extensions and front porch, erection of front boundary wall with gate and railings.

Date Decision: 14.02.20

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 60 Mayfield Road
South Croydon
CR2 0BF

Type: Discharge of Conditions

Proposal : Discharge of Condition 4 (Construction Logistics) attached to planning permission 18/03459/FUL for the demolition of garage and rear extensions, erection of part single-storey, part two-storey rear and side extension, rear roof dormer, associated alterations, change of use from C1 (B_B) to C3 to provide 5 residential units (2 x studio, 2 x 1 bed, 1 x 3 bed), formation of vehicular access and provision of associated parking, cycle and refuse storage.

Date Decision: 31.01.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05966/DISC
Location : 22 Briton Crescent
South Croydon
CR2 0JF

Ward : **Sanderstead**
Type: Discharge of Conditions

Proposal : Discharge of condition no.5 (Construction Logistics Plan) attached to planning permission ref. 18/04026/FUL. (Demolition of the existing dwelling and erection of a three storey development for nine apartments with associated, nine off-street parking spaces, cycle storage and refuse store at 22 Briton Crescent Croydon, CR2 0JF)

Date Decision: 07.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05996/HSE
Location : 2 Sundown Avenue
South Croydon
CR2 0RP

Ward : **Sanderstead**
Type: Householder Application

Proposal : Demolition of existing attached garage and replacement with a single storey side extension

Date Decision: 12.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05998/LP
Ward : **Sanderstead**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 10 Addington Road
South Croydon
CR2 8RB
Type: LDC (Proposed) Operations
edged

Proposal : Installation of rooflights in front roofslope; erection of gable end and two dormer
extensions in rear roof slope; erection of single storey rear extension

Date Decision: 12.02.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 20/00179/NMA
Location : 32 Langley Oaks Avenue
South Croydon
CR2 8DH
Ward : **Sanderstead**
Type: Non-material amendment

Proposal : Non-material amendment to planning permission ref. 19/05353/HSE for demolition of an
outbuilding and erection of a dormer above the garage at first floor level and a single
storey side extension

Date Decision: 12.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 20/00272/NMA
Location : 7 Heathhurst Road
South Croydon
CR2 0BB
Ward : **Sanderstead**
Type: Non-material amendment

Proposal : Non-material amendment to planning application 17/06177/HSE

Date Decision: 11.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/03161/FUL
Location : 17 Ballards Way
South Croydon
CR2 7JP
Ward : **Selsdon And Addington
Village**
Type: Full planning permission

Proposal : Creation of 1 No. new three-storey house adjacent to No.17 Ballards Way

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Date Decision: 31.01.20

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/05409/HSE **Ward : Selsdon And Addington Village**
Location : 89 Farnborough Avenue Type: Householder Application
South Croydon
CR2 8HJ
Proposal : Demolition of outbuildings and erection of a single storey side/rear wrap around extension

Date Decision: 12.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05571/HSE **Ward : Selsdon And Addington Village**
Location : 7 Palace Green Type: Householder Application
Croydon
CR0 9AJ
Proposal : Construction of part single, part two-storey rear extension and 3 x rear dormers. Installation of 2 rooflights to each side roof slope. Alterations to the front elevation.

Date Decision: 03.02.20

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/05632/HSE **Ward : Selsdon And Addington Village**
Location : 14 Brent Road Type: Householder Application
South Croydon
CR2 7NR
Proposal : Alterations to ground levels and construction of new retaining wall in rear garden.

Date Decision: 13.02.20

Permission Granted

Level: Delegated Business Meeting

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Ref. No. : 19/05867/LP **Ward : Selsdon And Addington Village**
Location : 288 Addington Road **Type: LDC (Proposed) Operations edged**
South Croydon
CR2 8LF
Proposal : Proposed hip to gable loft conversion with rear dormer and rooflights to front

Date Decision: 05.02.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 20/00258/NMA **Ward : Selsdon And Addington Village**
Location : 54 Farnborough Crescent **Type: Non-material amendment**
South Croydon
CR2 8HA
Proposal : Non-material amendment to planning permission ref. 19/02427/HSE (Erection of single storey front/side, two storey side, single storey rear extension and rear box dormer extension.) Alterations to the internal arrangements and replacement of existing garage door with a window.

Date Decision: 05.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/03822/DISC **Ward : South Croydon**
Location : Coombe Lodge Playing Fields **Type: Discharge of Conditions**
Melville Avenue
South Croydon
CR2 7HY
Proposal : Discharge of condition 3 (Highway Works) of planning permission reference 19/00303/FUL granted on the 01/04/2019 for the 'Change of use of the site from playing fields (D2) to temporary secondary school (D1) until 31st December 2020 for 360 pupils, with associated erection of a temporary three storey school building, car parking, cycle store, bin store, fencing, soft and hard landscaping.'

Date Decision: 28.01.20

Approved

Level: Delegated Business Meeting

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/05832/DISC **Ward : South Croydon**
Location : 12 Spencer Road **Type: Discharge of Conditions**
South Croydon
CR2 7EH
Proposal : Discharge of Condition 2 (cycle and refuse), 3 (landscaping), and 4 (materials) attached to planning permission 19/02444/FUL for the demolition of garage, erection of a two storey side extension, single storey rear extension, installation of rooflights, and conversion of dwelling to provide 1 x 1 bedroom unit, 3 x 2 bedroom unit, 1 x 3 bedroom unit with associated landscaping, parking, refuse and cycle storage

Date Decision: 06.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05885/DISC **Ward : South Croydon**
Location : Horizon Apartments **Type: Discharge of Conditions**
11 South Park Hill Road
South Croydon
Proposal : Discharge of condition 4 (Materials) attached to permission 18/00693/FUL for demolition of the existing dwelling; and, erection of a three/four storey building comprising 1 studio, 4 two bedroom and 1 three bedroom flats with associated landscaping, parking, refuse and cycle provision.

Date Decision: 10.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/06030/FUL **Ward : South Croydon**
Location : Normanton Park Hotel **Type: Full planning permission**
34-36 Normanton Road
South Croydon
CR2 7AR

Proposal : Construction of 6x additional car parking spaces to create a total of 31 spaces.

Date Decision: 14.02.20

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 230A Sydenham Road
Croydon
CR0 2EB
Type: Full planning permission
Proposal : Erection of single-storey rear/side extension.
Date Decision: 14.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05655/GPDO
Location : 31 Whitehorse Road
Croydon
CR0 2JH
Ward : **Selhurst**
Type: Prior Appvl - Class M A1/A2 to dwelling

Proposal : Change of Use of Class A1/A2 to dwellinghouses Use Class C3

Date Decision: 27.01.20

(Approval) refused

Level: Delegated Business Meeting

Ref. No. : 19/03129/TRE
Location : 158A The Glade
Croydon
CR0 7UE
Ward : **Shirley North**
Type: Consent for works to protected trees

Proposal : Oak Tree in my front garden is dying and that is the verdict of Mr A N Rowland of 51A, Clifford Road, Se25 5JS one of the Tree contractors on the Directory of Tree contractors List sanctioned by Croydon Council. There is a great deal of dead wood on the Tree in addition to copious amounts of moss or some such material. Although the dead wood and reshaping can be sorted, the same problems will recur. Furthermore, there is a School not far from my house and the dead or weakened branches are therefore a potential danger to passing children and adults as the Tree is near the pavement. In this connection I am requesting permission to have the Tree felled.

Date Decision: 28.01.20

Withdrawn application

Level: Delegated Business Meeting

Ref. No. : 19/04061/FUL
Ward : **Shirley North**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 46 Stroud Green Way
Croydon
CR0 7BA
Type: Full planning permission
Proposal : Demolition of existing shed and stores and erection of 1 no. new build 2 bedroom house with dropped kerb, parking space, refuse storage, boundary fence and associated alterations

Date Decision: 11.02.20

Withdrawn application

Level: Delegated Business Meeting

Ref. No. : 19/04136/OUT
Location : 104 Wickham Road
Croydon
CR0 8BD
Type: Outline planning permission
Ward : **Shirley North**
Proposal : Demolition of existing dwellinghouse. Erection of a 3/4 storey building comprising 9 flats, creation of vehicular access and parking area, cycle and refuse storage and amenity space including roof garden

Date Decision: 14.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04699/FUL
Location : Old Lion Works
141B Wickham Road
Croydon
CR0 8TE
Type: Full planning permission
Ward : **Shirley North**
Proposal : Demolition of existing building and erection of part 1 and part 2 storey building containing 2 x 2 bedroom apartments

Date Decision: 12.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05128/HSE
Location : 11 Woodmere Avenue
Croydon
CR0 7PG
Type: Householder Application
Ward : **Shirley North**
Proposal : Erection of dormer extension in rear roofslope and installation of rooflights in front and side roofslapes.

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Date Decision: 29.01.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05418/GPDO
Location : 57 Barnfield Avenue
Croydon
CR0 8SF

Ward : Shirley North
Type: Prior Appvl - Class A Larger
House Extns

Proposal : Erection of single storey rear extension projecting out 4 metres with a maximum height of 3 metres

Date Decision: 27.01.20

Approved (prior approvals only)

Level: Delegated Business Meeting

Ref. No. : 19/05861/DISC
Location : Land Adjacent To Malling Close And Land
Adjacent To Stockbury Road
Croydon

Ward : Shirley North
Type: Discharge of Conditions

Proposal : Details pursuant to condition 17 (reduction in carbon dioxide) for 16/06422/FUL - Demolition of a single-storey temporary structure and garages. Erection of a four/ six storey building, 3 three-storey buildings and 1 two/four storey building to provide a total of 18 one bedroom and 23 two bedroom flats. Formation of vehicular access and provision of associated car parking, landscaping and other associated works.

Date Decision: 04.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05862/DISC
Location : Land Adjacent To Knole Close
Croydon

Ward : Shirley North
Type: Discharge of Conditions

Proposal : Discharge of Condition 7 (CO2 emissions) of LPA reference: 16/06372/FUL (Demolition of garages and erection of a three storey three bedroom dwellinghouse).

Date Decision: 03.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05981/HSE
Location : 55 Barnfield Avenue
Croydon
CR0 8SF
Proposal : Retention of single storey rear/side extension.

Ward : Shirley North
Type: Householder Application

Date Decision: 14.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 20/00074/LP
Location : 26 Wilks Gardens
Croydon
CR0 8UJ
Proposal : Erection of 3 dormers in rear roofslope, installation of 3 rooflights in front roofslope and removal of chimney stack.

Ward : Shirley North
Type: LDC (Proposed) Operations edged

Date Decision: 06.02.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 20/00094/GPDO
Location : 61 Orchard Way
Croydon
CR0 7NQ
Proposal : Erection of a single storey rear extension projecting out 6 metres from the rear wall of the original house with a height to the eaves of 2.95 metres and a maximum overall height of 2.95 metres

Ward : Shirley North
Type: Prior Appvl - Class A Larger House Extns

Date Decision: 07.02.20

(Approval) refused

Level: Delegated Business Meeting

Ref. No. : 20/00307/LP
Location : 2 Angelica Gardens
Croydon
CR0 8XB
Proposal :

Ward : Shirley North
Type: LDC (Proposed) Operations edged

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Proposal : Erection of single-storey rear extension.

Date Decision: 12.02.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 20/00360/LP

Ward : Shirley North

Location : 58 Ash Tree Way
Croydon
CR0 7SJ

Type: LDC (Proposed) Operations
edged

Proposal : Erection of hip to gable roof and rear dormer and installation of 3 rooflights in front
roofslope.

Date Decision: 14.02.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/04763/FUL

Ward : Shirley South

Location : Trinity School Of John Whitgift
Shirley Road
Croydon
CR0 7ER

Type: Full planning permission

Proposal : Second floor/roof extension to the existing Turner Building to provide 8 classrooms as
well as some office and storage spaces including alterations to the proposed fenestration.

Date Decision: 13.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05480/DISC

Ward : Shirley South

Location : Addington Golf Club
197-205 Shirley Church Road
Croydon
CR0 5AB

Type: Discharge of Conditions

Proposal : Discharge of Condition 2 (materials) attached to planning application for 19/03616/FUL
for Improvement of existing vehicle accesses to Golf Club to include creating an entrance
and exit and erection of walls, railings and pillar

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Date Decision: 05.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05786/HSE
Location : 2 Midholm Road
Croydon
CR0 8AN
Ward : **Shirley South**
Type: Householder Application
Proposal : Demolition of garage, erection of single storey side, front and rear extension, associated alterations

Date Decision: 30.01.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 20/00525/LP
Location : 8 Ferris Avenue
Croydon
CR0 8QQ
Ward : **Shirley South**
Type: LDC (Proposed) Operations edged
Proposal : Extension of the existing roof to provide a loft conversion.

Date Decision: 14.02.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/03780/CAT
Location : 21A South Norwood Hill
South Norwood
London
SE25 6AA
Ward : **South Norwood**
Type: Works to Trees in a Conservation Area
Proposal : Fence Line - Cut all trees back to fence line boundary - Known as mixed group

Date Decision: 29.01.20

No objection (tree works in Con Areas)

Level: Delegated Business Meeting

Ref. No. : 19/04820/ADV
Ward : **South Norwood**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 10 High Street
South Norwood
London
SE25 6EP
Type: Consent to display advertisements
Proposal : Non-Illuminated fascia signs

Date Decision: 06.02.20

Consent Granted (Advertisement)

Level: Delegated Business Meeting

Ref. No. : 19/05528/DISC
Location : 24 Station Road
South Norwood
SE25 5AF
Type: Discharge of Conditions
Ward : **South Norwood**
Proposal : Discharge of condition 3 (details and samples of the external materials to be used for the development, including fenestration and signage - to change the signage details from that approved under reference 18/04617/DISC) pursuant to planning permission 16/06491/FUL for the Erection of a four/ five storey building comprising 10 one bedroom and 4 two bedroom flats and 227 sq m commercial space (Flexible Use Class A1 - A3 and D1) together with landscaping and other associated works

Date Decision: 05.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05603/LP
Location : 16 King's Road
South Norwood
London
SE25 4ES
Type: LDC (Proposed) Operations edged
Ward : **South Norwood**
Proposal : House in Multiple Occupation - C4
internal alterations as shown on attached plans.

Date Decision: 29.01.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 20/00162/LP
Ward : **South Norwood**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 24 Sundial Avenue
South Norwood
London
SE25 4BX

Type: LDC (Proposed) Operations
edged

Proposal : Erection of hip to gable and rear dormer and installation of 3 rooflights in front roofslope.

Date Decision: 07.02.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 20/00163/LP
Location : 32 Southern Avenue
South Norwood
London
SE25 4BS

Ward : **South Norwood**
Type: LDC (Proposed) Operations
edged

Proposal : Erection of hip to gable and rear dormer and installation of 3 rooflights in front roofslope.

Date Decision: 07.02.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 20/00239/CAT
Location : Harris Academy South Norwood
2 Cumberlow Avenue
South Norwood
London
SE25 6AE

Ward : **South Norwood**
Type: Works to Trees in a
Conservation Area

Proposal : T10: Narrowleaf Ash - shorten lateral branches back from fenceline to achieve a 2m clearance. T18: Narrowleaf Ash - Shorten branches away from metal fence to achieve a 0.5m clearance. T2: Plum - Fell and treat stump. T3: Black Elder - Fell and treat stump. T6: Ash - Prune out branch stubs. T7: Sycamore - Fell and treat stump. T46: Common Beech - Shorten laterals to allow a 2.0m clearance from building. T47: Narrowleaf Ash - Reduce crown height - by 1.5m. Reduce crown radial spread by 1.5m.

Date Decision: 14.02.20

No objection (tree works in Con Areas)

Level: Delegated Business Meeting

Ref. No. : 19/04027/FUL

Ward : **Selsdon Vale And Forestdale**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 226 Addington Road
South Croydon
CR2 8LD

Type: Full planning permission

Proposal : Alterations to existing front and rear elevation, installation of roof lights on front roof slope, alterations, erection of roof extension to include dormer extensions on rear roof slope, conversion of uppers floors to form 3 one bedroom flats and installation of balconies at rear at first and second floor levels and associated refuse, cycle stores and parking

Date Decision: 12.02.20

Permission Granted

Level: Planning Committee - Minor Applications

Ref. No. : 19/04748/HSE
Location : 193 Markfield
Court Wood Lane
Croydon
CR0 9HR

Ward : **Selsdon Vale And Forestdale**
Type: Householder Application

Proposal : Removal of 2 metres of soil into the rear of the garden to increase size of patio area (retrospective).

Date Decision: 14.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05706/LP
Location : 40 Boxford Close
South Croydon
CR2 8SY

Ward : **Selsdon Vale And Forestdale**
Type: LDC (Proposed) Operations edged

Proposal : Loft conversion including erection of a rear dormer with rooflights.

Date Decision: 29.01.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 19/05881/DISC
Location : 2 Elmpark Gardens
South Croydon
CR2 8RU

Ward : **Selsdon Vale And Forestdale**
Type: Discharge of Conditions

Proposal : Discharge of Condition 3 (cycle & refuse storage, boundary treatments and visibility splays) and Condition 5 (Construction Management Plan) attached to planning permission ref.19/02272/FUL.

Date Decision: 07.02.20

Not approved

Level: Delegated Business Meeting

Ref. No. : 19/04103/FUL **Ward : Thornton Heath**
Location : 116 Moffat Road **Type: Full planning permission**
Thornton Heath
CR7 8PW
Proposal : Rear extensions and other alterations to convert the building into two flats
Date Decision: 31.01.20

Withdrawn application

Level: Delegated Business Meeting

Ref. No. : 19/05015/FUL **Ward : Thornton Heath**
Location : 26 Beulah Road **Type: Full planning permission**
Thornton Heath
CR7 8JE
Proposal : Alterations and erection of second floor rear/loft extension, Conversion to form 1 x three bedroom flat and 1 x one bedroom flat with associated refuse and cycle storage
Date Decision: 31.01.20

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/05591/HSE **Ward : Thornton Heath**
Location : 2C Zion Place **Type: Householder Application**
Thornton Heath
CR7 8RR
Proposal : Alterations to the roof, including dormers in the rear roof slope and roof lights in the front roof slope.
Date Decision: 29.01.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05654/FUL **Ward : Thornton Heath**
Location : 23 Norfolk Road **Type: Full planning permission**
Thornton Heath
CR7 8ND
Proposal : Conversion of a existing house into 2 flats, 1 two bed and 1 three bed.

Ref. No. : 19/05915/GPDO
Location : 47 Norbury Avenue
Thornton Heath
CR7 8AL

Ward : Thornton Heath
Type: Prior Appvl - Class A Larger
House Extns

Proposal : Erection of single storey rear extension projecting out 6 metres with a maximum height of 3 metres

Date Decision: 28.01.20

Prior Approval No Jurisdiction (GPDO)

Level: Delegated Business Meeting

Ref. No. : 19/05977/DISC
Location : The Welcome Inn
300 Parchmore Road
Thornton Heath
CR7 8HB

Ward : Thornton Heath
Type: Discharge of Conditions

Proposal : Detail pursuant Condition 5 (d) Ventilation arrangement in respect to planning ref 18/01213/ful granted for alterations including construction of single storey addition to the rear outbuilding and partial demolition of single storey rear extension to existing pub in connection with the retention of the A4 public house use at the basement and ground floor levels, and conversion of the upper floors to provide 4 x 1 bed flats and conversion of the rear out building to provide a 1 bedroom maisonette cottage.

Date Decision: 04.02.20

Part Approved / Part Not Approved

Level: Delegated Business Meeting

Ref. No. : 19/05978/DISC
Location : The Welcome Inn
300 Parchmore Road
Thornton Heath
CR7 8HB

Ward : Thornton Heath
Type: Discharge of Conditions

Proposal : Details pursuant to Condition 6 (refuse provision) and Condition 7 (cycle storage) in respect to planning permission 18/01213/ful granted for Alterations including construction of single storey addition to the rear outbuilding and partial demolition of single storey rear extension to existing pub in connection with the retention of the A4 public house use at the basement and ground floor levels, and conversion of the upper floors to provide 4 x 1 bed flats and conversion of the rear out building to provide a 1 bedroom maisonette cottage

Date Decision: 04.02.20

Not approved

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Level: Delegated Business Meeting

Ref. No. : 19/02877/FUL **Ward : Waddon**
Location : 9 Court Drive **Type: Full planning permission**
Croydon
CR0 4QA
Proposal : Retrospective planning application for single storey extension to the outbuilding and to continue the use of the outbuilding as a music teaching room and one on one voice coaching (Use Class D2)
Date Decision: 07.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/03492/FUL **Ward : Waddon**
Location : 13 Stafford Road **Type: Full planning permission**
Croydon
CR0 4NG
Proposal : Change of use of ground floor from tattoo parlour (sui generis) to residential (class C3) and associated external alterations comprising of new shopfront.
Date Decision: 06.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04106/DISC **Ward : Waddon**
Location : Garage Blocks Rear Of 38 - 40 **Type: Discharge of Conditions**
Thorneloe Gardens
Croydon
CR0 4EN
Proposal : Discharge of Condition 21 (Drainage and surface water disposal) attached to permission 16/06337/P for 'Demolition of garages and erection of a two to three-storey building comprising 6 one bedroom and 4 two bedroom flats together with car parking, landscaping and other associated works.'
Date Decision: 07.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05503/LE **Ward : Waddon**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 3 Borough Hill
Croydon
CR0 4LP

Type: LDC (Existing) Use edged

Proposal : Lawful Development for use of Existing Flats 1, 2 & 3 as two bedroom flats for a period exceeding 4 years.

Date Decision: 31.01.20

Lawful Dev. Cert. Granted (existing)

Level: Delegated Business Meeting

Ref. No. : 19/05646/FUL
Location : 23 Stafford Road
Croydon
CR0 4NG

Ward : **Waddon**
Type: Full planning permission

Proposal : Application for an external shutter on the front of the premises

Date Decision: 14.02.20

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/05694/FUL
Location : Flat 2, 52 Waddon Road
Croydon
CR0 4LF

Ward : **Waddon**
Type: Full planning permission

Proposal : Front door and toplight to be moved forward

Date Decision: 04.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05765/GPDO
Location : 104 South End
Croydon
CR0 1DQ

Ward : **Waddon**
Type: Prior Appvl - Class O offices to houses

Proposal : Conversion of the office building into 3 residential units

Date Decision: 30.01.20

Approved (prior approvals only)

Level: Delegated Business Meeting

Ref. No. : 19/05803/HSE
Ward : **Waddon**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 64 Croydon Road
Croydon
CR0 4PB
Type: Householder Application
Proposal : Excavation of part of rear garden adjacent to outbuilding (retrospective).

Date Decision: 31.01.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05870/FUL
Location : 29 Cuthbert Road
Croydon
CR0 3RB
Ward : **Waddon**
Type: Full planning permission

Proposal : Erection of single storey rear extension and associated external alterations to facilitate conversion of office accommodation B1(a) to 3 bedroom dwelling (C3).

Date Decision: 06.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05925/NMA
Location : 1 Borough Hill
Croydon
CR0 4LP
Ward : **Waddon**
Type: Non-material amendment

Proposal : Non-material Amendment to planning permission 16/05004/FUL (Excavation of the basement area to form an additional one bedroom flat; erection of railings enclosure at the front) alterations including the repositioning of the stairs from the rear to the front of the property, construction of lightwell at the rear and railings enclosure at the front.

Date Decision: 11.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05961/LE
Location : 71 Ravenswood Road
Croydon
CR0 4BL
Ward : **Waddon**
Type: LDC (Existing) Use edged

Proposal : Use of dwelling as HMO for up to 6 occupants.

Date Decision: 30.01.20

Lawful Dev. Cert. Granted (proposed)

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Level: Delegated Business Meeting

Ref. No. : 19/05968/ADV **Ward : Waddon**
Location : 330 Purley Way Type: Consent to display
Croydon advertisements
CR0 4XJ
Proposal : Installation of 1 x illuminated fascia and 3 x non illuminated signs
Date Decision: 30.01.20

Consent Granted (Advertisement)

Level: Delegated Business Meeting

Ref. No. : 19/06004/FUL **Ward : Waddon**
Location : 45 Imperial Way Type: Full planning permission
Croydon
CR0 4RR
Proposal : First floor extension for use as storage and office space ancillary to the hotel
Date Decision: 12.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/06014/FUL **Ward : Waddon**
Location : 37A Stafford Road Type: Full planning permission
Croydon
CR0 4NG
Proposal : Alterations, including the erection of a second floor rear extension.
Date Decision: 14.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 20/00038/NMA **Ward : Waddon**
Location : The Minster Junior School Type: Non-material amendment
Warrington Road
Croydon
CR0 4BH
Proposal : Non material amendment to 18/02965/FUL including change of description - from-
Installation of an artificial sand-dressed sports pitch and associated floodlighting and
fencing to -Installation of an artificial sports pitch and associated floodlighting
Date Decision: 06.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 20/00111/FUL
Location : J Sainsbury Plc
2 Trafalgar Way
Croydon
CR0 4XT
Proposal : Replacement of two existing gas coolers
Date Decision: 13.02.20

Ward : Waddon
Type: Full planning permission

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 20/00133/DISC
Location : 11 Barham Road
South Croydon
CR2 6LD
Proposal : Discharge of condition 7 (Construction Method Statement) attached to planning permission 18/03319/FUL for the conversion of property to form 5 self-contained flats (3x 1 bed, 1 x 2 bed and 1 x 3 bed flats), formation of basement accommodation with associated front and rear light wells, erection of a part 1, part 2 storey side and rear extensions, erection of a rear roof dormer, insertion of 3 roof lights in front roof slope and provision of associated hard and soft landscaping, a new front boundary wall and refuse and cycle parking (allowed on appeal ref. APP/L5240/W/18/3216899).
Date Decision: 05.02.20

Ward : Waddon
Type: Discharge of Conditions

Approved

Level: Delegated Business Meeting

Ref. No. : 19/01923/FUL
Location : 1A And 1B Holland Road
South Norwood
London
SE25 5RF
Proposal : Extensions to building including an additional storey to create 3 x 2 bed flats, replacing the existing 2 x 1 bedroom flats
Date Decision: 07.02.20

Ward : Woodside
Type: Full planning permission

Permission Refused

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 19-23 Clifford Road
South Norwood
London
SE25 5JJ

Type: Discharge of Conditions

Proposal : Discharge of Condition 13 - Contaminated Land - attached to Planning Permission 17/06263/FUL for Alterations, Partial-demolition of buildings, alterations to existing buildings including roof and lower ground floor, erection of three storey side extension, formation of 13 one bedroom and 2 studio flats at lower ground, ground, first and second floors, formation of B1 (c) light industrial unit at ground floor, provision of associated cycle storage and bin storage, provision of associated amenity space.

Date Decision: 29.01.20

Not approved

Level: Delegated Business Meeting

Ref. No. : 19/05698/FUL

Ward : **Woodside**

Location : 75 Crowther Road
South Norwood
London
SE25 5QR

Type: Full planning permission

Proposal : Erection of a new end of terrace two storey, one bedroom dwelling

Date Decision: 28.01.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/05708/FUL

Ward : **Woodside**

Location : 88 Portland Road
South Norwood
London
SE25 4PQ

Type: Full planning permission

Proposal : Alterations to front and side elevations, conversion of ground floor shop (A1) to form 2 x studio flats (C3) and part demolition and erection of a single storey rear extension and associated amenity space, cycle parking and refuse storage

Date Decision: 29.01.20

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/05817/GPDO

Ward : **Woodside**

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : 72 Woodside Green
South Norwood
London
SE25 5EU
Type: Prior Appvl - Class O offices to houses

Proposal : Prior Approval applicaiton under Schedule 2 Part 3 Class O of GPDO 2015 as amended for conversion of existing B1 office use into C3 residential use to form 6 flats.

Date Decision: 04.02.20

Approved (prior approvals only)

Level: Delegated Business Meeting

Ref. No. : 19/05829/DISC
Location : Spence Court
7 Woodside Green
South Norwood
London
SE25 5EY
Type: Discharge of Conditions
Ward : Woodside

Proposal : Discharge of condition 7(reduction in CO2 emissions) pursuant to planning permission 16/03106/P for the construction of a second floor extension to provide an additional three bedroom flat; construction of roof terrace and roof garden.

Date Decision: 05.02.20

Not approved

Level: Delegated Business Meeting

Ref. No. : 19/05888/GPDO
Location : 19 Dundee Road
South Norwood
London
SE25 4QN
Type: Prior Appvl - Class A Larger House Extns
Ward : Woodside

Proposal : Erection of single storey rear extension projecting out 5.96 metres with a maximum height of 3.5 metres

Date Decision: 28.01.20

Prior Approval No Jurisdiction (GPDO)

Level: Delegated Business Meeting

Ref. No. : 20/00193/LP
Location : 107 Estcourt Road
South Norwood
London
SE25 4SA
Type: LDC (Proposed) Operations edged
Ward : Woodside

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Proposal : Use of dwelling as a small HMO within Use Class C4

Date Decision: 29.01.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 18/05898/DISC

Ward : West Thornton

Location : 791 London Road
Thornton Heath
CR7 6AW

Type: Discharge of Conditions

Proposal : Discharge of Conditions 2, 4, 7, 10, 11 of Planning Permission Ref 18/01106/FUL for Demolition of existing buildings, erection of part two / part four storey building comprising ground floor retail unit (A1 Use Class), 6 one bedroom flats, 1 studio flat and 2 two bedroom flats, provision of associated refuse and cycle storage

Date Decision: 14.02.20

Part Approved / Part Not Approved

Level: Delegated Business Meeting

Ref. No. : 19/02886/FUL

Ward : West Thornton

Location : 702 Mitcham Road
Croydon
CR0 3AB

Type: Full planning permission

Proposal : Removal of existing structures, erection of a 4 storey building comprising 9 two bedroom flats, provision of associated off-street parking, and cycle storage, and refuse storage.

Date Decision: 06.02.20

Permission Granted

Level: Delegated Business Meeting

Ref. No. : 19/04989/DISC

Ward : West Thornton

Location : 280-288 Thornton Road
Croydon
CR0 3EU

Type: Discharge of Conditions

Proposal : Details pursuant to condition 1 (Construction Logistics Plan) for planning permission ref. 18/03278/FUL for Demolition of existing buildings and erection of a part 3, part 4 storey building comprising 24 self-contained residential units, ancillary on-site car and cycle parking, refuse and recycling and landscaping.

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Proposal : Details pursuant to condition 3 (Drainage details) for planning permission ref. 18/03278/FUL for Demolition of existing buildings and erection of a part 3, part 4 storey building comprising 24 self-contained residential units, ancillary on-site car and cycle parking, refuse and recycling and landscaping.

Date Decision: 29.01.20

Approved

Level: Delegated Business Meeting

Ref. No. : 19/05804/FUL
Location : 30 Mayfield Road
Thornton Heath
CR7 6DG
Ward : West Thornton
Type: Full planning permission

Proposal : Alterations, conversion of single dwelling to 1 x 1 bedroom unit and 1 x 2 bedroom unit, erection of a dormer in the rear roof slope, roof lights in the front roof slope and single storey rear extension, with associated refuse and cycle storage

Date Decision: 07.02.20

Permission Refused

Level: Delegated Business Meeting

Ref. No. : 19/05917/GPDO
Location : 58 Headcorn Road
Thornton Heath
CR7 6JP
Ward : West Thornton
Type: Prior Appvl - Class A Larger House Extns

Proposal : Erection of single storey rear extension projecting out 6 metres with a maximum height of 3.5 metres

Date Decision: 28.01.20

Prior Approval No Jurisdiction (GPDO)

Level: Delegated Business Meeting

Ref. No. : 19/05949/HSE
Location : 53 Boston Road
Croydon
CR0 3EG
Ward : West Thornton
Type: Householder Application

Proposal : Erection of single/two storey side/rear extensions and erection of dormer extension in rear roofslope

Date Decision: 14.02.20

Permission Refused

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Level: Delegated Business Meeting

Ref. No. : 19/06024/GPDO
Location : 83 Wharfedale Gardens
Thornton Heath
CR7 6LE

Ward : West Thornton
Type: Prior Appvl - Class A Larger
House Extns

Proposal : Erection of a single storey rear extension which projects out by 6 metres from the rear wall of the original house with an eaves height of 3 metres and a maximum height of 4 metres

Date Decision: 04.02.20

Prior Approval No Jurisdiction (GPDO)

Level: Delegated Business Meeting

Ref. No. : 19/06077/LP
Location : 11 Buxton Road
Thornton Heath
CR7 7HJ

Ward : West Thornton
Type: LDC (Proposed) Use edged

Proposal : Change of use of Dwellinghouse (C3 use) to a HMO for 3-6 people (C4 use).

Date Decision: 29.01.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

Ref. No. : 20/00066/DISC
Location : 280-288 Thornton Road
Croydon
CR0 3EU

Ward : West Thornton
Type: Discharge of Conditions

Proposal : Details pursuant to condition 2 (Contaminated land - gas monitoring report only) for planning permission ref. 18/03278/FUL for Demolition of existing buildings and erection of a part 3, part 4 storey building comprising 24 self-contained residential units, ancillary on-site car and cycle parking, refuse and recycling and landscaping.

Date Decision: 07.02.20

Approved

Level: Delegated Business Meeting

Ref. No. : 20/00084/NMA

Ward : West Thornton

Appendix 1 - Decisions (Ward Order) since last Planning Control Meeting as at: 17th February 2020

Location : Silchester Court
London Road
Thornton Heath
CR7 6JD
Type: Non-material amendment

Proposal : Non-Material Amendment to Planning Permission 18/02719/FUL for Alterations, erection of front and rear roof extensions to form 4 x 1 bedroom flats and 5 x 2 bedroom flats, alterations to existing ground floor courtyard garage block, provision of associated parking, refuse storage and cycle storage.

Date Decision: 31.01.20

Approved

Level: Delegated Business Meeting

Ref. No. : 20/00509/LP
Location : 75 Leander Road
Thornton Heath
CR7 6JZ
Ward : **West Thornton**
Type: LDC (Proposed) Operations
edged

Proposal : Erection of rear dormer and installation of 2 rooflights in front roofslope.

Date Decision: 14.02.20

Lawful Dev. Cert. Granted (proposed)

Level: Delegated Business Meeting

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PLANNING COMMITTEE AGENDA

27th February 2020

Part 8 Other Planning Matters

Item 8.2

<p>Report of: Head of Development Management</p> <p>Author: Pete Smith</p>	<p>Title: Planning Appeal Decisions (January 2020)</p>
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1. PURPOSE

- 1.1 This report provides details of town planning appeal outcomes and the range of planning considerations that are being taken into account by the Planning Inspectors, appointed by the Secretary of State for Housing, Communities and Local Government.
- 1.2 The report covers all planning appeals, irrespective of whether the related planning application was determined by Planning Committee, Planning Sub Committee or by officers under delegated powers. It also advises on appeal outcomes following the service of a planning enforcement notice.
- 1.3 A record of appeal outcomes will also be helpful when compiling future Annual Monitoring Reports.

2. APPEAL DECISIONS

- 2.1 The following appeal decisions have been received by the Council during the reporting period.

<i>Application No:</i>	<i>18/03087/HSE</i>
<i>Site:</i>	<i>91 Hartland Way, CR0 8RJ</i>
<i>Proposed Development:</i>	<i>Erection of a wrap-around first-floor side and rear extension</i>
<i>Decision:</i>	<i>REFUSE PLANNING PERMISSION</i>
<i>Appeal Method:</i>	<i>WRITTEN REPRESENTATIONS</i>
<i>Inspector’s Decision</i>	<i>DISMISSED</i>
<i>Case Officer</i>	<i>Emil Ancewicz</i>
<i>Ward</i>	<i>Shirley South</i>

- 2.2 The main issue in this case was the effect of the first-floor flat roofed extension on the character and appearance of the surrounding area.
- 2.3 The Planning Inspector was concerned about the size of the proposed extension – on top of the existing flat roofed garage and ground floor rear extension. He concluded that the extension would have had an over-bulky appearance, in view of its scale and flat roofed design which would have poorly integrated with the existing building. He also felt that it would have been incongruous in relation to the Hartland Road street-scene.

2.4 The appeal was therefore DISMISSED.

Application No:	19/03603/HSE
Site:	52 Brook Road, Thornton Heath, CR7 7RB
Proposed Development:	Conversion of dwelling house to provide 2 self-contained flats
Decision:	REFUSE PLANNING PERMISSION
Appeal Method:	WRITTEN REPRESENTATIONS
Inspector's Decision	DISMISSED
Case Officer	Chris Grace
Ward	Bensham Manor

2.5 The main issues with this case were the principle of the loss of a small family house and whether the proposals would have provided a good standard accommodation for future occupiers (in terms of access to private amenity space).

2.6 At the time of the appeal site visit, the house had been stripped out internally and whilst the Planning Inspector was uncertain as to whether the original building was less than 130 square metres, he was satisfied that the conversion would have ended up in the loss of a three-bedroom dwelling, which would not have maintained existing supply of small family accommodation.

2.7 He was less concerned about the scheme's failure to deliver garden access for the upper floor flat – with that flat being oversized. He also saw no need to provide children's play facilities, in view of the lack of family accommodation proposed. That said, he remained concerned about the loss of family accommodation and DISMISSED the appeal on this basis alone.

Application No:	19/01451/HSE
Site:	18 Oaks Road, CR0 5HL
Proposed Development:	Erection of a two-storey side extension and a single-storey side extension
Decision:	REFUSE PLANNING PERMISSION
Appeal Method:	WRITTEN REPRESENTATIONS
Inspector's Decision	DISMISSED
Case Officer	Sam Dixon
Ward	Shirley South

2.8 This house is located within the Green Belt and the issues in this case focussed on the following:

- The appropriateness of the development within the Green Belt
- The effect of the development on openness of the Green Belt and the character and appearance of the area
- Whether any benefits outweighed the harm caused

- 2.9 The house is a large two storey property, situated within the Green Belt with an adjoining golf course to the rear. The Planning Inspector concluded that the two extensions (taken together) would have substantially exceeded the floorspace threshold outlined in CLP 2018 and would have entailed a disproportionate addition to a building within the Green Belt.
- 2.10 He also concluded that the scale of extensions would have resulted in a loss of openness, both spatially and visually. He felt that the scheme would have substantially infilled the spatial gaps between properties which he also felt was an integral part of the area's character. He also felt that the extensions would have been out of character with the host property, resulting in a building that would have appeared overly-stretched.
- 2.11 He saw little merit in the proposal to justify very special circumstances which needed to be significant to outweigh the harm caused to the Green Belt. The appeal was therefore DISMISSED.

Application No:	18/06057/FUL
Site:	91 High Street, Thornton Heath, CR7 8RY
Proposed Development:	Change of use of ground floor shop to a gaming centre (sui generis).
Decision:	REFUSE PLANNING PERMISSION
Appeal Method:	WRITTEN REPRESENTATIONS
Inspector's Decision	HEARING
Case Officer	James Udall
Ward	Thornton Heath

2.12 The main issue in this case was as follows:

- The effect of the development on the vitality and viability of the High Street
- The effect of the development on the living conditions of neighbouring residents
- Effect of the shop front alterations on the character and appearance of the host property

2.13 The unit is located within a secondary shopping frontage and a gaming centre does not fall within the range of uses that would normally have been acceptable within such areas. However, the Planning Inspector was satisfied that the property (which had been vacant for some considerable time) had been marketed sufficiently (since 2017). He was also concerned about the appearance of the property which he concluded was detrimental to the character of the area appearance of the area, with the incoming use contributing to vitality and viability. He acknowledged that the unit had been actively marketing in and around Thornton Heath but to no effect.

2.14 The appeal also focussed on the effect of the development on the living conditions of neighbours and mostly around the hours of use and the links

between the planning and licencing regimes. The appellant submitted three hours-of-use options (suggested planning conditions) and submitted evidence as to the likely occupation of the unit during the evening and into the early hours. He accepted that the level of noise likely to be generated by the use would have been low, although he recognised that there would be a regular flow of customers which would have had the potential to generate some noise which he felt could cause some inconvenience. In allowing the appeal, he ended up imposing a planning condition limiting the hours of use to 0700-00.00 Monday to Saturday and 0700-2300 Sundays and Bank Holidays.

2.15 As regards the shopfront works, there was much debate about the illumination of the shop front and the extent to which it would have jarred with views of the Thornton Heath clock-tower, which is recognised as an important local landmark. He accepted that a brightly illuminated shop front would have been incongruous and in view of this, he imposed a planning condition requiring details of the extent and form of the shop front illumination.

2.16 Views were expressed during the Hearing about suitability of the use in terms of the negative influence it might have on vulnerable people – also being close to schools and the Thornton Heath Leisure Centre. He was satisfied that the management of the premises (controlled through the licencing regime) would have satisfactorily dealt with these issues.

2.17 The appeal was ALLOWED.

Application No:	18/06176/FUL
Site:	64 Foxley Lane, CR8 3EE
Proposed Development:	Erection of 3x2 storey, 2 bed mews houses to the rear of 64 Foxley Lane
Decision:	REFUSE PLANNING PERMISSION
Appeal Method:	WRITTEN REPRESENTATIONS
Inspector's Decision	DISMISSED
Case Officer	Hayley Crabb
Ward	Purley and Woodcote

2.18 The main issues in this case were as follows:

- The effect of the development on the character and appearance of the area and the living conditions of the occupiers of neighbouring properties (especially the degree of privacy and level of outdoor amenity space for residents of 64 Foxley Lane - in use as flats)
- The effect of the development on the amenities of future occupiers
- The level of car parking associated with the proposed development
- The suitability of refuse storage arrangements and pedestrian access
- Whether the housing mix was acceptable

2.19 64 Foxley Lane is a semi-detached property situated on the north side of

Foxley Lane and has had a somewhat involved and complex planning history over the years. The existing rear outbuilding (which was extended a few years ago and used for residential purposes without planning permission) has been the subject of previous enforcement action and subsequent prosecution and confiscation proceedings. Officers are still working with the owner of the land to remove unauthorised structures; more recently, parts of the unauthorised extensions have been removed with the enforcement notice incrementally being complied with. Continued pressure is being exerted on the owner of the land to ensure full compliance is being suitably maintained.

- 2.15 These proposed mews houses would have been sited towards the rear part of the garden (which rises front to back and then drops down to a rear access-way). The Planning Inspector noted that the development would have required significant excavation and would have involved the removal of the outbuilding (referred to above). He noted that the mews houses would have almost occupied the full width of the plot and would have taken up most of the existing garden (leaving existing flats with no meaningful amenity space). He felt that this would have been at odds with the prevailing character and established pattern of development. He also concluded that the flat roofed appearance would have been at odds with surrounding character and appearance of surrounding development.
- 2.16 The separation between the existing and proposed properties would have been around 13 metres and he was concerned that this would have resulted in an unacceptable level of overlooking, causing an unacceptable level of intrusion and mutual loss of privacy. He was also concerned about the significant loss of amenity space for existing occupiers (62, 64 and 66 Foxley Lane). Whilst he was not concerned about daylight/sunlight effects, he accepted the Council's arguments levelled at the loss of outlook and visual intrusion.
- 2.17 As regards car parking and highways issues, the development ended up being proposed as "car free", as the applicant could not substantiate a vehicular access right from the north. The PTAL is low and he was far from convinced that future occupiers would not have owned a car and he was far from satisfied that this development would not have unacceptably added to existing on street car parking pressures, adversely affecting the safety of the highway and other highway users. He also agreed with the Council that the single pedestrian access to the houses (up a steep pathway to the side of 64 Foxley Lane) would have been challenging for those future residents with reduced mobility.
- 2.18 Turning to refuse storage and collection, he felt that the arrangements would have been convoluted and inconvenient for future residents – taking bins up a steeply sloping site (over quite a distance).
- 2.19 Finally, he saw no basis to require the delivery of the 30% strategic target for 3 bed homes, bearing in mind that the scheme proposed less than 10 residential units.

2.20 The scheme was comprehensively DISMISSED and officers are now continuing to seek full compliance of the planning enforcement notice.

Application No:	19/00521/FUL
Site:	138 Brigstock Road, Thornton Heath, CR7 7JB
Proposed Development:	Change of use of property (with a hip to gable roof extension and a first-floor side extension and rear basement extension) from 4x2 bed flats to a 13-bed hotel
Decision:	REFUSE PLANNING PERMISSION
Appeal Method:	WRITTEN REPRESENTATIONS
Inspector's Decision	DISMISSED
Case Officer	Paul Young
Ward	Bensham Manor

2.21 The main issue in this case included the following issues

- The effect of the development on the character and appearance of the area
- The effect on the living conditions of immediate neighbours (overshadowing, outlook, noise and disturbance)
- Highway safety concerns
- Loss of housing

2.22 The appeal property comprises a two-storey detached building with a hipped-roof arrangement and a two-storey flat roofed extension. Whilst the Planning Inspector noted that most of the buildings within Brigstock Road had hipped roofs – he accepted that there was some form of variation which led him to accept the principle of the proposed hip to gable extension. However, he concluded that the proposed side and rear roof extensions would have added significantly to the bulk of the building (doubling its footprint) which would have resulted in an overly dominant bulk towards the rear and a blank elevation to its north eastern side. He also concluded that the rear mansard arrangement would have exacerbated the degree of discordance.

2.23 In terms of neighbour impact, he concluded that in view of the scale of extensions and the likely effect on daylight, sunlight and visual intrusion caused (linked to the additional comings and goings associated with a 13-bed hotel) neighbour amenity would have been harmed. He was also far from satisfied that evidence had been submitted by the appellant to confirm that the proposed development would not have been harmful to highway safety (especially car parking stress) and agreed with the Council that the loss of the 4 flats would have ran contrary of development plan policy.

2.24 The scheme was comprehensively DISMISSED.